

PROJECT LOCATION

LOCATION SKETCH

THIS PROJECT HAS BEEN PREPARED USING THE HORIZONTAL GEORGIA COORDINATE SYSTEM OF 1984 (NAD 1983/94 EAST ZONE, AND THE NORTH AMERICAN VERTICAL DATUM (NAVD) OF 1988.

DESIGN DATA:  
TRAFFIC A.D.T.: N/A  
TRAFFIC A.D.T.: N/A  
% TRUCKS: N/A  
SPEED DESIGN: 35 MPH

LOCATION & DESIGN APPROVAL DATE: N/A

FUNCTIONAL CLASS:  
LOCAL ROAD/STREET - URBAN

THIS PROJECT IS 100% IN DEKALB COUNTY AND IS 100% IN CONG. DIST. NO. 06.

PROJECT DESIGNATION: S.F.  
DESIGNED IN ENGLISH UNITS.

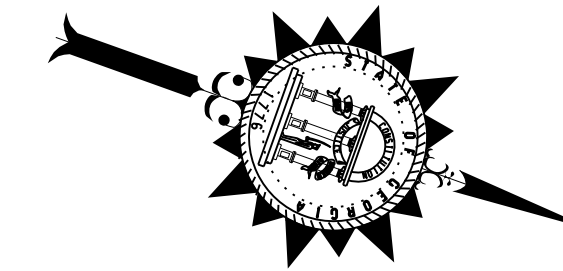
24 HOUR CONTACT:

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Name  
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Brookhaven, GA 30319  
City, State Zip  
404-637-0524  
Phone Number  
hari.karikaran@brookhavenga.gov  
Email Address

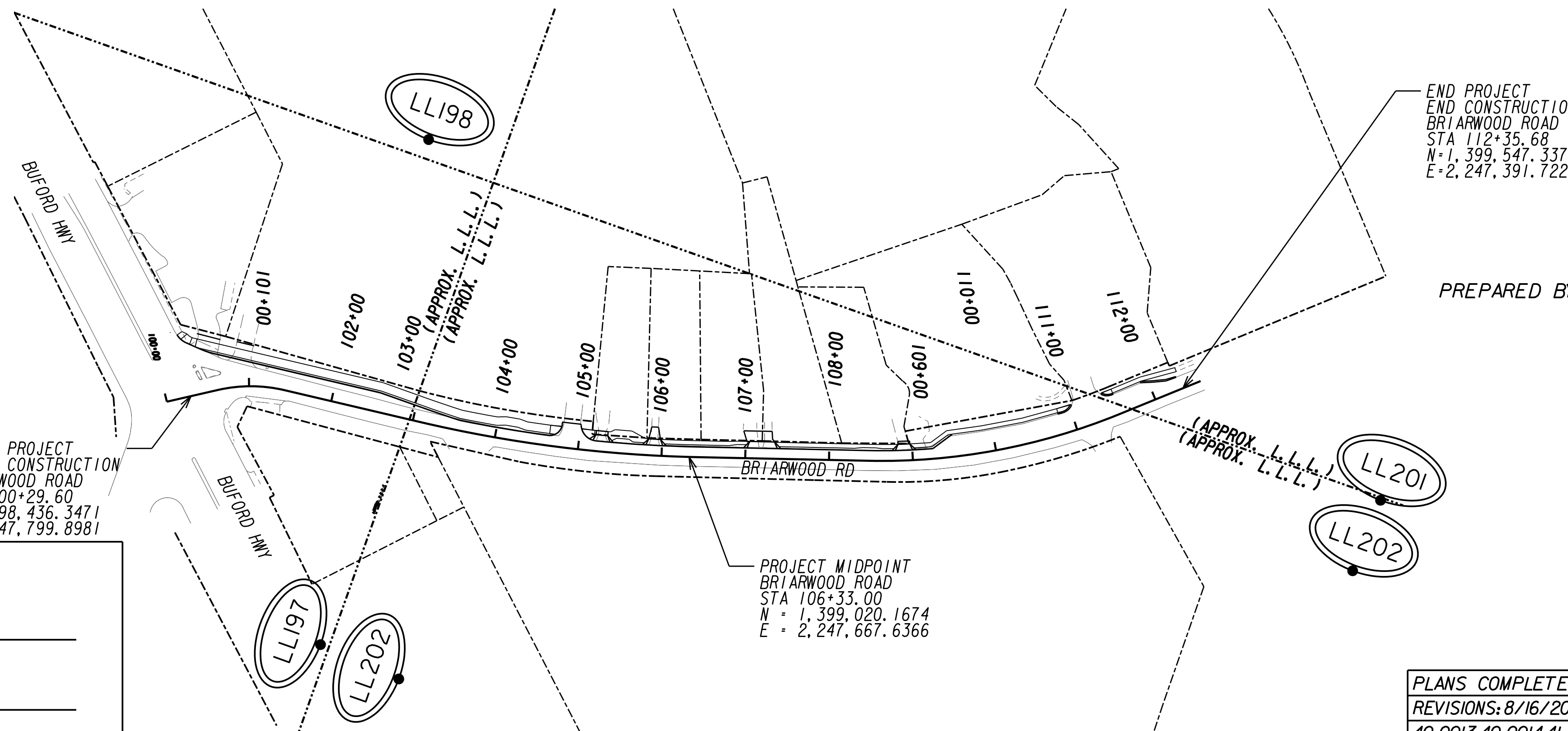
# CITY OF BROOKHAVEN PUBLIC WORKS DEPARTMENT

## PLAN AND PROFILE OF PROPOSED BRIARWOOD ROAD SIDEWALK PROJECT

FEDERAL ROUTE \* N/A  
STATE ROUTE \* N/A  
P.I. NO. WALK 16-110



Know what's below.  
Call before you dig.



BEGIN PROJECT  
BEGIN CONSTRUCTION  
BRIARWOOD ROAD  
STA 100+29.60  
N = 1,398,436.3471  
E = 2,247,799.8981

PROJECT MIDPOINT  
BRIARWOOD ROAD  
STA 106+33.00  
N = 1,399,020.1674  
E = 2,247,667.6366

END PROJECT  
END CONSTRUCTION  
BRIARWOOD ROAD  
STA 112+35.68  
N = 1,399,547.3376  
E = 2,247,391.7220

PREPARED BY:



**ICE INFRASTRUCTURE**  
CONSULTING & ENGINEERING  
4940 Peachtree Industrial Blvd., Suite 310  
Norcross, GA 30071  
Phone: 470-233-7021  
Email: tyler.mcintosh@ice-eng.com

DESIGN

	GEORGIA SOIL AND WATER CONSERVATION COMMISSION
Donald T McIntosh	
Level II Certified Design Professional	
CERTIFICATION NUMBER	0000062488
ISSUED	10/19/2016
EXPIRES	10/19/2019

LENGTH OF PROJECT	Project No. WALK 16-110
	MILES
NET LENGTH OF ROADWAY	0.228
NET LENGTH OF BRIDGES	0.000
NET LENGTH OF PROJECT	0.228
NET LENGTH OF EXCEPTIONS	0.000
GROSS LENGTH OF PROJECT	0.228



PLANS COMPLETED	07-18-2019
REVISIONS:	8/16/2016: ADDED SHTS 4-0003, 40-0013, 40-0014, 41-0002A, 41-0002B, 56-0007, 56-0008, 56-0009; REV SHTS 1-0001, 2-0001, 3-0001, 4-0001, 4-0002, 5-0001, 5-0002, 5-0003, 6-0001, 9-0001, 13-0001, 24-0001, 26-0001, 26-0003, 32-0001, 41-0002, 51-0001, 52-0007, 54-0001, 54-0002, 54-0003
	9/3/2019; REV SHTS 1-0001, 3-0001, 56-0004

DWG. NO.		DESCRIPTION	
01-0001		COVER SHEET	
02-0001		INDEX	
03-0001		REVISION SUMMARY	
04-0001 TO 04-0003		GENERAL NOTES	
05-0001 TO 05-0003		TYPICAL SECTIONS	
06-0001		SUMMARY OF QUANTITIES	
07-0001		QUANTITIES REQUIRED BY AMENDMENT	
08-0001		QUANTITIES REQUIRED ON CONSTRUCTION	
09-0001		DETAILED ESTIMATE	
13-0001 TO 13-0003		MAINLINE PLAN	
15-0001 TO 15-0003		MAINLINE PROFILE	
17-0001		DRIVEWAY PROFILES	
23-0001 TO 23-0006		EARTHWORK CROSS SECTIONS	
24-000A TO 24-000B		UTILITY PLANS LEGEND & SUMMARY	
24-0001 TO 24-0003		UTILITY PLANS	
26-0001 TO 26-0003		SIGNING AND MARKING PLANS	
32-0001		RETAINING WALL PLANS	
38-0001 TO 38-0002		SPECIAL CONSTRUCTION DETAIL	
CONSTRUCTION DETAILS			DATE
40-0001	A1	DRIVEWAYS WITH TAPERED ENTRANCES CONCRETE VALLEY GUTTERS	7/21/2011
40-0002	A2	CONCRETE VALLEY GUTTER AT STREET INTERSECTION, 6" OR 8" CONCRETE VALLEY GUTTER AT DRIVE, PLACING PAVEMENT ADJACENT TO GUTTER, ADDITIONAL PAVING AT STREET INTERSECTION, 4' CORRUGATED CONCRETE MEDIAN	7/21/2011
40-0003	A3	CONCRETE SIDEWALK DETAILS CURB CUT (WHEELCHAIR) RAMPS	9/15/2016
40-0004	A4	DETECTABLE WARNING SURFACE TURNATED DOME SIZE, SPACING AND ALIGNMENT REQUIREMENTS	6/18/2009
40-0005	S-7	SERRATED SLOPE DETAIL BENCHING DETAIL	6/18/2009
40-0006	T-1	SIGN PLATES	1/1/2000
40-0007	T-3A	TYPE 7, 8 AND 9 SQUARE TUBE POST INSTALLATION DETAIL	7/1/2002
40-0008	T-5A	DETAILS OF REGULATORY SIGNS (SHEET 1 OF 2)	1/21/2003
40-0009	T-11A	PAVEMENT MARKING PLACEMENT NON-LIMITED ACCESS ROADWAY	9/15/2016
40-0010	T-12A	DETAILS OF PAVEMENT MARKING ARROW LOCATION	1/1/2000
40-0011	T-12B	DETAILS OF PAVEMENT MARKINGS - ARROWS	4/11/2000
40-0012	T-14	DETAIL OF PAVEMENT MARKING HATCHING	11/21/2008
40-0013	T-15A	DETAILS OF RAISED PAVEMENT MARKER LOCATION NON-LIMITED ACCESS ROADWAY	9/15/2016
40-0014	T-15C	DETAILS OF RAISED PAVEMENT MARKERS	9/22/2011
GEORGIA STANDARDS			DATE
41-0001	9031L	GRAVITY WALL TYPICAL SECTIONS, RAISING HEADWALL, AND TYPICAL PIPE PLUG (SHEET 1 OF 2)	9/30/2016
41-0002	ST-03	BROOKHAVEN STREETScape DETAIL ST-03, GUARD RAILING SYSTEMS (SHEET 1 OF 3)	8/28/2018
41-0002A	ST-03	BROOKHAVEN STREETScape DETAIL ST-03, GUARD RAILING SYSTEMS (SHEET 2 OF 3)	8/28/2018
41-0002B	ST-03	BROOKHAVEN STREETScape DETAIL ST-03, GUARD RAILING SYSTEMS (SHEET 3 OF 3)	8/28/2018
41-0003	9032B	CONCRETE CURB & GUTTER CONCRETE CURBS, CONCRETE MEDIANS	11/15/2011
41-0004	9102	TRAFFIC CONTROL DETAIL FOR LANE CLOSURE ON TWO-LANE HIGHWAY	3/30/2006
EROSION CONTROL PLANS			
51-0001		ESPCP GENERAL NOTES	
52-0001	EC-L1	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 1 OF 7)	3/2/2017
52-0002	EC-L2	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 2 OF 7)	11/28/2018
52-0003	EC-L3	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 3 OF 7)	3/2/2017
52-0004	EC-L4	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 4 OF 7)	3/2/2017
52-0005	EC-L5	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 5 OF 7)	3/2/2017
52-0006	EC-L6	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 6 OF 7)	11/28/2018
52-0007	EC-L7	EROSION CONTROL LEGEND AND UNIFORM CODE (SHEET 7 OF 7)	3/2/2017
54-0001 TO 54-0003		BMP LOCATION DETAILS	
EROSION CONTROL DETAILS AND GEORGIA STANDARDS			DATE
56-0001	D-24A	TEMPORARY SILT FENCE (SHEET 1 OF 4)	1/1/2011
56-0002	D-24B	TEMPORARY SILT FENCE BERM DITCH, INSTALLATION, BRUSH BARRIER (SHEET 2 OF 4)	1/1/2011
56-0003	D-24C	TEMPORARY SILT FENCE J-HOOKS, INLET SEDIMENT TRAPS (SHEET 3 OF 4)	1/1/2011
56-0004	D-41	CONSTRUCTION EXIT	4/18/2018
56-0005	D-54	SOD INSTALLATION	4/22/2016
56-0006		CURB INLET FILTER "PIGS IN BLANKET"	
56-0007	D-35	PERMANENT SOIL REINFORCING MAT (TURF REINFORCING MAT) INSTALLATION ON DITCHES	1/19/2011
56-0008	D-55A	RIPRAP OUTLET PROTECTION (SHEET 1 OF 2)	4/22/2016
56-0009	D-55B	RIPRAP OUTLET PROTECTION (SHEET 2 OF 2)	4/22/2016



REVISION DATES	
8/16/2019	

**INDEX**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	02-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	



### PROJECT GENERAL NOTES

1. AN N.O.I. (NOTICE OF INTENT) IS NOT REQUIRED FOR THIS PROJECT. THE TOTAL DISTURBED AREA IS 0.45 ACRES. THE TOTAL PROJECT AREA IS 0.57 ACRES.
2. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE GEORGIA DEPARTMENT OF TRANSPORTATION STANDARD AND SUPPLEMENTAL SPECIFICATIONS, CURRENT EDITION.
3. INGRESS AND EGRESS SHALL BE MAINTAINED AT ALL TIMES TO ADJACENT PROPERTIES. REFER TO SUB-SECTION 107.07 OF THE GEORGIA STANDARD SPECIFICATIONS.
4. RIGHT-OF-WAY MARKERS IN RESIDENTIAL LAWN AND DEVELOPED COMMERCIAL AREAS SHALL BE PLACED FLUSH WITH THE FINISHED SURFACE.
5. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO FURNISH SUITABLE BORROW MATERIAL FOR THE PROJECT AND DISPOSE OF ANY UNSUITABLE OR WASTE MATERIAL.
6. THE CONTRACTOR SHALL STRICTLY ADHERE TO DUST CONTROL REGULATIONS. ALL AREAS SUBJECTED TO DUST FORMATION MUST BE PERIODICALLY WATERED SUFFICIENT TO RETARD DUST. ALL COSTS FOR DUST CONTROL SHALL BE INCLUDED IN PRICE BID FOR GRADING COMPLETE - LUMP SUM.
7. STRUCTURES, TREES, SHRUBS AND OTHER PLANT MATERIAL THAT FALL WITHIN THE RIGHT-OF-WAY AND EASEMENT LIMITS, BUT OUTSIDE THE CONSTRUCTION, SHALL NOT BE DISTURBED UNLESS DIRECTED BY THE ENGINEER.
8. ALL DRIVEWAYS SHALL BE MAINTAINED DURING CONSTRUCTION. ALL DRIVEWAYS TO BE CONSTRUCTED SHALL BE REPLACED IN KIND, I.E. ASPHALT FOR ASPHALT, CONCRETE FOR CONCRETE ETC. ANY OTHER DRIVEWAY MATERIAL OR SPECIALIZED DRIVEWAY WILL NOT BE REPLACED IN KIND (I.E. PAVERS) AND WILL BE REPLACED WITH ASPHALT OR CONCRETE. ALL EARTH OR GRAVEL DRIVES SHALL BE PAVED WITH ASPHALT TO THE RIGHT-OF-WAY LIMIT OR TIE-IN POINT. DRIVEWAYS SHALL BE PAVED AS FOLLOWS:
  - ASPHALTIC DRIVES
  - RESIDENTIAL - 1-1/2" ASPH. CONC. 12.5 MM SUPERPAVE, 165 LB/SY  
- 6" GRADED AGGREGATE BASE
  - COMMERCIAL - 1/2" ASPH. CONC. 12.5 MM SUPERPAVE, 165 LB/SY  
- 2" ASPH. CONC. 19 MM SUPERPAVE, 220 LB/SY  
- 6" GRADED AGGREGATE BASE
  - CONCRETE DRIVES
  - RESIDENTIAL - 6" CONCRETE VALLEY GUTTER  
- 4" CONCRETE DRIVEWAY
  - COMMERCIAL - 8" CONCRETE VALLEY GUTTER  
- 6" CONCRETE DRIVEWAY
9. ALL CONCRETE SIDEWALKS AND WHEEL CHAIR RAMPS LOCATED IN THE RADIUS RETURN SHALL BE 8" THICKNESS.
10. AT LOCATIONS WHERE NEW PAVEMENT IS TO BE PLACED ADJACENT TO EXISTING PAVEMENT WITHOUT AN OVERLAY OR WHERE CURBING IS TO BE PLACED ACROSS A PAVED AREA, A JOINT SHALL BE SAWED ON A LINE ESTABLISHED BY THE ENGINEER TO ENSURE A PAVEMENT REMOVAL TO A NEAT LINE. THE COST FOR SAWED JOINTS, WHEN REQUIRED, SHALL BE INCLUDED IN PRICE BID FOR OTHER CONTRACT ITEMS, EXCEPT WHEN SAWING P.C.C. CONCRETE PAVEMENT.
11. THE CONTRACTOR'S ATTENTION IS DIRECTED TO ARTICLES 104.05 AND 107.07 OF THE GDOT STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND SEQUENCE OF OPERATIONS IN REGARDS TO MAINTENANCE OF TRAFFIC DURING CONSTRUCTION.
12. PRICE BID FOR TRAFFIC CONTROL - LUMP SUM SHALL INCLUDE, BUT IS NOT LIMITED TO CONSTRUCTION, MAINTENANCE AND REMOVAL OF TEMPORARY SIGNING AND PAVEMENT MARKINGS, BARRICADES, CHANNELIZING DEVICES ETC., REQUIRED FOR MAINTENANCE OF TRAFFIC DURING CONSTRUCTION. ALL TEMPORARY SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES LATEST EDITION AND/OR AS DIRECTED BY THE ENGINEER.
13. ALL CUT AND FILL SLOPES SHALL BE GRASSED AS DIRECTED BY THE ENGINEER IMMEDIATELY AFTER SLOPES ARE ESTABLISHED IN ORDER TO REDUCE EROSION. IF THE SEASON DOES NOT PERMIT GRASSING, TEMPORARY MULCH SHALL BE USED AS DIRECTED BY THE ENGINEER. REFER TO SECTION 161 OF THE STANDARD SPECIFICATIONS.

14. THE CONTRACTOR SHALL ENSURE THAT POSITIVE AND ADEQUATE DRAINAGE IS MAINTAINED AT ALL TIMES WITHIN THE PROJECT LIMITS. THIS MAY INCLUDE, BUT NOT LIMITED TO, REPLACEMENT OR RECONSTRUCTION OF EXISTING DRAINAGE STRUCTURES THAT HAVE BEEN DAMAGED OR REMOVED OR REGRADING AS REQUIRED BY THE ENGINEER, EXCEPT FOR THOSE DRAINAGE ITEMS SHOWN AT SPECIFIC LOCATIONS IN THE PLANS AND HAVING SPECIFIC PAY ITEMS IN THE DETAILED ESTIMATE. NO SEPARATE PAYMENT WILL BE MADE FOR ANY COSTS INCURRED TO COMPLY WITH THIS REQUIREMENT.
15. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO OR CONCURRENT WITH LAND DISTURBANCE ACTIVITIES AND SHALL BE MAINTAINED AT ALL TIMES. ADDITIONAL EROSION AND SEDIMENT CONTROL DEVICES SHALL BE INSTALLED IF DEEMED NECESSARY BY ON-SITE INSPECTION OR AS DIRECTED BY THE ENGINEER.
16. ALL SILT FENCE MUST BE PLACED AS ACCESS IS OBTAINED DURING CLEARING. NO GRADING SHALL BE DONE UNTIL SILT FENCE INSTALLATION IS COMPLETE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN ALL SILT FENCES AND TO REPAIR OR REPLACE ANY SILT FENCE THAT IS NOT SATISFACTORY. EROSION CONTROL GATES SHALL BE PLACED IMMEDIATELY AFTER DRAINAGE STRUCTURES ARE IN PLACE. ALL EROSION CONTROL DEVICES SHALL BE PLACED ACCORDING TO THE PLANS AND AS DIRECTED BY THE ENGINEER. SEE THE GDOT STANDARD SPECIFICATIONS REGARDING EROSION CONTROL AND THE MANUAL FOR EROSION AND SEDIMENT CONTROL BY G.S.W.C.C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING WETLAND AREAS FREE FROM SILTATION. THE CONTRACTOR SHALL OBTAIN AND ABIDE BY ALL CORPS OR ENGINEERS RULES AND REGULATIONS CONCERNING CONSTRUCTION ADJACENT TO WATERWAYS AND MAINTAIN WATER QUALITY.
17. CONSTRUCTION LAYOUT WILL BE REQUIRED BY THE CONTRACTOR. ALL COST FOR THIS ITEM WILL BE INCLUDED IN THE PRICE BID FOR OTHER CONTRACT ITEMS.
18. ANY ADDITIONAL QUANTITIES ABOVE WHAT IS SHOWN IN THE PLAN SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER.
19. WHEN SURFACE IS ASPHALTIC CEMENT CONCRETE, STRIPING (WHITE AND YELLOW) AND ARROW MARKING SHALL BE APPLIED USING GDOT STANDARDS FOR THERMOPLASTIC STRIPING. WHEN SURFACE IS PORTLAND CEMENT CONCRETE, STRIPING (WHITE AND YELLOW) AND ARROW MARKING SHALL BE APPLIED USING GDOT STANDARDS FOR PREFORMED CONTRAST STRIPING.
20. WHEN NECESSARY, EXISTING STRIPING SHALL BE REMOVED BY GRINDING, UNLESS SPECIFIED BY BROOKHAVEN TRAFFIC ENGINEER.
21. ALL FINAL SIGNAGE MUST BE INSTALLED CONCURRENTLY WITH THE PERFORMANCE OF THE STRIPING WORK.
22. CONTACT THE BROOKHAVEN PUBLIC WORKS (678-382-6700) ONE WEEK PRIOR TO COMMENCEMENT OF ANY STRIPING WORK.



#### REVISION DATES

8/16/2019		

#### GENERAL NOTES BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN

CHECKED:		DATE:		DRAWING No.
BACKCHECKED:		DATE:		
CORRECTED:		DATE:		
VERIFIED:		DATE:		

04-0001

UTILITY OWNER	SERVICE	CONTACT NUMBERS
ATLANTA GAS LIGHT	GAS	404-548-4796
DEKALB COUNTY WATER	WATER	770-621-7264
DEKALB COUNTY - SEWER	SANITARY SEWER	770-621-7264
AT&T	TELEPHONE	706-701-6081
COMCAST COMMUNICATIONS	CABLE TV	404-597-4353
CROWN CASTLE NG NETWORKS, INC.	FIBER OPTICS	404-409-7533
GOOGLE FIBER	FIBER OPTICS	770-324-7693
GEORGIA POWER	ELECTRIC	404-947-0729

NOTES:

UTILITY DISCLAIMER: EXISTING UTILITY LINES SHOWN ARE APPROXIMATE LOCATIONS ONLY. UTILITY LOCATION WAS PERFORMED BY UTILITY OWNER MARK-UPS. THE CONTRACTOR/INSTALLER SHALL FIELD VERIFY ALL EXISTING UTILITY LINE LOCATIONS PRIOR TO ANY CONSTRUCTION.

CONTRACTOR/INSTALLER SHALL CONTACT 811 PRIOR TO ANY CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL UTILITY COORDINATION, ALL COORDINATION WITH ADJACENT PROPERTY OWNERS AND THE REPAIR OF ANY DAMAGED IRRIGATION FACILITIES.



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### GENERAL NOTES - STANDARD SIGNS

- ALL STANDARD HIGHWAY SIGNS SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH THE DETAILS SHOWN IN THE PLANS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, AND THE GEORGIA SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND/OR SPECIAL PROVISIONS.
- SIGN ERECTION STATIONS ARE APPROXIMATE AND MAY BE ADJUSTED TO MEET FIELD CONDITIONS WHERE NECESSARY, BUT SHALL BE WITHIN THE LIMITATIONS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION. NO SIGN LOCATION SHALL BE CHANGED BY THE CONTRACTOR OR BY THE PROJECT ENGINEER WITHOUT PRIOR APPROVAL FROM THE CITY OF BROOKHAVEN TRAFFIC ENGINEER.
- ALL STANDARD HIGHWAY SIGNS SHALL BE ERECTED AT A HEIGHT OF 7 FEET ABOVE THE NORMAL EDGE OF PAVEMENT TO THE BOTTOM OF THE SIGN OR ASSEMBLY.
- 4b. HORIZONTAL CLEARANCE FOR STANDARD HIGHWAY SIGNS ON ALL OTHER ROADWAYS SHALL BE 6 FEET FROM THE EDGE OF THE PAVED SHOULDER OR 12 FEET FROM THE NORMAL EDGE OF PAVEMENT TO THE NEARER EDGE OF THE SIGN(S), WHICHEVER IS GREATER. THE HORIZONTAL CLEARANCE IN NON-MOUNTABLE CURB SECTIONS SHALL BE AT LEAST 2 FEET FROM THE CURB FACE TO THE NEARER EDGE OF THE SIGN(S).
- 4c. HORIZONTAL CLEARANCE FOR STANDARD HIGHWAY SIGNS MOUNTED BEHIND GUARD RAIL SHALL BE 6 FEET FROM THE FACE OF THE GUARD RAIL TO THE NEARER EDGE OF THE SIGN(S).
- SINGLE PLATE, HORIZONTAL RECTANGULAR SIGNS OVER 48 INCHES IN WIDTH SHALL BE MOUNTED ON TWO POSTS WITH 2 EACH 2 INCH x 1/2 INCH x (WIDTH OF SIGN) ALUMINUM OR GALVANIZED STEEL STRAPS. THE STRAPS SHALL BE FLUSH WITH THE BACK OF THE SIGN WITH ONE EACH ACROSS THE TOP AND BOTTOM OF THE SIGN. THE CENTERLINE OF EACH POST SHALL BE INSET 1/6TH OF THE SIGN WIDTH FROM THE EDGE OF THE SIGN. SIGN PLATE BOLT HOLES SHALL BE 3/8 INCH DIAMETER, DRILLED OR PUNCHED, AS SHOWN ON THE SIGN PLATE DETAILS.
- EACH 42 OR 48 INCH WIDE x 18 OR 24 INCH HIGH SIGN REQUIRES ONE 2 INCH x 1/2 INCH x (WIDTH OF SIGN) ALUMINUM OR GALVANIZED STEEL STRAP LOCATED IN THE CENTER OF THE SIGN AND FLUSH WITH THE BACK OF THE SIGN.
- SIGN ASSEMBLIES SHALL BE MOUNTED ON ALUMINUM OR GALVANIZED STEEL STRAP FRAMES. FOR DETAILS AND STRAP SPECIFICATIONS REFER TO SIGN ASSEMBLY-TYPICAL FRAMING DETAILS.
- TYPE 9 (VERY HIGH INTENSITY) REFLECTIVE SHEETING SHALL BE USED FOR ALL STANDARD HIGHWAY SIGNS REQUIRING REFLECTORIZED BACKGROUNDS EXCEPT AS SPECIFIED BELOW OR SPECIFIED OTHERWISE IN THE PLANS. EITHER CLASS 1 OR CLASS 2 ADHESIVE BACKING IS PERMISSIBLE.
- TYPE 11 (VERY HIGH INTENSITY) REFLECTIVE SHEETING SHALL BE USED FOR ALL RED SERIES SIGNS (R1-1, R1-2, R1-3P, R5-1, R5-1A, R5-1B).
- TYPE 11 (VERY HIGH INTENSITY) REFLECTIVE SHEETING SHALL BE USED FOR ALL WARNING SIGNS
- TYPE 11 (VERY HIGH INTENSITY) FLUORESCENT YELLOW GREEN REFLECTIVE SHEETING SHALL BE USED FOR SCHOOL ZONE (S1-1, S2-1, S3-1, S4-3, AND THE TOP PORTION OF THE S5-1) SIGNS. ALL REGULATORY SIGNS WITHIN THE SCHOOL ZONE SHALL HAVE TYPE 9 (VERY HIGH INTENSITY) REFLECTIVE SHEETING.
- A 1/2 INCH MINIMUM AIR SPACE SHALL BE REQUIRED BETWEEN ALL SIGN PLATES WITHIN AN ASSEMBLY.
- WHERE SIGNS WITHIN AN ASSEMBLY EXTEND BELOW THE STANDARD MOUNTING HOLES ON THE POST(S), ADDITIONAL 3/8 INCH DIAMETER HOLE(S), DRILLED OR PUNCHED, SHALL BE REQUIRED TO PROPERLY MOUNT THE ASSEMBLY.
- FOR DETAILS OF SPECIAL DESIGN HIGHWAY SIGNS, SEE DETAILS OF MISCELLANEOUS SIGNS.
- REFER TO PLAN SHEETS FOR LOCATION OF THE DISTRICT ENGINEERS OFFICE TO BE SHOWN ON ALL R552-1 (LIMITED ACCESS) SIGNS IN THIS PROJECT, IF ANY.
- THE CONTRACTOR WILL, AS REQUESTED BY THE CITY OF BROOKHAVEN TRAFFIC ENGINEER, BE REQUIRED TO REMOVE ANY EXISTING SIGNS THAT ARE DUPLICATED OR ARE CONTRARY TO THESE SIGN PLANS.

REVISION DATES	
8/16/2019	

### GENERAL NOTES BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	04-0002
CORRECTED:	DATE:	
VERIFIED:	DATE:	



# National Flood Hazard Layer FIRMette



### Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) <i>Zone A, V, A99</i>
		With BFE or Depth <i>Zone AE, AO, AH, VE, AR</i>
		Regulatory Floodway

OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile <i>Zone X</i>
		Future Conditions 1% Annual Chance Flood Hazard <i>Zone X</i>
		Area with Reduced Flood Risk due to Levee. See Notes. <i>Zone X</i>
		Area with Flood Risk due to Levee <i>Zone D</i>

OTHER AREAS		Area of Minimal Flood Hazard <i>Zone X</i>
		Effective LOMRs
OTHER AREAS		Area of Undetermined Flood Hazard <i>Zone D</i>

GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall

OTHER FEATURES		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary

OTHER FEATURES		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature

MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 8/15/2019 at 3:54:28 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.



REVISION DATES	
8/16/2019	

### GENERAL NOTES BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	04-0003
CORRECTED:	DATE:	
VERIFIED:	DATE:	

### ALLOWABLE RANGES TABLE

FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:

#### A. NORMAL CROWN

SECTION WITH GRADES 0.5% OR GREATER	SECTION WITH GRADES LESS THAN 0.5%
0.0150 FT/FT - MINIMUM	0.0156 FT/FT - MINIMUM
0.0208 FT/FT - DESIRABLE	0.0208 FT/FT - DESIRABLE
0.0250 FT/FT - MAXIMUM	0.0300 FT/FT - MAXIMUM

#### B. SUPERELEVATION RATE

S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.

#### C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)

RATE OF CHANGE	CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT
MINIMUM 1:150	0.67%
DESIRABLE 1:200	0.50%
MAXIMUM 1:300	0.33%

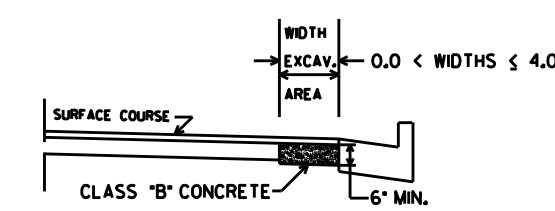
LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.

#### D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES

50% OF TRANSITION INSIDE CURVE - MAXIMUM  
33% OF TRANSITION INSIDE CURVE - DESIRABLE  
20% OF TRANSITION INSIDE CURVE - MINIMUM

NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.

#### E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (IN FEET) EQUAL TO THE SPEED DESIGN (IN MPH).



NO SCALE CLASS "B" CONCRETE BASE OR PAVEMENT WIDENING Item Code 500-9999 - CU.Yds.

In excavated areas between the existing paving and new curb and gutter that are 4'-0" or less in width, Class "B" concrete shall be placed in lieu of the base and paving specified by the typical section. Payment will be made under "Class B Concrete Base and Pavement Widening".  
In excavated areas greater than 4'-0" in width, the Contractor shall place base and paving as specified on the typical section.  
See plans for details of curb and gutter construction.

CLASS "B" CONCRETE BASE OR WIDENING DETAIL

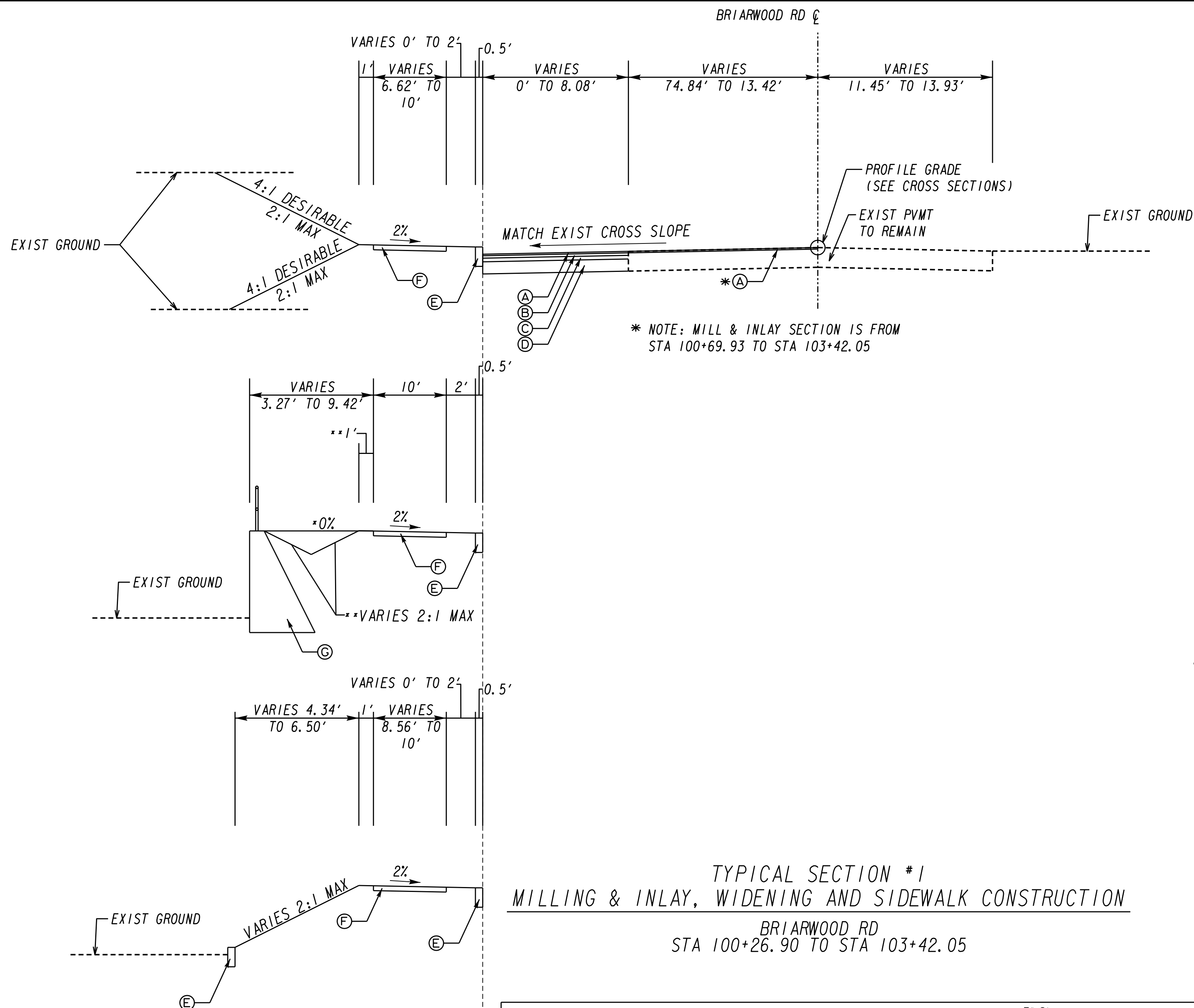
#### LEGEND

- (A) RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE 11, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LBS/SY)
- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LBS/SY)
- (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LBS/SY)
- (D) GRADED AGGREGATE BASE, 10 INCH, INCL MATL
- (E) 6" CONC HEADER CURB, TP 2, SEE GA CONST STD 9032B FOR DETAILS
- (F) 4" CONCRETE SIDEWALK
- (G) GRAVITY WALL W/ MOUNTED HANDRAIL, SEE GA CONST STD 9031L & BROOKHAVEN STREETScape DETAIL ST-03
- (H) SIDEWALK TURN DOWN WALL - SEE DETAIL ON DRWG 5-0002
- (I) SIDEWALK TURN UP WALL - SEE DETAIL ON DRWG 5-0003
- (J) 24" X 6" CONC CURB & GUTTER, TP 2, SEE GA CONST STD 9032B FOR DETAILS

STA 100+29.60 TO STA 100+53.01  
STA 100+74.09 TO STA 100+89.14  
STA 101+05.34 TO STA 101+50.00

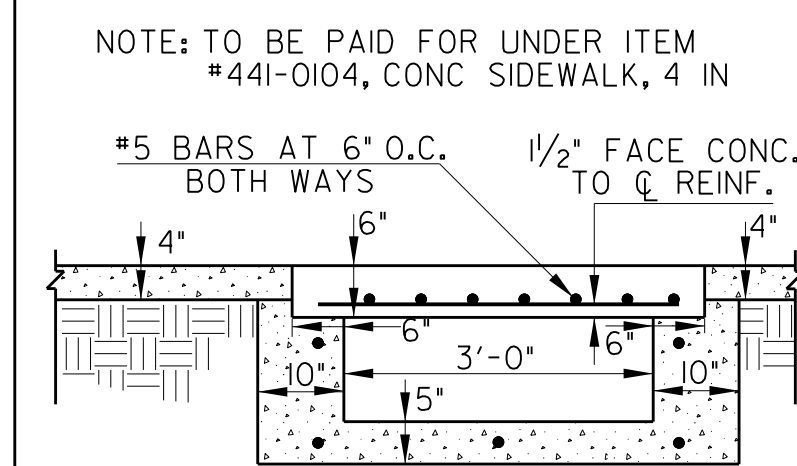
\*\*STA 101+50.00 TO STA 102+00.00  
\*STA 102+00.00 TO STA 103+00.00  
\*STA 103+00.00 TO STA 103+42.05

STA 100+53.01 TO STA 100+74.09  
STA 100+89.14 TO STA 101+05.34



### TYPICAL SECTION #1 MILLING & INLAY, WIDENING AND SIDEWALK CONSTRUCTION

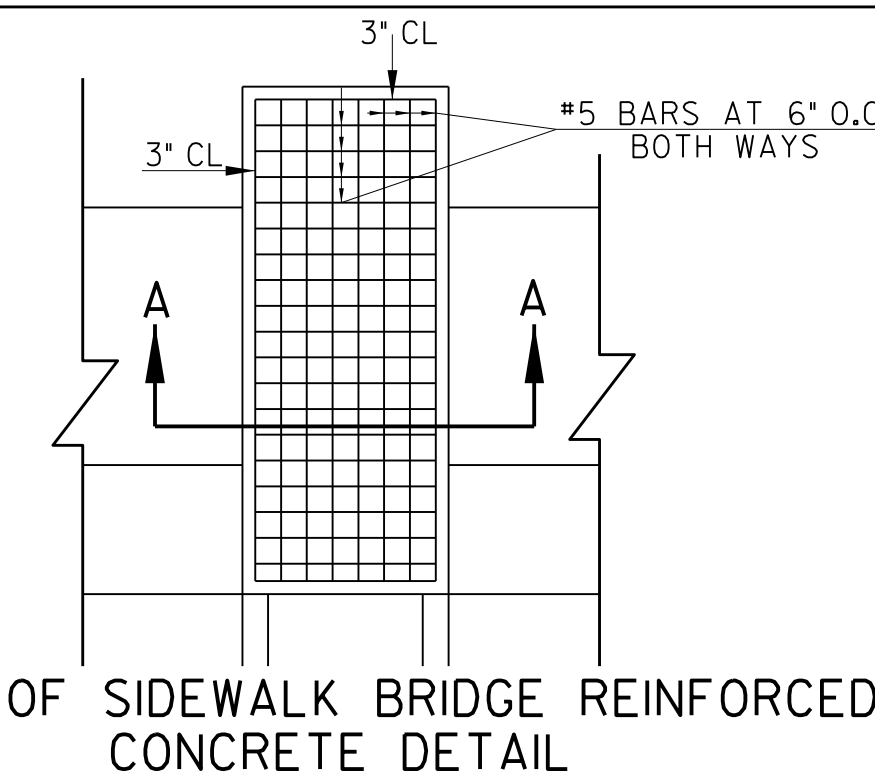
BRIARWOOD RD  
STA 100+26.90 TO STA 103+42.05



SECTION A-A

#### DETAIL A

BRIARWOOD RD  
STA 100+53.01 TO STA 100+74.09



DETAIL OF SIDEWALK BRIDGE REINFORCED CONCRETE DETAIL

**INFRASTRUCTURE**  
CONSULTING & ENGINEERING

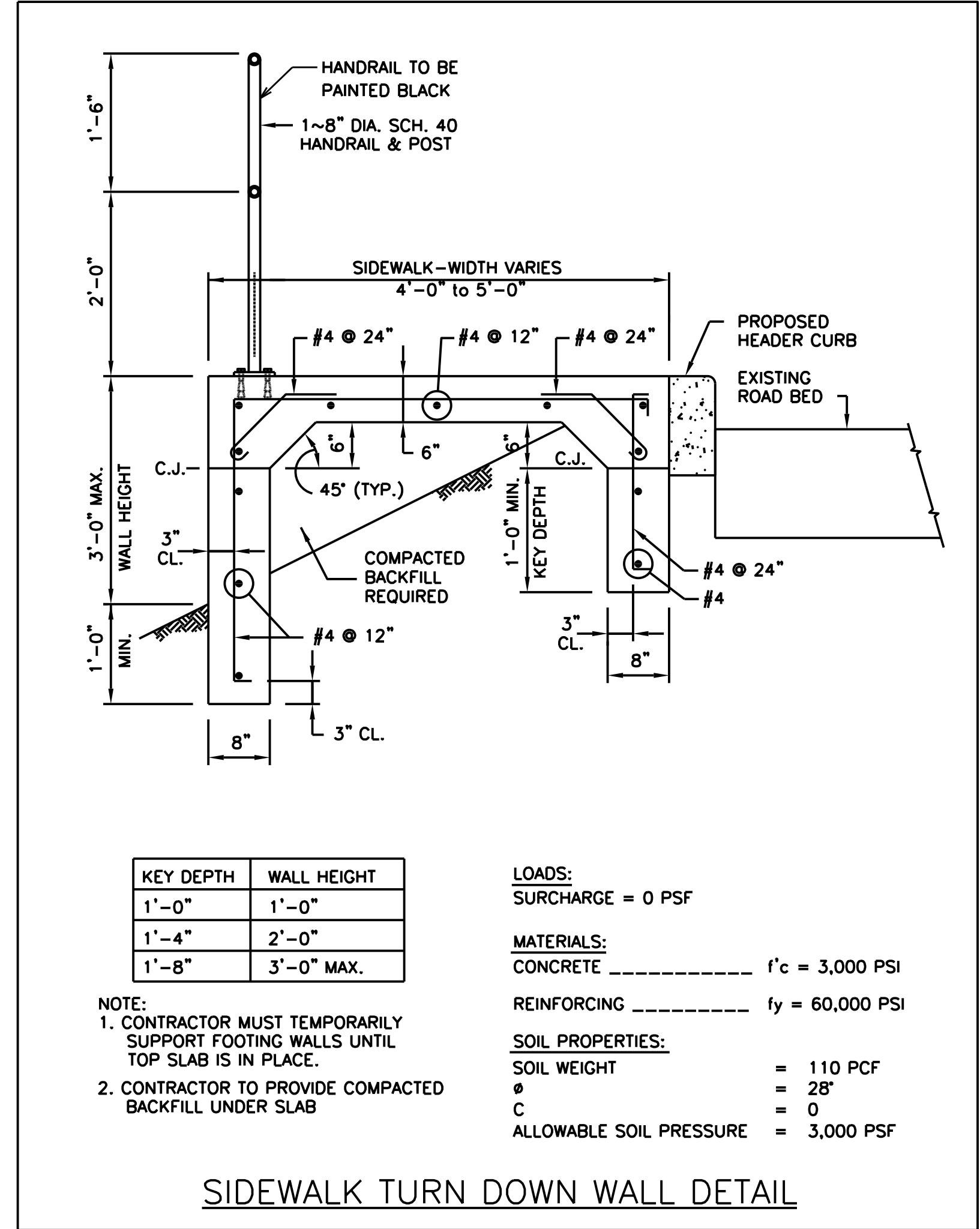
NOT TO SCALE

#### REVISION DATES

8/16/2019

#### TYPICAL SECTIONS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	



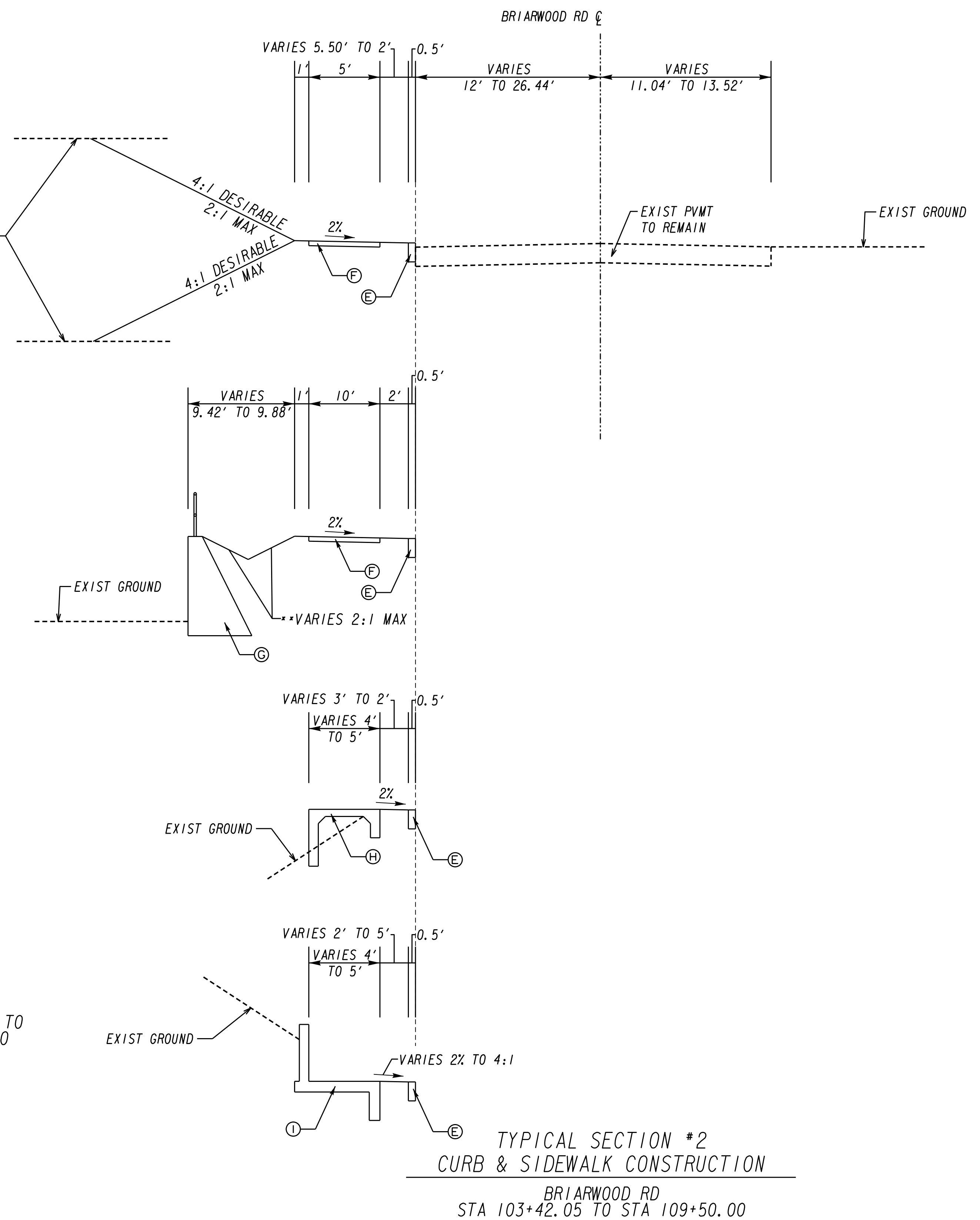
SIDEWALK TURN DOWN WALL DETAIL

STA 103+50.00 TO STA 105+35.00  
STA 108+82.50 TO STA 109+50.00

STA 103+42.05 TO STA 103+50.00

STA 108+40.00 TO STA 108+82.50

STA 105+35.00 TO STA 108+40.00



- LEGEND**
- (A) RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE 11, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LBS/SY)
  - (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LBS/SY)
  - (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LBS/SY)
  - (D) GRADED AGGREGATE BASE, 10 INCH, INCL MATL
  - (E) 6" CONC HEADER CURB, TP 2, SEE GA CONST STD 9032B FOR DETAILS
  - (F) 4" CONCRETE SIDEWALK
  - (G) GRAVITY WALL W/ MOUNTED HANDRAIL, SEE GA CONST STD 9031L & BROOKHAVEN STREETScape DETAIL ST-03
  - (H) SIDEWALK TURN DOWN WALL - SEE DETAIL ON DRWG 5-0002
  - (I) SIDEWALK TURN UP WALL - SEE DETAIL ON DRWG 5-0003
  - (J) 24" X 6" CONC CURB & GUTTER, TP 2, SEE GA CONST STD 9032B FOR DETAILS

**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING

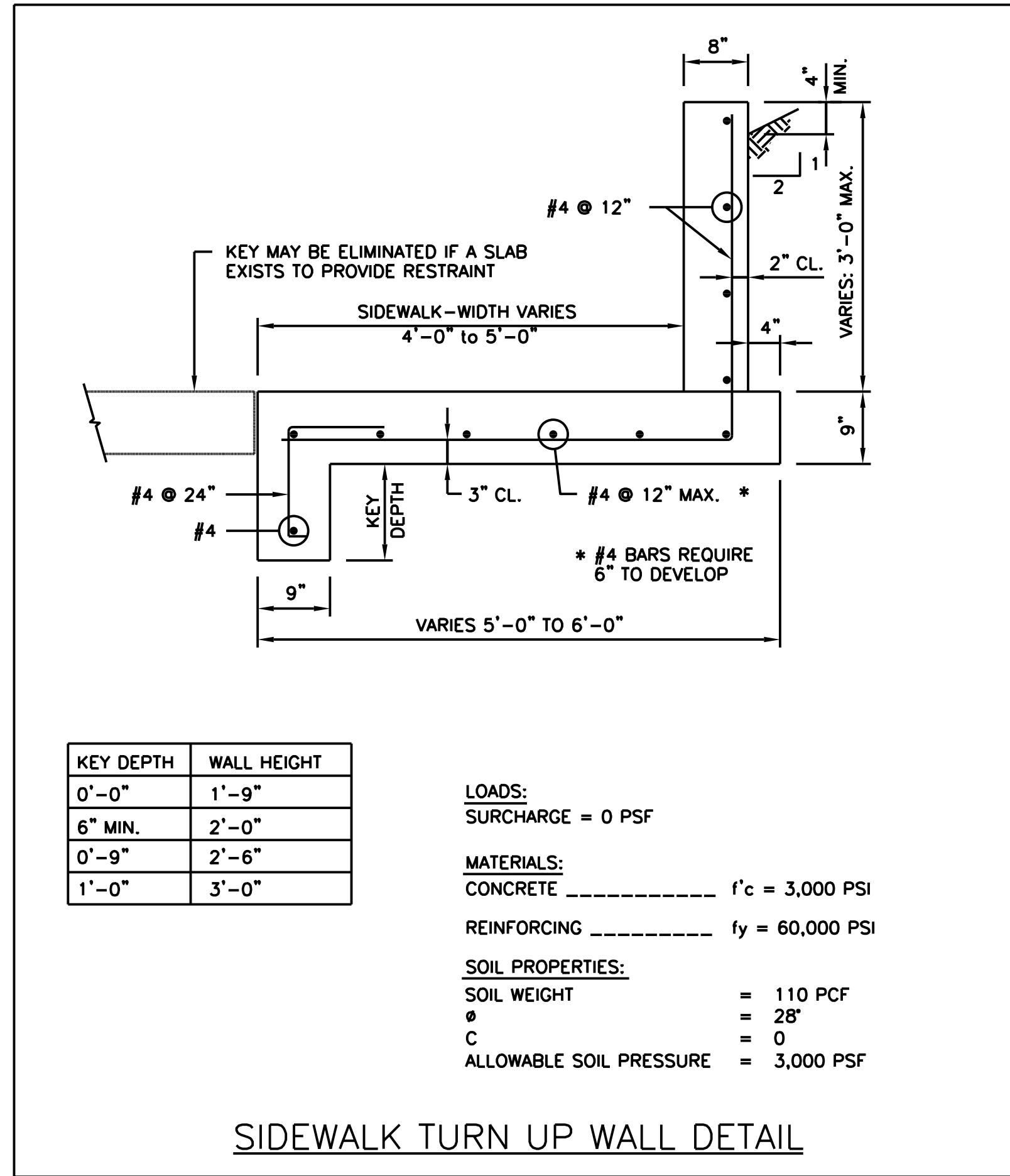
NOT TO SCALE

REVISION DATES	
8/16/2019	

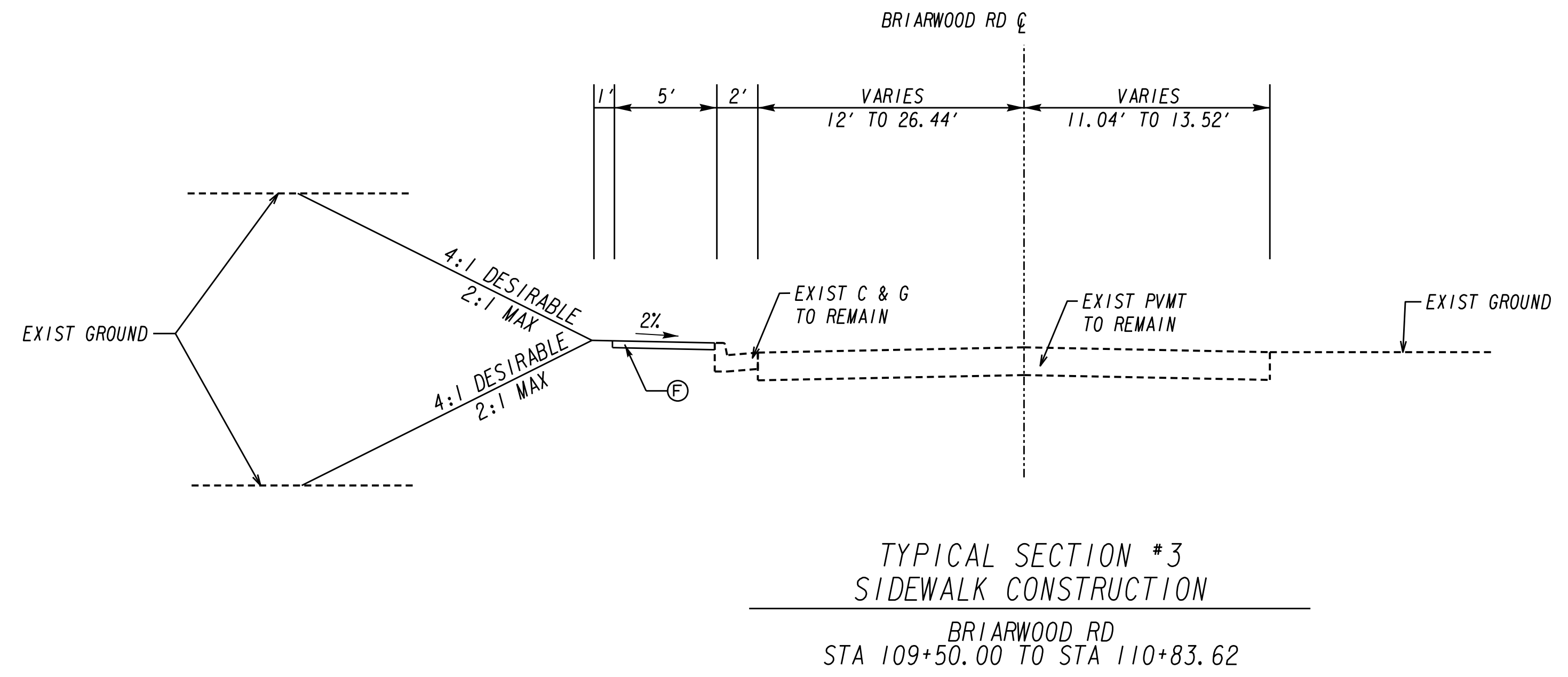
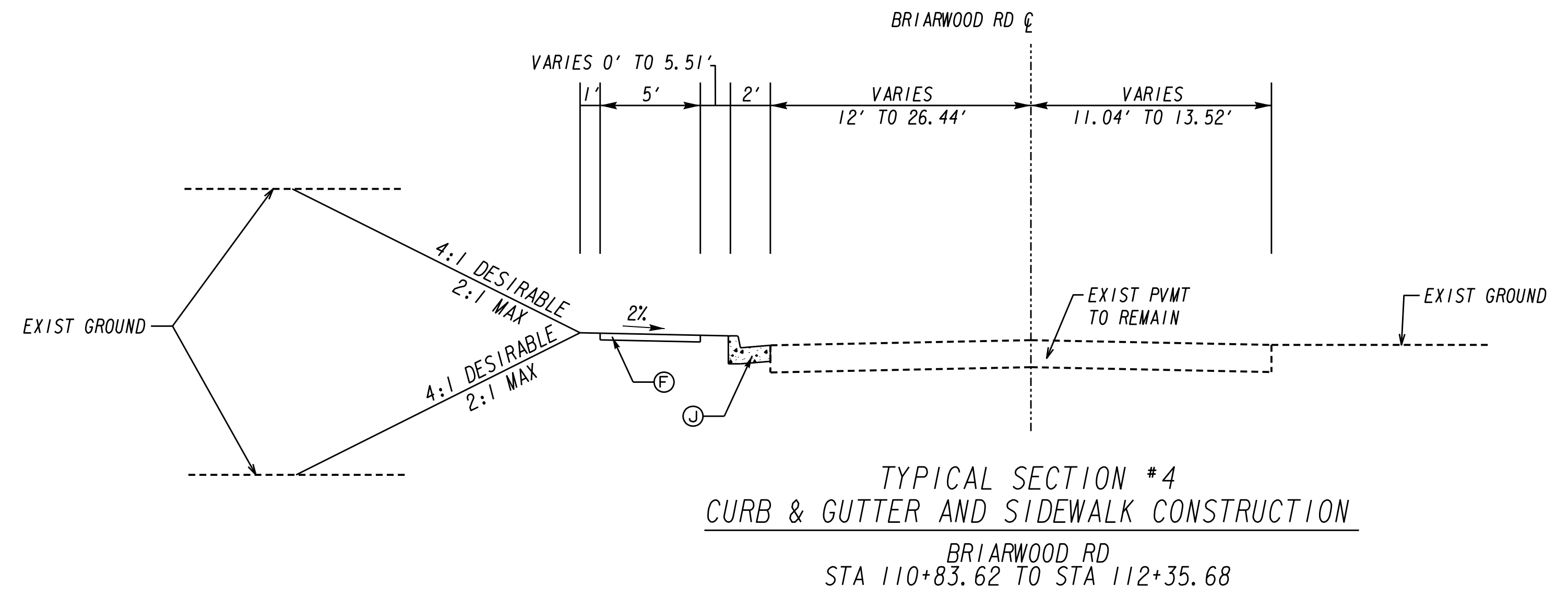
**TYPICAL SECTIONS**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-0002
CORRECTED:	DATE:	
VERIFIED:	DATE:	





STA 109+50.00 TO STA 110+83.62



- LEGEND**
- (A) RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE 11, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LBS/SY)
  - (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LBS/SY)
  - (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LBS/SY)
  - (D) GRADED AGGREGATE BASE, 10 INCH, INCL MATL
  - (E) 6" CONC HEADER CURB, TP 2, SEE GA CONST STD 9032B FOR DETAILS
  - (F) 4" CONCRETE SIDEWALK
  - (G) GRAVITY WALL W/ MOUNTED HANDRAIL, SEE GA CONST STD 9031L & BROOKHAVEN STREETScape DETAIL ST-03
  - (H) SIDEWALK TURN DOWN WALL - SEE DETAIL ON DRWG 5-0002
  - (I) SIDEWALK TURN UP WALL - SEE DETAIL ON DRWG 5-0003
  - (J) 24" X 6" CONC CURB & GUTTER, TP 2, SEE GA CONST STD 9032B FOR DETAILS

**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING

NOT TO SCALE

REVISION DATES	
8/16/2019	

**TYPICAL SECTIONS**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-0003
CORRECTED:	DATE:	
VERIFIED:	DATE:	

# SUMMARY OF QUANTITIES

## MISCELLANEOUS ROADWAY

ITEM	UNIT	TOTAL
GRADING COMPLETE - WALK 116-10	LS	1
TRAFFIC CONTROL - WALK 116-10	LS	1
6" CONC HEADER CURB, TP 2	LF	940
24" X 6" CONC CURB & GUTTER, TP 2	LF	80
CONCRETE SIDEWALK, 4 IN	SY	820
*CONCRETE SIDEWALK, 8 IN	SY	63
CONCRETE VALLEY GUTTER, 6"	SY	90
SIDEWALK TURN DOWN WALL (SEE DRWG 5-0002)	LF	43
SIDEWALK TURN UP WALL (SEE DRWG 5-0003)	LF	260
CLASS B CONCRETE, RETAINING WALL	CY	130
HANDRAIL SPECIAL DESIGN (SEE DRWGS 41-0002, -0002A & -0002B)	LF	200
RELOCATE WATER METER	EA	3
CONC SPILLWAY, SPCL DES	EA	1
STN DUMPED RIP RAP, TP 1, 18 IN	SY	12
PLASTIC FILTER FABRIC	SY	12
REMOVE AND RESET SIGN	EA	3
REMOVE METAL GATE - STA 100+96 LT	EA	1
RIGHT-OF-WAY MARKERS	EA	7
REMOVE & RECONSTRUCT EXIST MAILBOXES	EA	4
LOOP DETECTOR, 6 FT X 40 FT, QUADRUPOLE	EA	2

\*ALL SIDEWALK CONCRETE WITHIN RADIUS RETURNS TO BE 8 IN THICK.

## TEMPORARY EROSION CONTROL

ITEM	UNIT	TOTAL
TEMPORARY GRASSING	AC	0.20
MULCH	TN	2
CONSTRUCTION EXIT	EA	2
MAINTENANCE OF CONSTRUCTION EXIT	EA	2
TEMPORARY SILT FENCE TYPE A	LF	1100
MAINTENANCE OF TEMPORARY SILT FENCE TYPE A	LF	550
*CONSTRUCT AND REMOVE CURB INLET PROTECTION	EA	1
*MAINTENANCE OF CURB INLET PROTECTION	EA	1

\*CONSTRUCT AND REMOVE CURB INLET PROTECTION TO BE PAID FOR UNDER PAY ITEM 163-0550.  
MAINTENANCE OF CURB INLET PROTECTION TO BE PAID FOR UNDER PAY ITEM 165-0105.

## PERMANENT EROSION CONTROL

ITEM	UNIT	TOTAL
PERMANENT GRASSING	AC	0.20
AGRICULTURAL LIME	TN	1
FERTILIZER MIXED GRADE	TN	0.20
FERTILIZER NITROGEN CONTENT	LBS	10
SOD	SY	300
EROSION CONTROL MATS, SLOPES	SY	150

## SURFACING QUANTITIES

ITEM	UNIT	TOTAL
RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE 11, GP 2 ONLY, INCL BITUM MATL & H LIME	TN	60
RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME	TN	18
RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME	TN	36
GRADED AGGREGATE BASE COURSE, INCL MATL	TN	100
BITUMINOUS TACK COAT	GL	12
DRIVEWAY CONCRETE, 4 IN TK	SY	80
CLASS B CONC, BASE OR PAVEMENT WIDENING	CY	3
*AGGREGATE SURFACE COURSE	TN	100

\*FOR USE IN INCLEMENT WEATHER TO FACILITATE THE MOVEMENT OF LOCAL TRAFFIC ALONG ROADWAY CONSTRUCTION AND TO PERMIT INGRESS & EGRESS AT DRIVES.

## PAVEMENT MARKING

ITEM	UNIT	TOTAL
THERMOPLASTIC PVMT MARKING, ARROW, TP 2	EA	3
THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, WHITE	LF	1100
THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, YELLOW	LF	560
THERMOPLASTIC SOLID TRAF STRIPE, 24 IN, WHITE	LF	28
THERMOPLASTIC SOLID TRAF STRIPE, 8 IN, WHITE	LF	140
THERMOPLASTIC SKIP TRAF STRIPE, 5 IN, WHITE	GLF	100
THERMOPLASTIC TRAF STRIPING, WHITE	SY	40
RAISED PVMT MARKERS, TP 1	EA	14
RAISED PVMT MARKERS, TP 3	EA	7
PREFORMED PLASTIC SOLID PVMT MKG, 8 IN, CONTRAST (BLACK-WHITE), TP PB	LF	170

## SIGN QUANTITIES

STATION	INSTL. NO.	HIGHWAY SIGNS						SQUARE TUBE POST			
		SIGN CODE	TP1 MATL, REFL SHEETING TP 9			TP1 MATL, REFL SHEETING TP 3			TYPE 7		
			SIZE	QUANTITY	SQ FEET	SIZE	QUANTITY	SQ FEET	LENGTH (FEET)	QUANTITY	TOTAL LENGTH
100+50	1	R1-2	36X36X36	1	3.90				13	1	13.0
101+16	2	R560-5				24X18	1	3.00	12	1	12.0
<b>TOTAL</b>				<b>1</b>	<b>3.90</b>		<b>1</b>	<b>3</b>	<b>25.0</b>	<b>2</b>	<b>25.0</b>

**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING

### REVISION DATES

8/16/2019

**SUMMARY QUANTITIES  
CARTECAY DRIVE SIDEWALK  
CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	06-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	





ITEM NO.	DESCRIPTION	UNITS	QUANTITY
<b>ROADWAY</b>			
150-1000	TRAFFIC CONTROL - WALK 16-118	LS	1
210-0100	GRADING COMPLETE - WALK 16-118	LS	1
310-1101	GR AGGR BASE CRS, INCL MATL	TN	100
318-3000	AGGR SURF CRS	TN	100
402-3103	RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE II, GP 2 ONLY, INCL BITUM MATL & H LIME	TN	60
402-3121	RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME	TN	36
402-3190	RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME	TN	18
413-0750	BITUM TACK COAT	GL	12
441-0014	DRIVEWAY CONCRETE, 4 IN TK	SY	80
441-0104	CONC SIDEWALK, 4 IN	SY	820
441-0108	CONC SIDEWALK, 8 IN	SY	63
441-0300	CONC SPILLWAY, SPCL DES	EA	1
441-4020	CONC VALLEY GUTTER, 6 IN	SY	90
441-5002	CONCRETE HEADER CURB, 6 IN, TP 2	LF	940
441-6012	CONC CURB & GUTTER, 6 IN X 24 IN, TP 2	LF	80
500-3201	CLASS B CONCRETE, RETAINING WALL	CY	130
500-9999	CLASS B CONC, BASE OR PVMT WIDENING	CY	3
515-2050	HANDRAIL SPECIAL DESIGN	LF	200
603-2018	STN DUMPED RIP RAP, TP 1, 18 IN	SY	12
603-7000	PLASTIC FILTER FABRIC	SY	12
610-0306	REM METAL GATE - STA 100+96 LT	EA	1
634-1200	RIGHT OF WAY MARKERS	EA	7
999-0001	SIDEWALK TURN-DOWN WALL (SEE DRWG 5-0002)	LF	43
999-0002	SIDEWALK TURN-UP WALL (SEE DRWG 5-0003)	LF	260
999-0003	REMOVE AND RECONSTRUCT EXIST MAILBOXES	EA	4
<b>PERMANENT EROSION CONTROL</b>			
700-6910	PERMANENT GRASSING	AC	0.20
700-7000	AGRICULTURAL LIME	TN	1
700-8000	FERTILIZER MIXED GRADE	TN	0.20
700-8100	FERTILIZER NITROGEN CONTENT	LBS	10
700-9300	SOD	SY	300
716-2000	EROSION CONTROL MATS, SLOPES	SY	150
<b>TEMPORARY EROSION CONTROL</b>			
163-0232	TEMPORARY GRASSING	AC	0.20
163-0240	MULCH	TN	2
163-0300	CONSTRUCTION EXIT	EA	2
163-0550	CONSTRUCT AND REMOVE INLET SEDIMENT TRAP	EA	1
165-0010	MAINTENANCE OF TEMPORARY SILT FENCE, TP A	LF	550
165-0101	MAINTENANCE OF CONSTRUCTION EXIT	EA	2
165-0105	MAINTENANCE OF INLET SEDIMENT TRAP	EA	1
171-0010	TEMPORARY SILT FENCE, TP A	LF	1100
<b>SIGNING AND MARKING</b>			
611-5551	RESET SIGN	EA	3
636-1020	HIGHWAY SIGNS, TP 1 MATL, REFL SHEETING, TP 3	SF	3
636-1033	HIGHWAY SIGNS, TP 1 MATL, REFL SHEETING, TP 9	SF	4
636-2070	GALV STEEL POSTS, TP 7	LF	25
653-0120	THERMOPLASTIC PVMT MARKING, ARROW, TP 2	EA	3
653-1501	THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, WHITE	LF	1100
653-1502	THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, YELLOW	LF	560
653-1704	THERMOPLASTIC SOLID TRAF STRIPE, 24 IN, WHITE	LF	28
653-1804	THERMOPLASTIC SOLID TRAF STRIPE, 8 IN, WHITE	LF	140
653-3501	THERMOPLASTIC SKIP TRAF STRIPE, 5 IN, WHITE	GLF	100
653-6004	THERMOPLASTIC TRAF STRIPING, WHITE	SY	40
654-1001	RAISED PVMT MARKERS TP 1	EA	14
654-1003	RAISED PVMT MARKERS TP 3	EA	7
657-1085	PREFORMED PLASTIC SOLID PVMT MKG, 8 IN, CONTRAST (BLACK-WHITE), TP PB	LF	170
<b>UTILITY</b>			
670-9730	RELOCATED EXIST WATER METER, INCL BOX	EA	3
647-6300	LOOP DETECTOR, 6 FT X 40 FT, QUADRUPOLE	EA	2



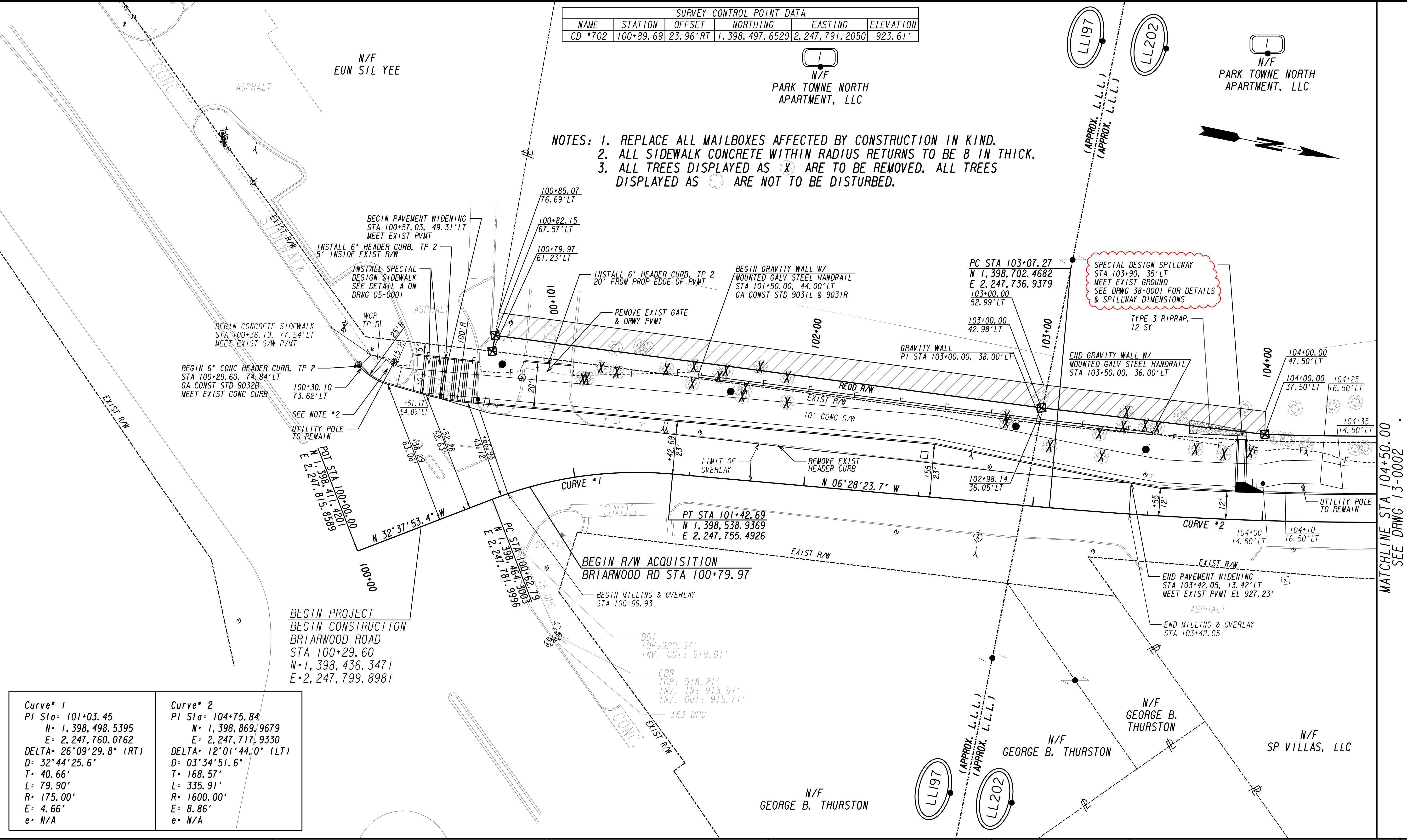
REVISION DATES	
8/16/2019	

**DETAILED ESTIMATE  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	09-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

SURVEY CONTROL POINT DATA					
NAME	STATION	OFFSET	NORTHING	EASTING	ELEVATION
CD *702	100+89.69	23.96'RT	1,398,497.6520	2,247,791.2050	923.61'

NOTES: 1. REPLACE ALL MAILBOXES AFFECTED BY CONSTRUCTION IN KIND.  
 2. ALL SIDEWALK CONCRETE WITHIN RADIUS RETURNS TO BE 8 IN THICK.  
 3. ALL TREES DISPLAYED AS X ARE TO BE REMOVED. ALL TREES DISPLAYED AS O ARE NOT TO BE DISTURBED.

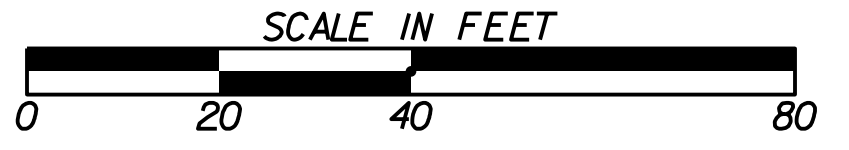


Curve* 1	Curve* 2
PI Sta= 101+03.45	PI Sta= 104+75.84
N= 1,398,498.5395	N= 1,398,869.9679
E= 2,247,760.0762	E= 2,247,717.9330
DELTA= 26°09'29.8" (RT)	DELTA= 12°01'44.0" (LT)
D= 32°44'25.6"	D= 03°34'51.6"
T= 40.66'	T= 168.57'
L= 79.90'	L= 335.91'
R= 175.00'	R= 1600.00'
E= 4.66'	E= 8.86'
e= N/A	e= N/A

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES

BEGIN LIMIT OF ACCESS.....BLA  
 END LIMIT OF ACCESS.....ELA  
 LIMIT OF ACCESS  
 REQ'D R/W & LIMIT OF ACCESS  
 ORANGE BARRIER FENCE  
 ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)

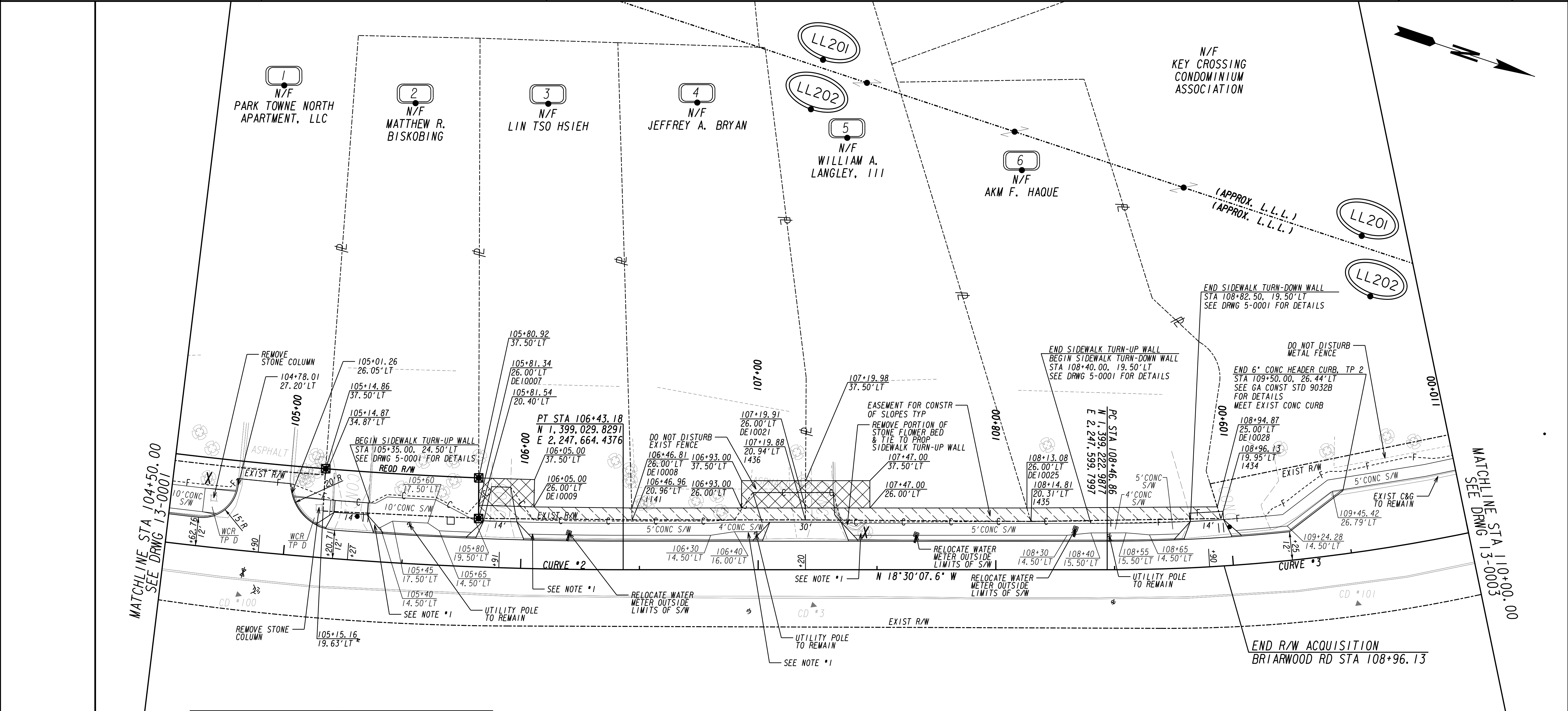
**INFRASTRUCTURE**  
 CONSULTING & ENGINEERING



REVISION DATES	
8/16/2019	

**CONSTRUCTION PLAN**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**  
**STA 100+36.07 TO STA 104+50.00**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	



<p><b>Curve* 2</b>                  PI Sta: 104+75.84                  N= 1,398,869.9679                  E= 2,247,717.9330                  DELTA= 12°01'44.0" (LT)                  D= 03°34'51.6"                  T= 168.57'                  L= 335.91'                  R= 1600.00'                  E= 8.86'                  e= N/A</p>	<p><b>Curve* 3</b>                  PI Sta: 110+01.87                  N= 1,399,369.9780                  E= 2,247,550.6114                  DELTA= 23°21'13.6" (LT)                  D= 07°38'22.0"                  T= 155.00'                  L= 305.70'                  R= 750.00'                  E= 15.85'                  e= N/A</p>
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- NOTES: 1. REPLACE ALL MAILBOXES AFFECTED BY CONSTRUCTION IN KIND.  
 2. ALL SIDEWALK CONCRETE WITHIN RADIUS RETURNS TO BE 8 IN THICK.  
 3. ALL TREES DISPLAYED AS (X) ARE TO BE REMOVED. ALL TREES DISPLAYED AS (O) ARE NOT TO BE DISTURBED.

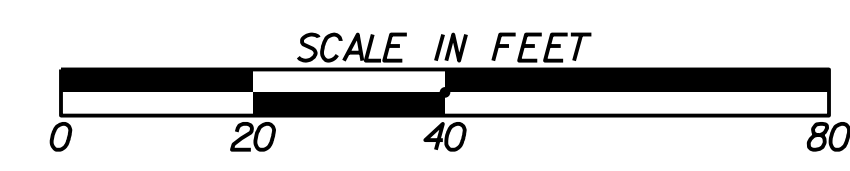
N/F SP VILLAS, LLC

NAME	STATION	OFFSET	NORTHING	EASTING	ELEVATION
CD *100	104+83.18	18.77'RT	1,398,879.9650	2,247,725.8510	931.18'
CD *3	107+23.62	15.13'RT	1,399,110.9140	2,247,653.2550	945.37'
CD *101	109+50.25	21.80'RT	1,399,328.1580	2,247,579.8760	965.00'

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES

BEGIN LIMIT OF ACCESS.....BLA  
 END LIMIT OF ACCESS.....ELA  
 LIMIT OF ACCESS  
 REQ'D R/W & LIMIT OF ACCESS  
 ORANGE BARRIER FENCE  
 ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)

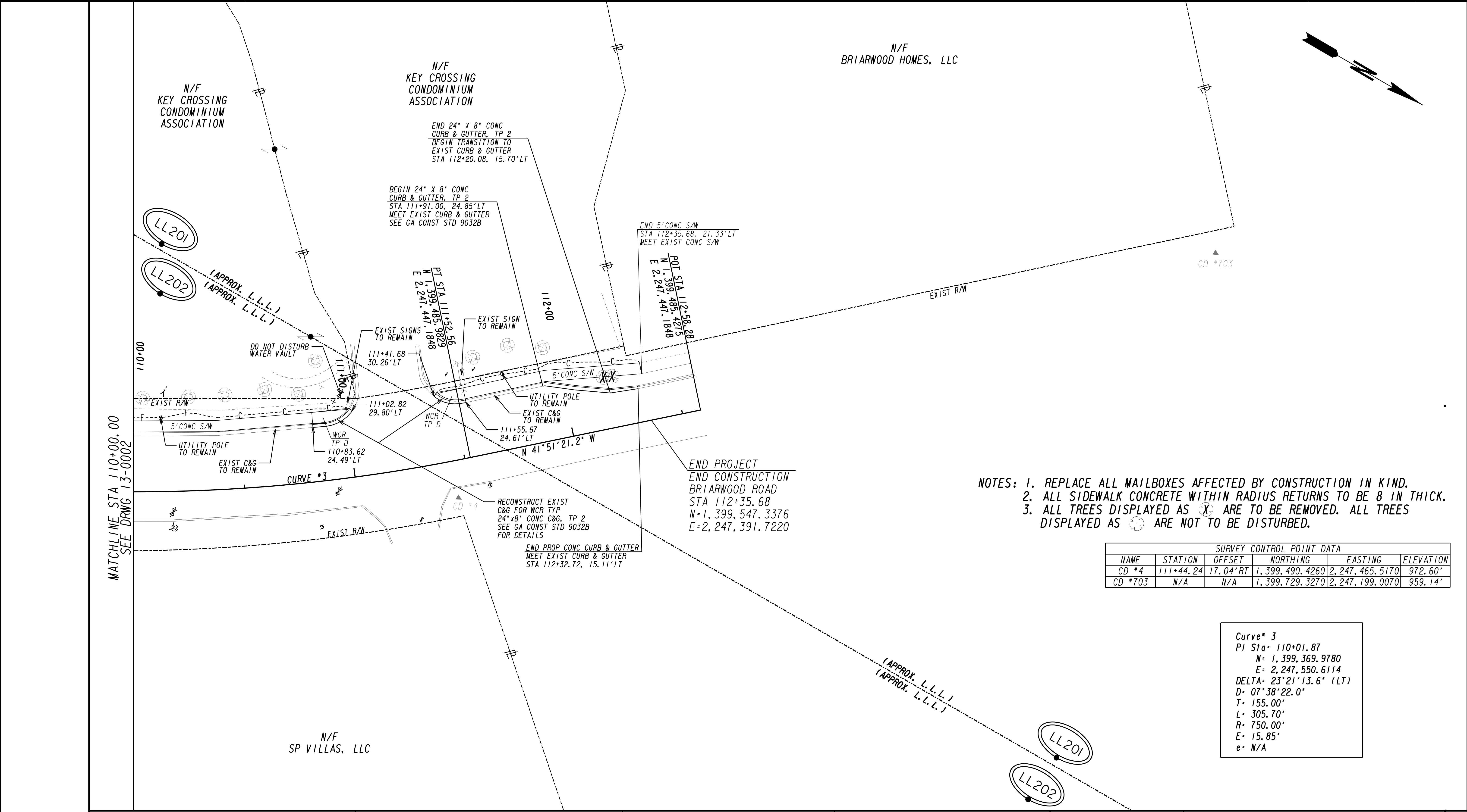
**INFRASTRUCTURE**  
 CONSULTING & ENGINEERING



NO.	DATE	DESCRIPTION

**CONSTRUCTION PLAN**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**  
**STA 104+50.00 TO STA 110+00.00**

CHECKED:	DATE:	DRAWING No. <b>13-0002</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	

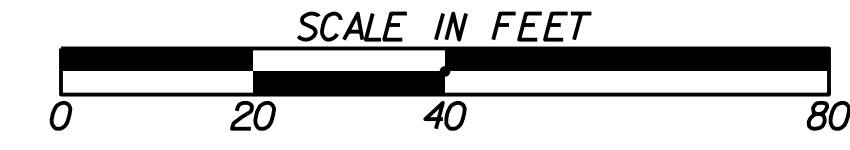
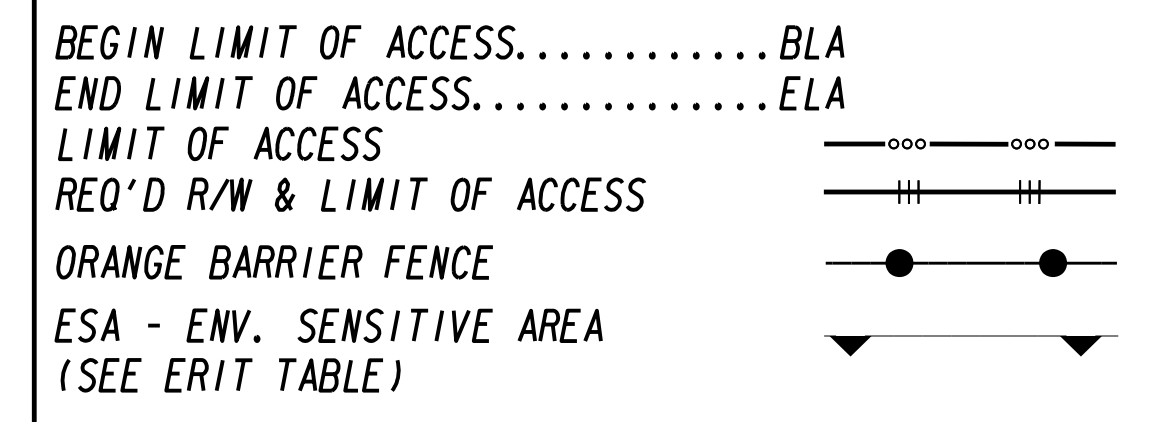


- NOTES: 1. REPLACE ALL MAILBOXES AFFECTED BY CONSTRUCTION IN KIND.  
 2. ALL SIDEWALK CONCRETE WITHIN RADIUS RETURNS TO BE 8 IN THICK.  
 3. ALL TREES DISPLAYED AS (X) ARE TO BE REMOVED. ALL TREES DISPLAYED AS (T) ARE NOT TO BE DISTURBED.

SURVEY CONTROL POINT DATA					
NAME	STATION	OFFSET	NORTHING	EASTING	ELEVATION
CD *4	111+44.24	17.04'RT	1,399,490.4260	2,247,465.5170	972.60'
CD *703	N/A	N/A	1,399,729.3270	2,247,199.0070	959.14'

Curve\* 3  
 PI Sta= 110+01.87  
 N= 1,399,369.9780  
 E= 2,247,550.6114  
 DELTA= 23°21'13.6" (LT)  
 D= 07°38'22.0"  
 T= 155.00'  
 L= 305.70'  
 R= 750.00'  
 E= 15.85'  
 e= N/A

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR  
 & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES

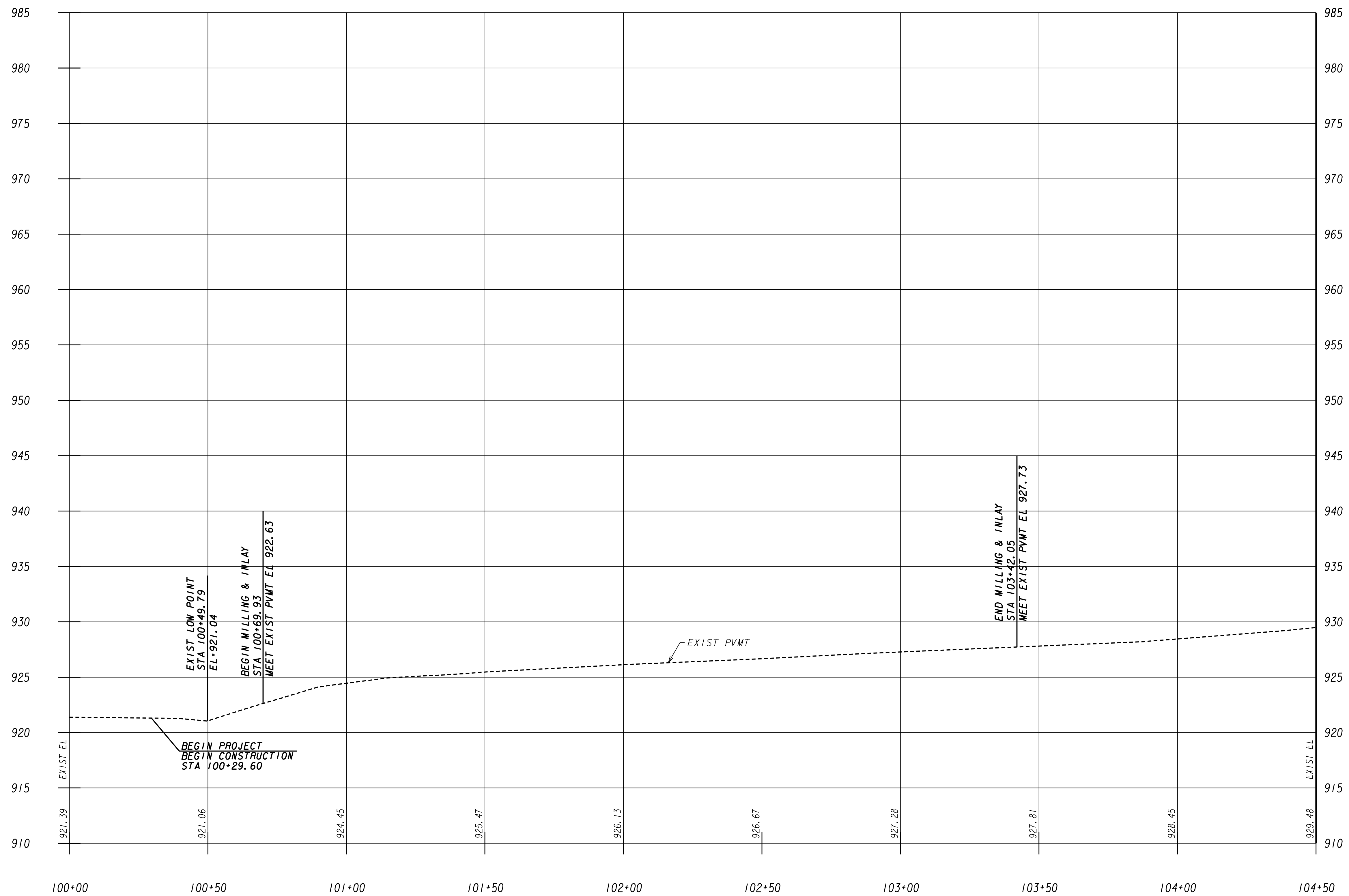


REVISION DATES	

**CONSTRUCTION PLAN  
 BRIARWOOD ROAD SIDEWALK  
 CITY OF BROOKHAVEN  
 STA 110+00.00 TO STA 112+35.68**

CHECKED:	DATE:	DRAWING No. <b>13-0003</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	





MATCHLINE STA 104+50.00  
SEE DRAWING 15-0002



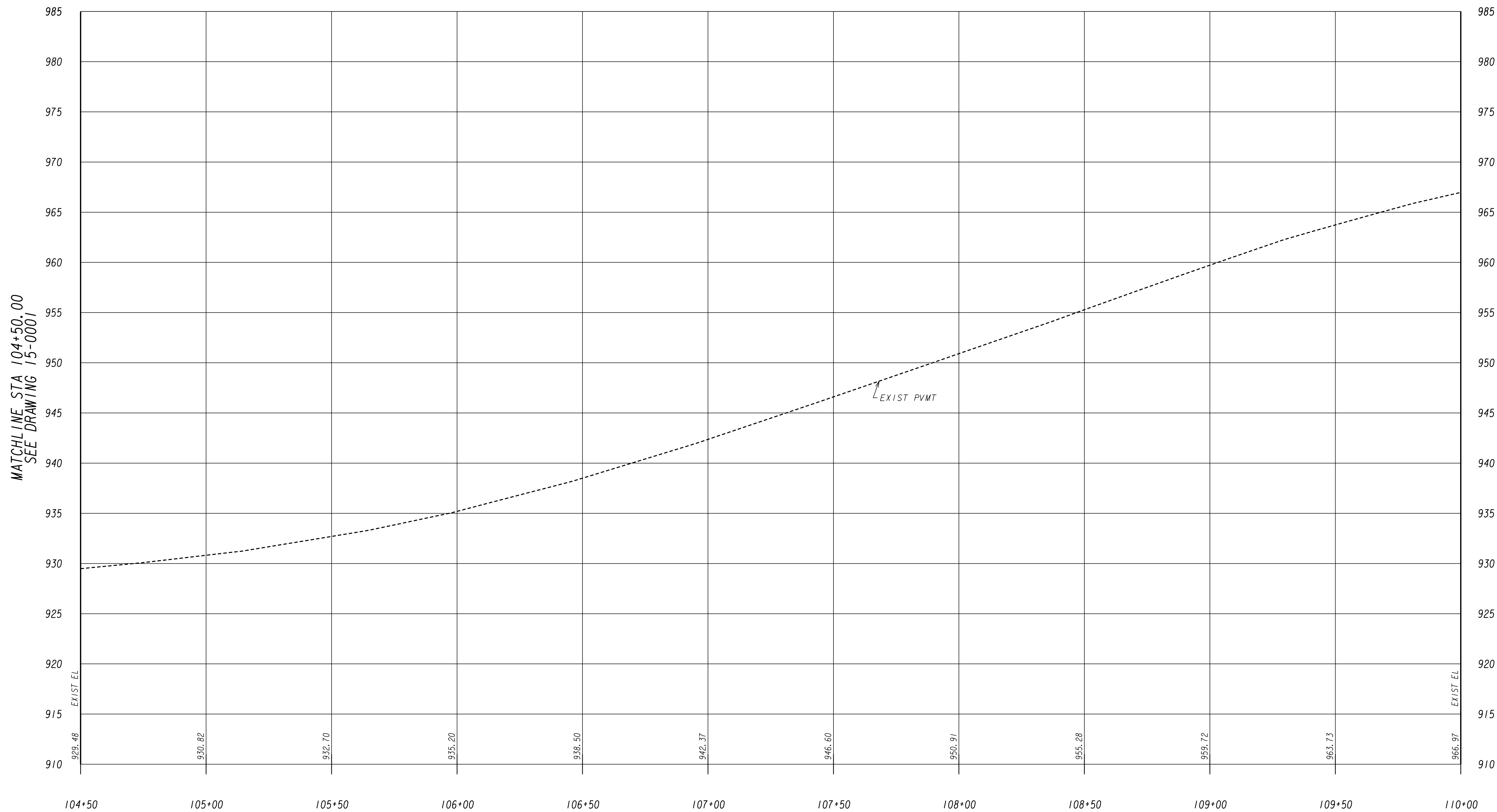
HORIZONTAL 1" = 20'  
VERTICAL 1" = 5'

REVISION DATES

NO.	DATE	DESCRIPTION

**MAINLINE PROFILE  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 100+36.07 TO STA 104+50**

CHECKED:	DATE:	DRAWING No. <b>15-0001</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	



HORIZONTAL 1" = 20'  
VERTICAL 1" = 5'

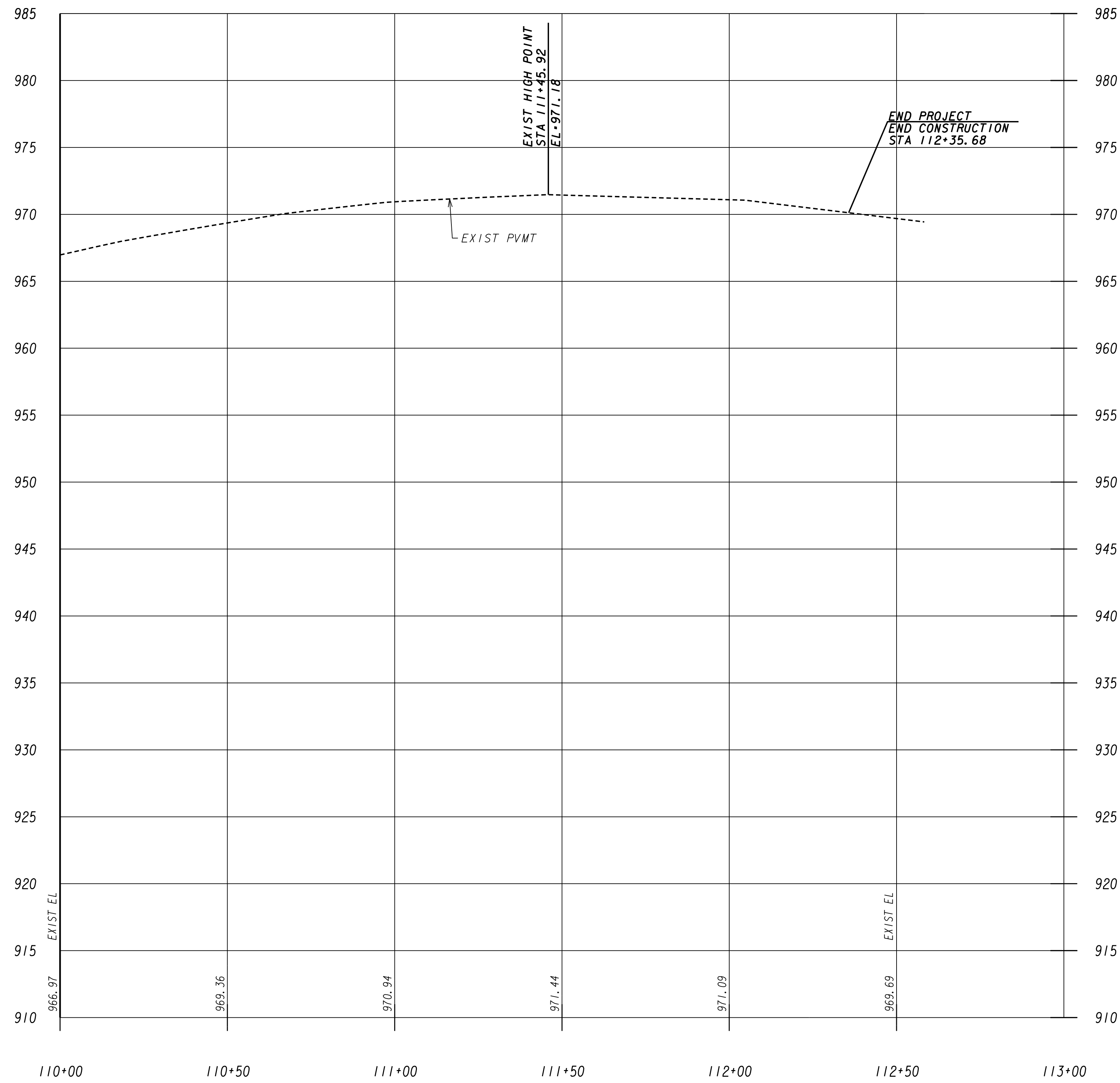
REVISION DATES

NO.	DATE	DESCRIPTION

**MAINLINE PROFILE  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 104+50 TO STA 110+00**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	15-0002
CORRECTED:	DATE:	
VERIFIED:	DATE:	

MATCHLINE STA 110+00.00  
SEE DRAWING 15-0002



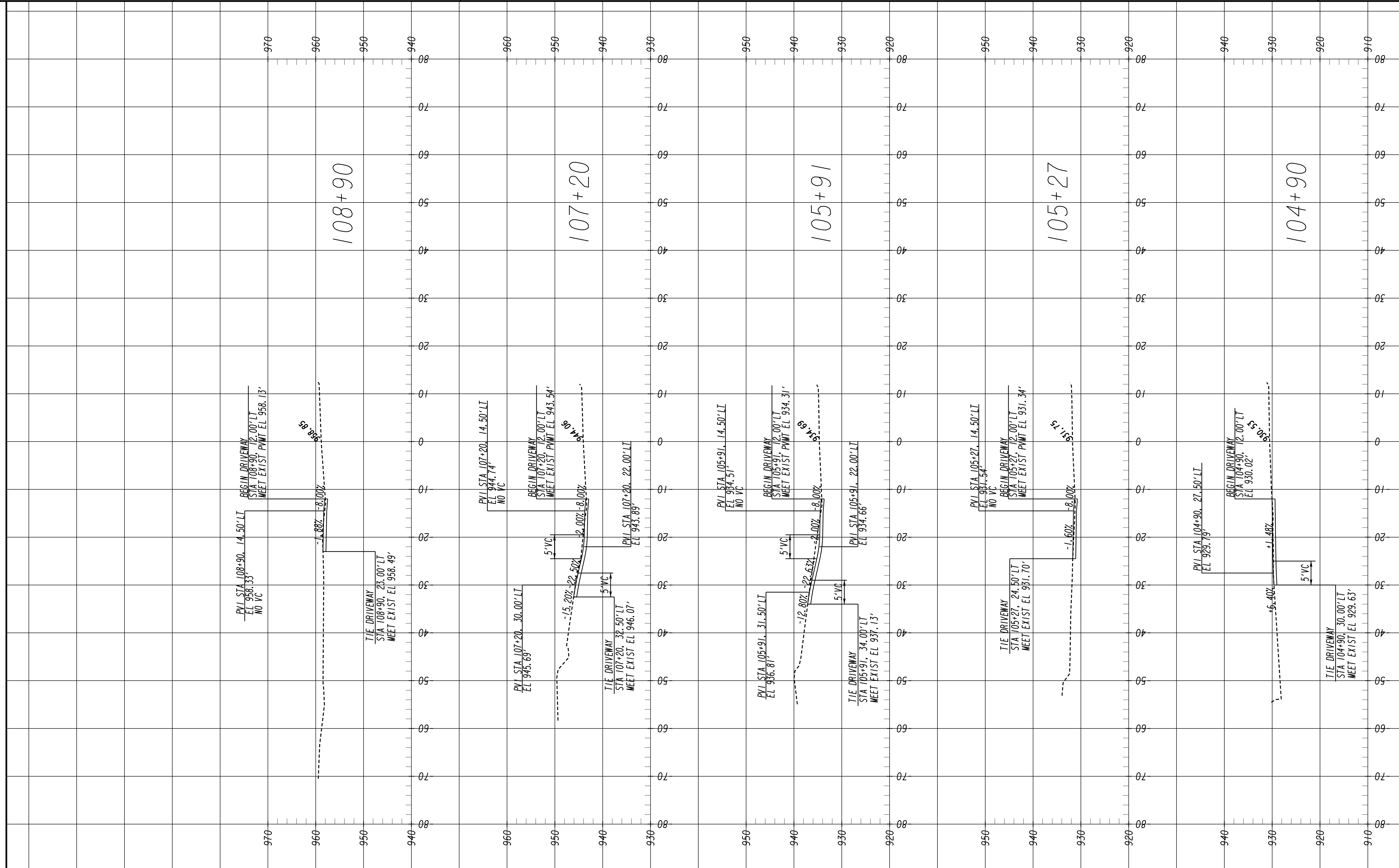
HORIZONTAL 1" = 20'  
VERTICAL 1" = 5'

REVISION DATES

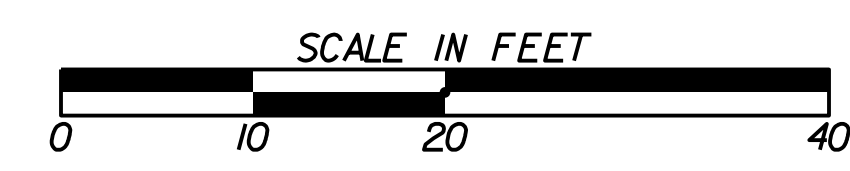
NO.	DATE	DESCRIPTION

**MAINLINE PROFILE  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 110+00 TO STA 112+35.68**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	15-0003
CORRECTED:	DATE:	
VERIFIED:	DATE:	



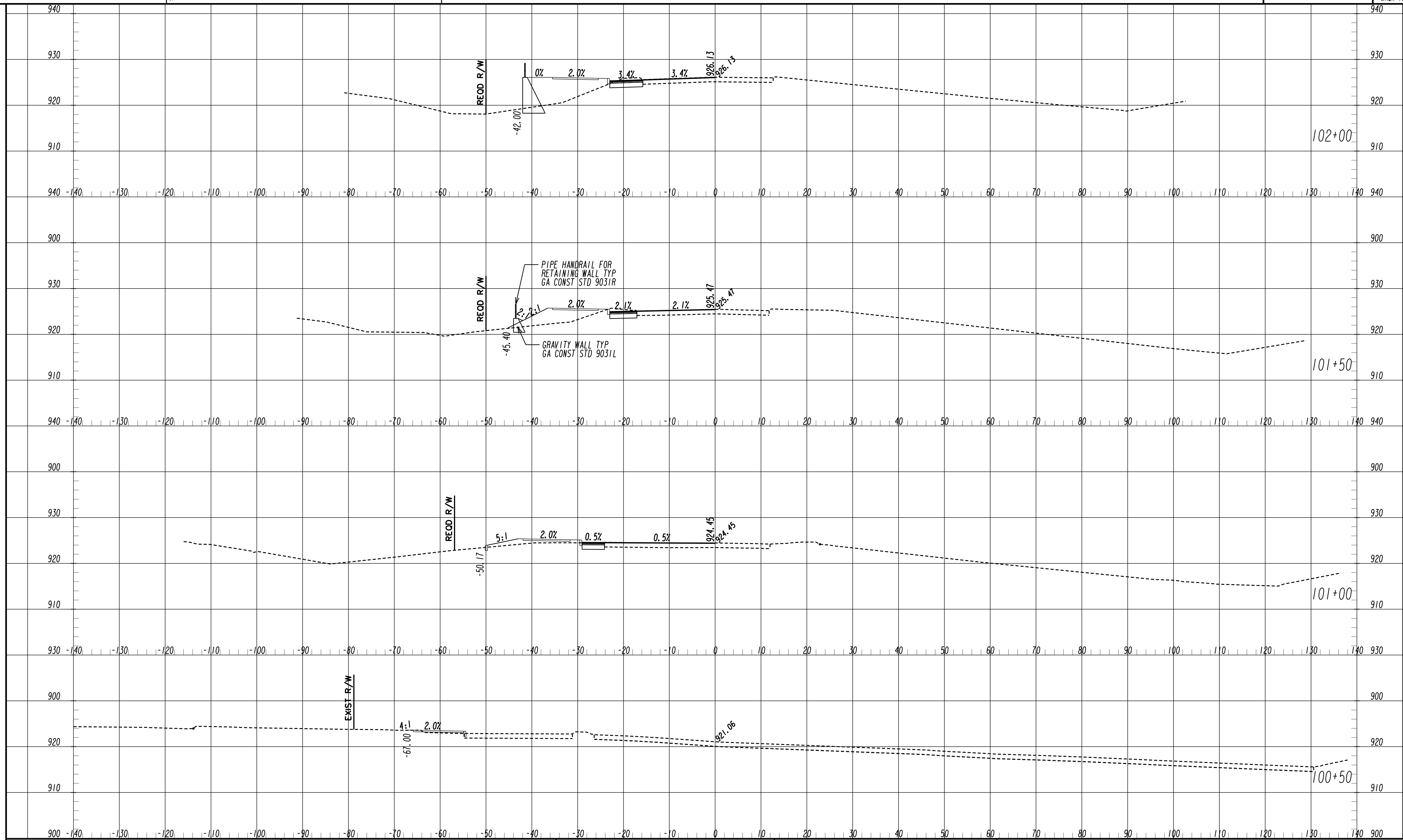
**INFRASTRUCTURE**  
CONSULTING & ENGINEERING



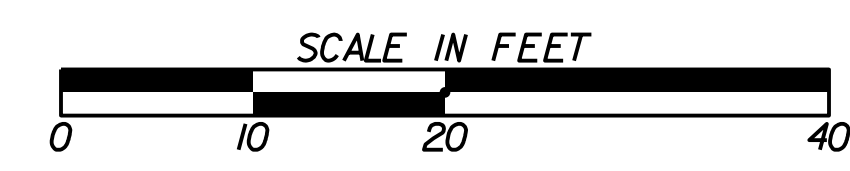
REVISION DATES

**DRIVEWAY PROFILE**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No. <b>17-0001</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	

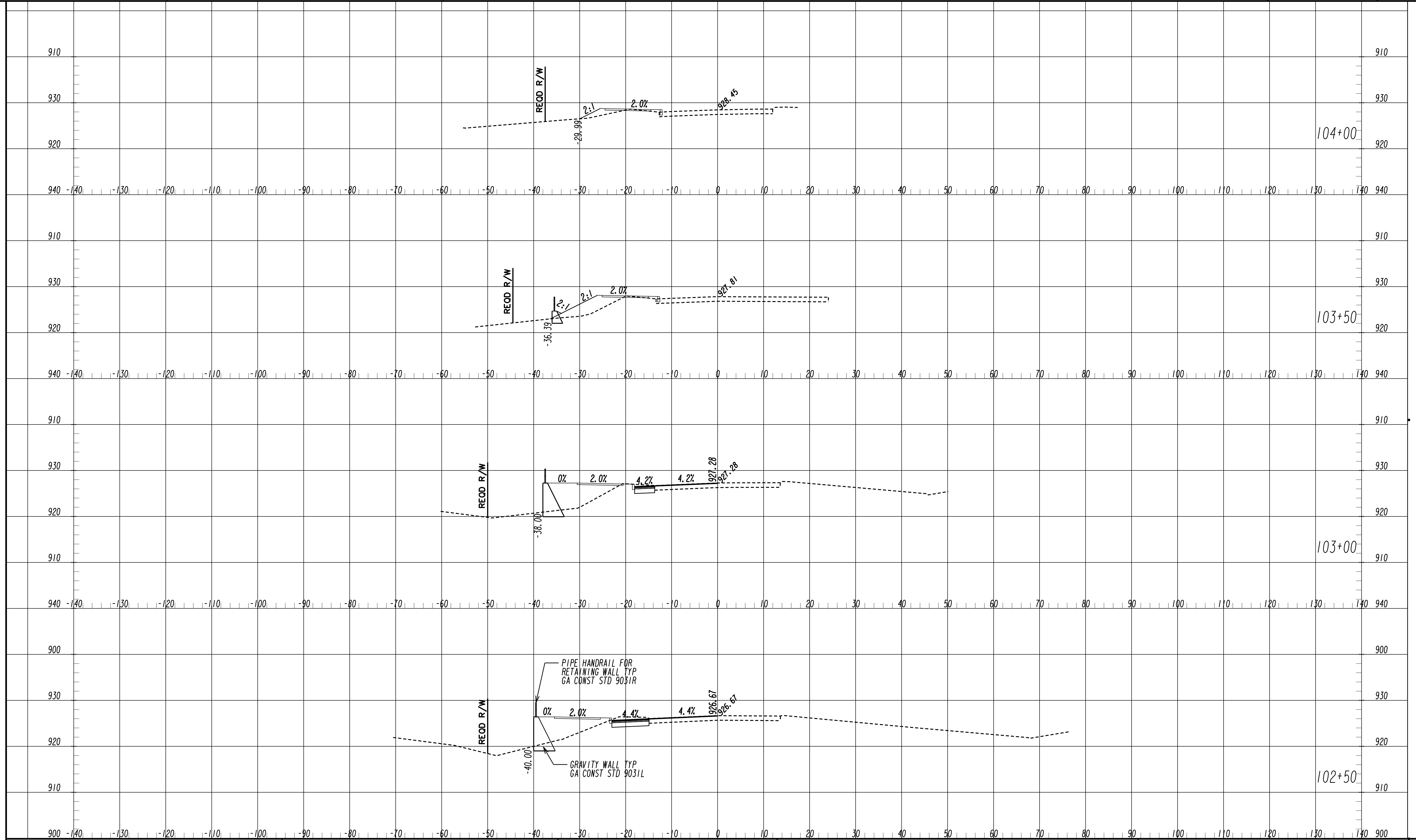


**INFRASTRUCTURE**  
CONSULTING & ENGINEERING

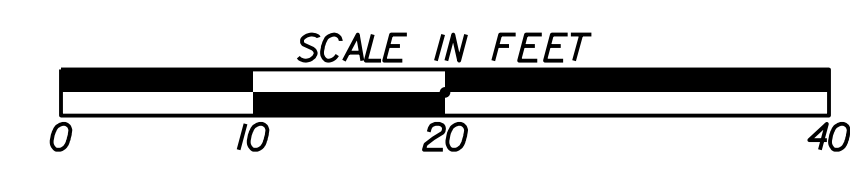


REVISION DATES

EARTHWORK CROSS SECTIONS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN STA 100+50 TO STA 102+00		CHECKED:	DATE:	DRAWING No.
		BACKCHECKED:	DATE:	23-0001
		CORRECTED:	DATE:	
		VERIFIED:	DATE:	



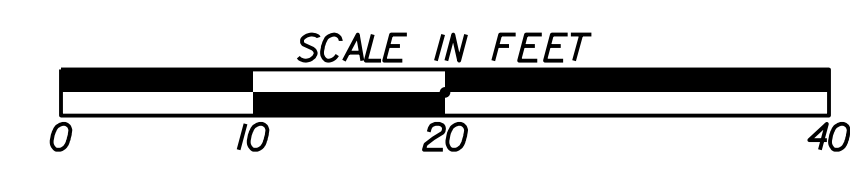
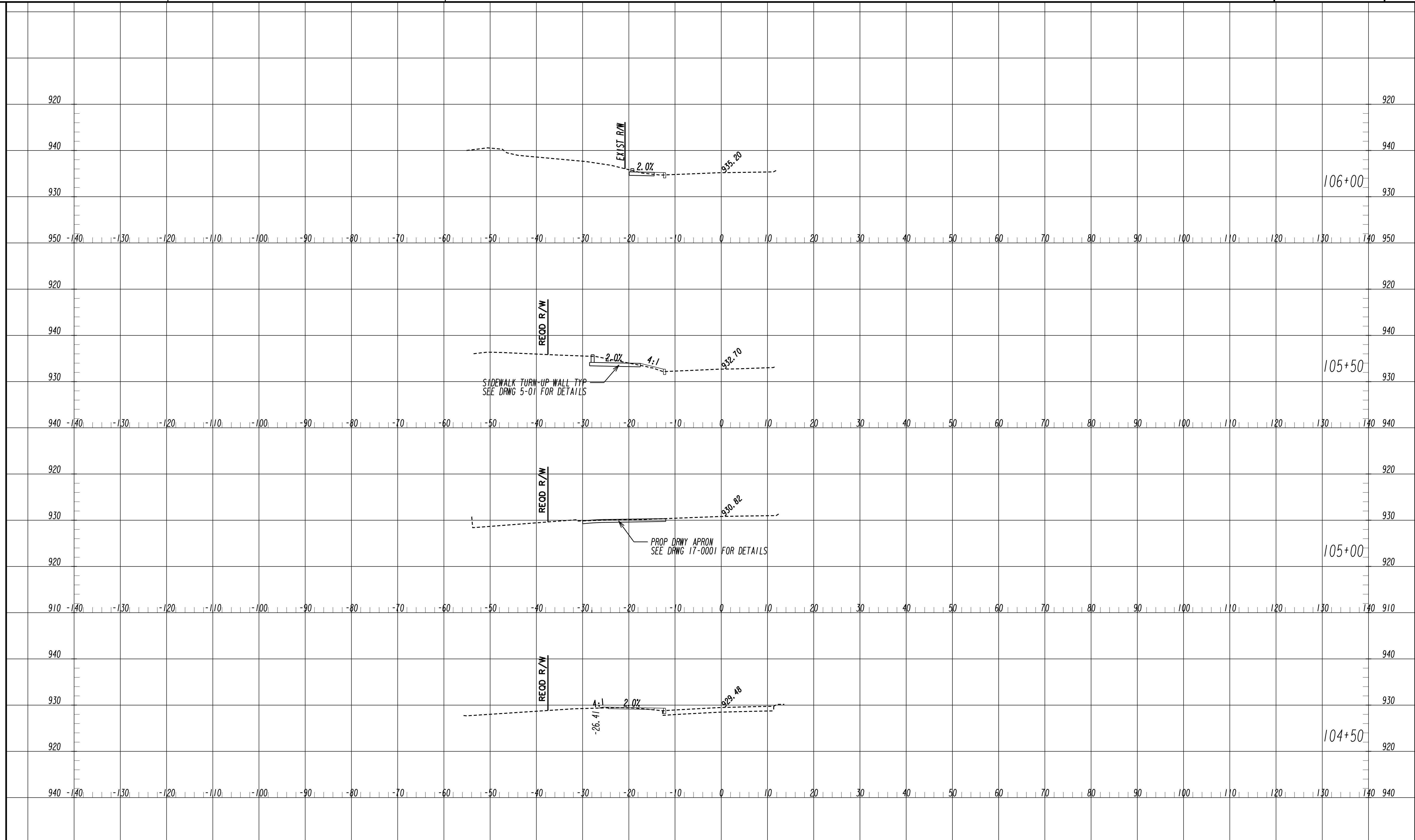
**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING



REVISION DATES

**EARTHWORK CROSS SECTIONS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 102+50 TO STA 104+00**

CHECKED:	DATE:	DRAWING No. <b>23-0002</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	

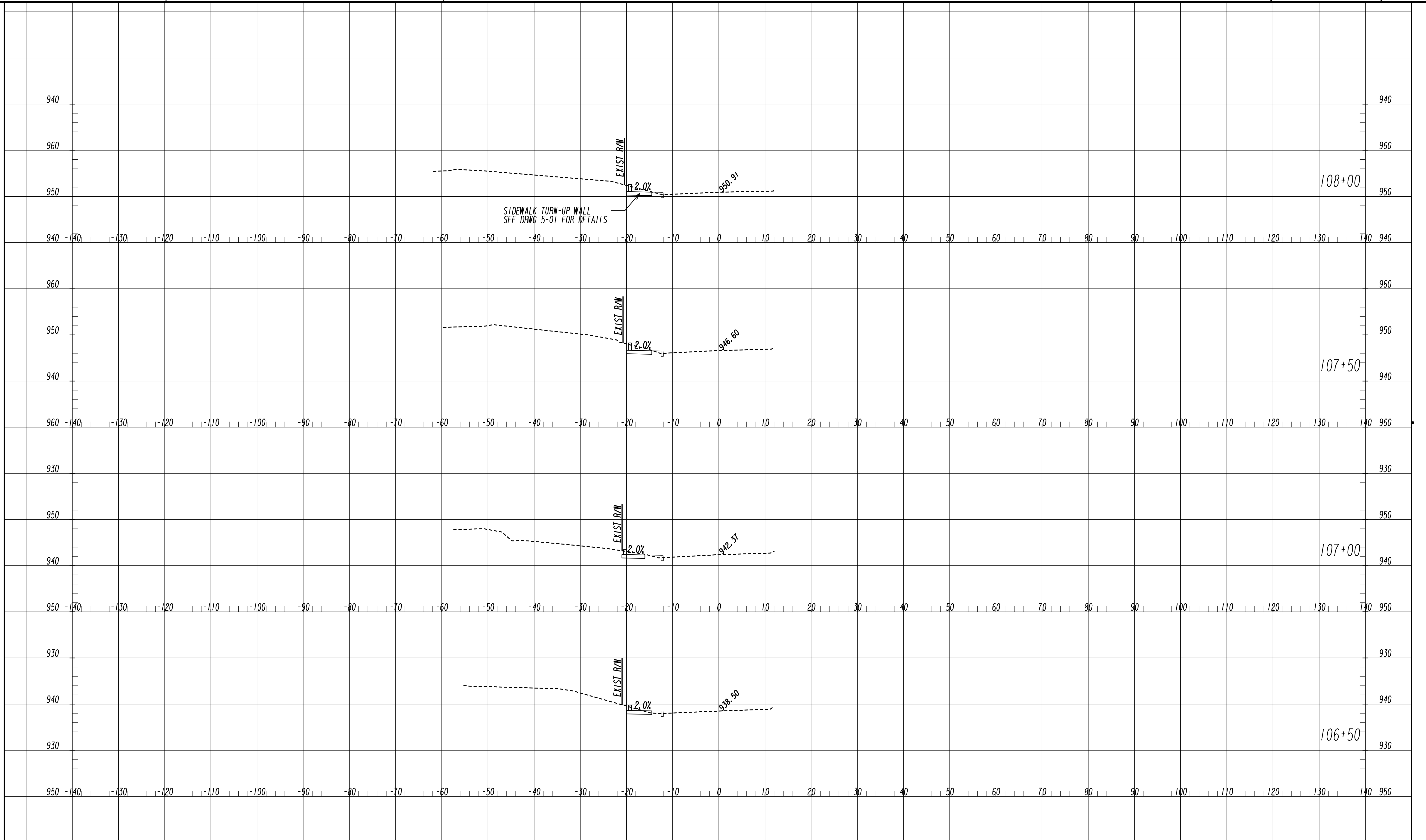


REVISION DATES

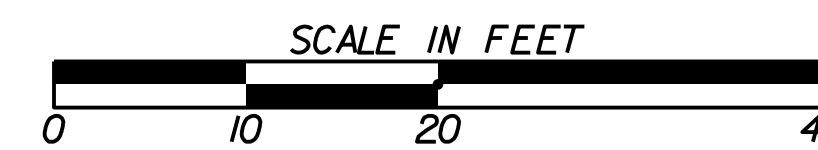
NO.	DATE	DESCRIPTION

**EARTHWORK CROSS SECTIONS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 104+50 TO STA 106+00**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	23-0003
CORRECTED:	DATE:	
VERIFIED:	DATE:	



**IE** INFRASTRUCTURE  
CONSULTING & ENGINEERING

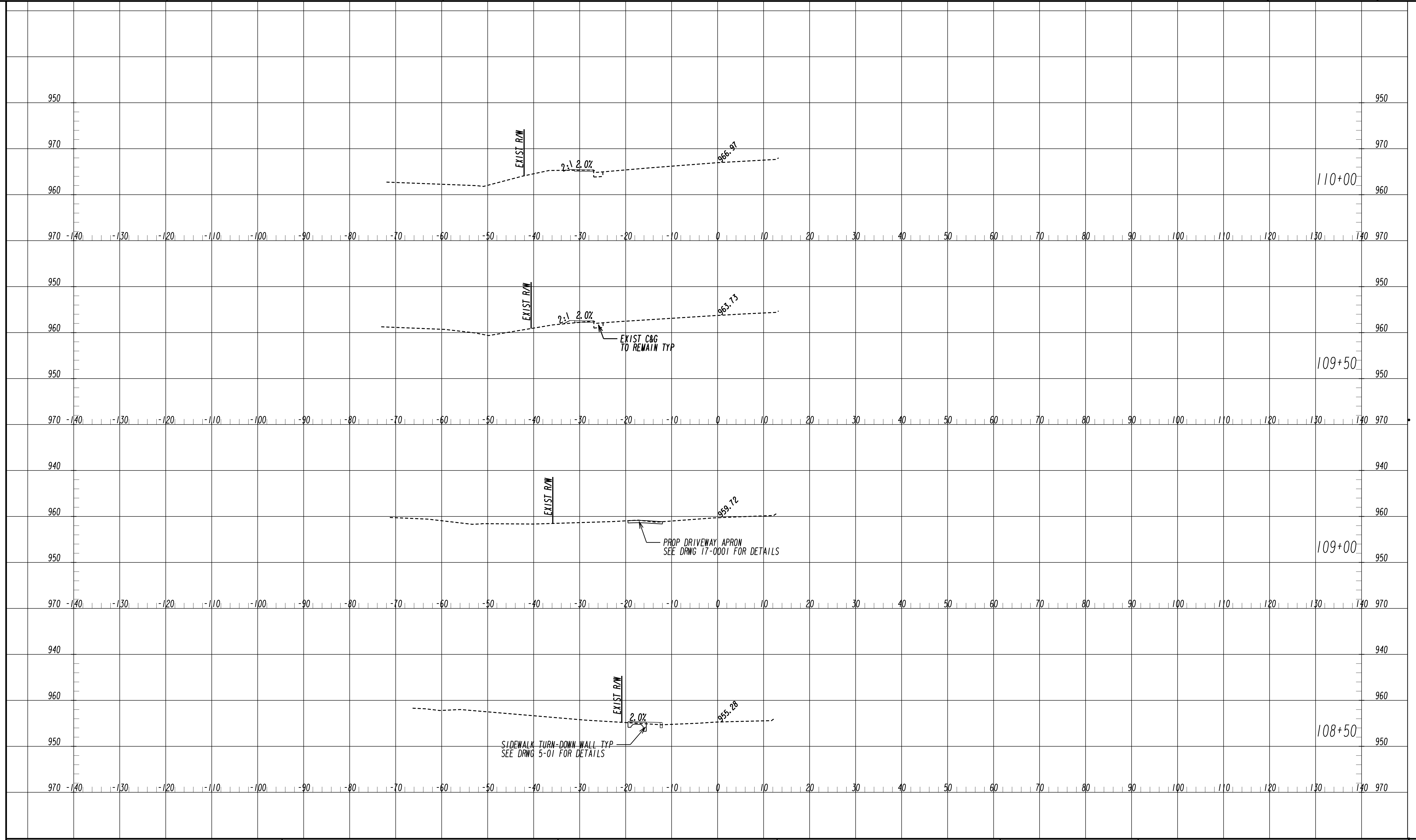


REVISION DATES

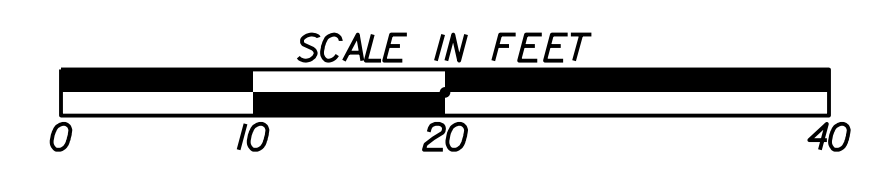
**EARTHWORK CROSS SECTIONS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 106+50 TO STA 108+00**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	23-0004
CORRECTED:	DATE:	
VERIFIED:	DATE:	





**JEINFRASTRUCTURE**  
CONSULTING & ENGINEERING

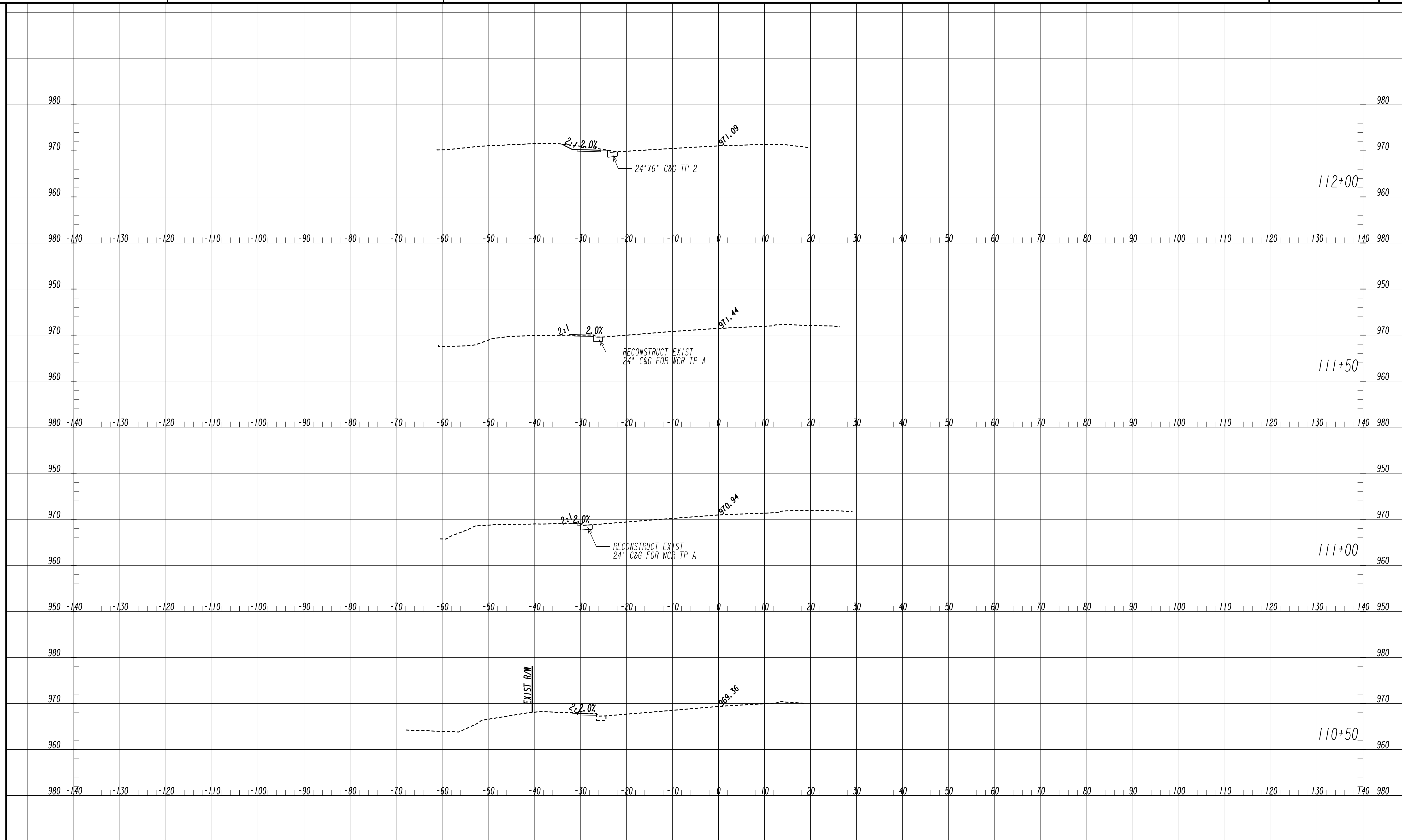


REVISION DATES

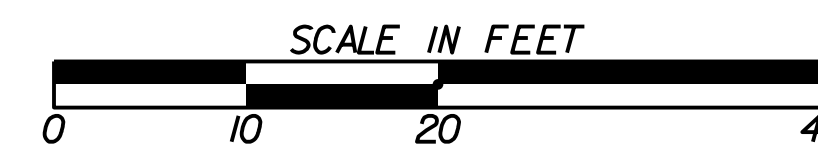
NO.	DATE	DESCRIPTION

**EARTHWORK CROSS SECTIONS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 108+50 TO STA 110+00**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	23-0005
CORRECTED:	DATE:	
VERIFIED:	DATE:	



**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING



REVISION DATES

**EARTHWORK CROSS SECTIONS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 110+50 TO STA 112+00**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	23-0006
CORRECTED:	DATE:	
VERIFIED:	DATE:	

UTILITY LINECODES

UTILITY SYMBOLS

Table with columns: EXISTING, TO BE REMOVED, PROPOSED, TYPE OF UTILITY. Rows include ELECTRIC, TELECOMMUNICATIONS, GUY WIRE, etc.

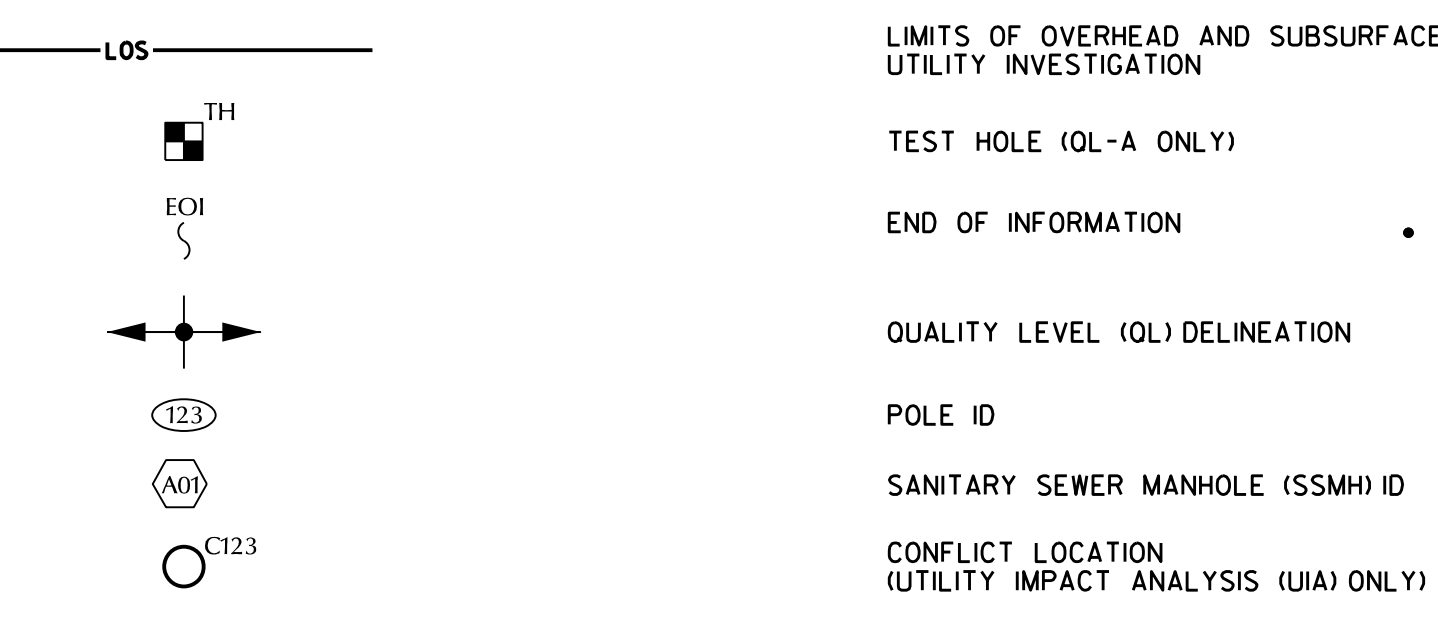
Table with columns: EXISTING, TO BE REMOVED, PROPOSED, TYPE OF UTILITY. Rows include ELECTRIC (OL-D), WATER, GAS, SANITARY SEWER, etc.

FOR PROPOSED/TEMPORARY TRAFFIC CONTROL INFORMATION REFER TO TRAFFIC SIGNAL PLANS

Table with columns: EXISTING, PROPOSED, TEMPORARY. Rows include UTILITY POLE/GUY POLE, LIGHT POLE, GUY ANCHOR, etc.

FOR PROPOSED/TEMPORARY TRAFFIC CONTROL INFORMATION REFER TO TRAFFIC SIGNAL PLANS

MISCELLANEOUS



QUALITY LEVELS AND DEFINITIONS
OL-D DEPICTED ACCORDING TO UTILITY RECORD INFORMATION AND IN-FIELD VISUAL INSPECTION. NO ELECTRONIC DESIGNATING INFORMATION WAS OBTAINED.

TELEPHONE PAIR SIZE TABLE
TELEPHONE PAIR SIZE | TELEPHONE CABLE DIAMETER
5 - 100 | 0.50 TO 2.00 IN
101 - 2400 | UP TO 3.50 IN



UTILITY LEGEND
UTILITY PLANS
BRIARWOOD ROAD SIDEWALK
CITY OF BROOKHAVEN

REVISION DATES table with columns for revision number, date, and description.

Table with columns: CHECKED, BACKCHECKED, CORRECTED, VERIFIED, DATE, DRAWING No., and a large number 24-000A.



NOT TO SCALE

UTILITY SUMMARY:

ALL OF THE FOLLOWING UTILITY OWNERS WERE REPORTED TO HAVE FACILITIES WITHIN THE VICINITY OF THIS PROJECT. UTILITIES ARE NOTED FOR EACH OWNER LISTED AND ARE SHOWN ON THE PLANS HEREON.

- 1. ATLANTA GAS LIGHT (AGL) - GAS  
CONTACT: ROBERT STACHLER  
10 PEACHTREE PLACE  
ATLANTA, GA 30309  
TEL.NO.: (404) 548-4796  
EMAIL: rstachle@southernco.com  
NOTE:
- 2. AT&T (ATT / D) - TELEPHONE  
CONTACT: HUNTER (CHARLES) SPINKS  
400 CHASTAIN CENTER BLVD, SUITE 121  
KENNESAW, GA 30144  
TEL.NO.: (706) 701-6081  
EMAIL: cs0477@att.com  
NOTE:
- 3. COMCAST COMMUNICATIONS (CC) - CATV  
CONTACT: CHARLES ROSS  
3205 SOUTH MARTIN STREET  
EAST POINT, GA 30344  
TEL.NO.: (404) 597-4353  
EMAIL: Charles.Ross@cable.comcast.com  
NOTE:
- 4. CROWN CASTLE NG NETWORKS, INC. - FIBER OPTICS  
CONTACT: JEREMY WILLIAMS  
1200 NORTHMEADOW PARKWAY, SUITE 180  
ROSWELL, GA 30076  
TEL.NO.: (404) 409-7533  
EMAIL: jeremy.williams@crowncastle.com  
NOTE:
- 5. GOOGLE FIBER - FIBER OPTICS  
CONTACT: GREG SPELL  
MILLENNIUM AT MIDTOWN  
10 10TH STREET, NE  
ATLANTA, GA 30309  
TEL.NO.: (770) 324-7693  
EMAIL: gregspell@google.com  
NOTE: HDPE FLEX DUCT AND GFIXL VAULT ONLY
- 6. DEKALB COUNTY - WATER AND SEWER  
CONTACT: JUAN NUNEZ  
4572 MEMORIAL DRIVE  
DECATUR, GA 30032  
TEL.NO.: (770) 621-7264  
EMAIL: jsnunez@dekalbcountyga.gov  
NOTE:
- 7. GEORGIA POWER - ELECTRIC  
CONTACT: LAMONTE WASLIEN  
241 RALPH MCGILL BLVD NE  
ATLANTA, GA 30308  
TEL.NO.: (404) 947-0729  
EMAIL: lwaslien@southernco.com  
NOTE:



NOT TO SCALE

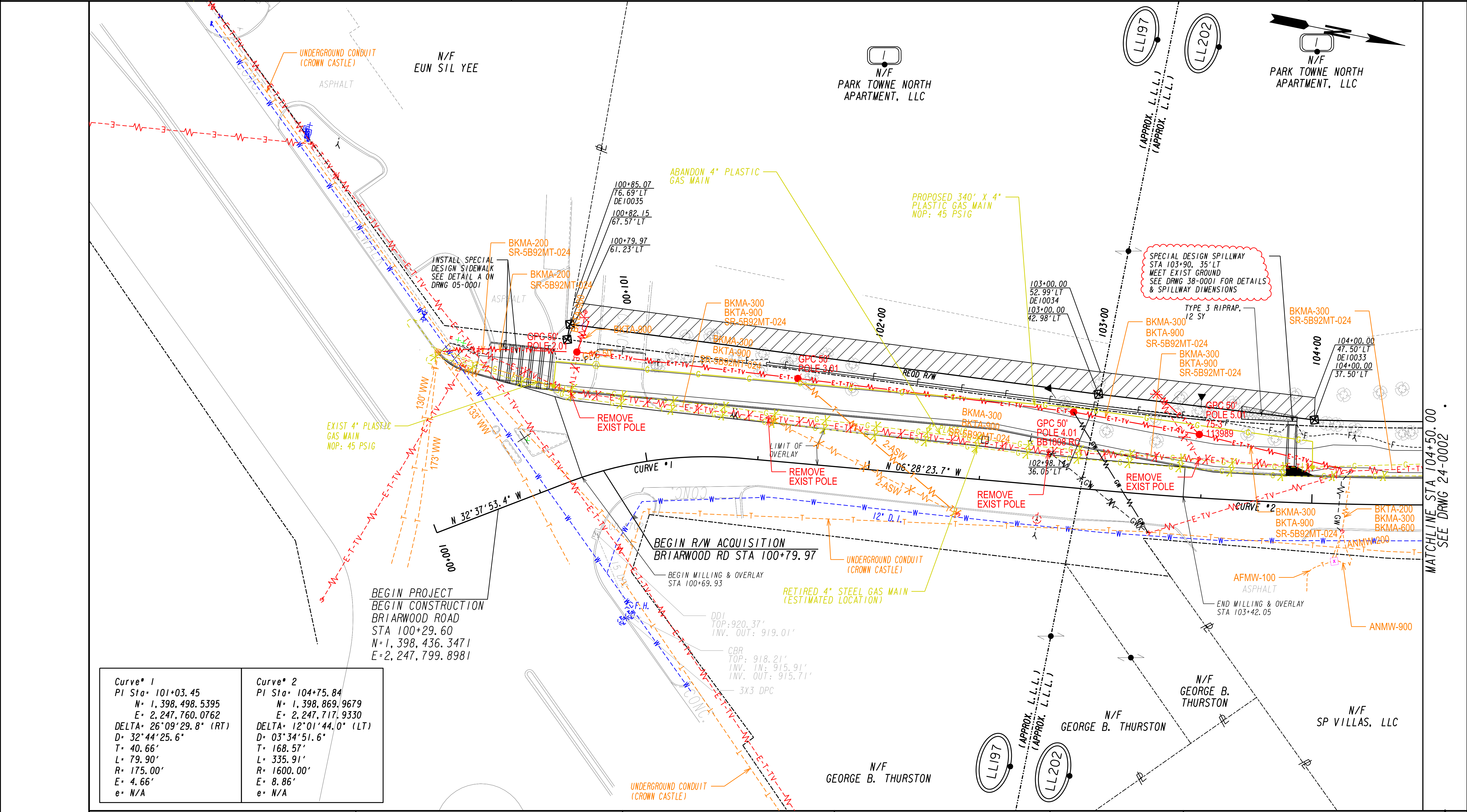
REVISION DATES

NO.	DATE	DESCRIPTION

UTILITY SUMMARY

UTILITY PLANS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	24-000B
CORRECTED:	DATE:	
VERIFIED:	DATE:	

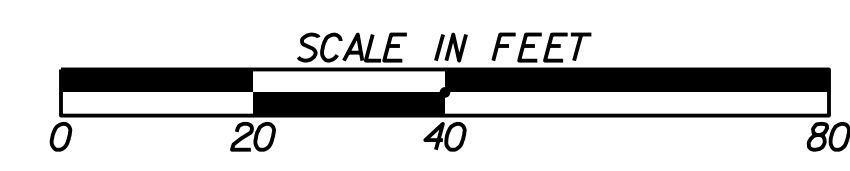


Curve* 1	Curve* 2
PI Sta+ 101+03.45	PI Sta+ 104+75.84
N+ 1,398,498.5395	N+ 1,398,869.9679
E+ 2,247,760.0762	E+ 2,247,717.9330
DELTA+ 26°09'29.8" (RT)	DELTA+ 12°01'44.0" (LT)
D+ 32°44'25.6"	D+ 03°34'51.6"
T+ 40.66'	T+ 168.57'
L+ 79.90'	L+ 335.91'
R+ 175.00'	R+ 1600.00'
E+ 4.66'	E+ 8.86'
e+ N/A	e+ N/A

PROPERTY AND EXISTING R/W LINE	-----e-----
REQUIRED R/W LINE	-----f-----
CONSTRUCTION LIMITS	-----g-----
EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES	-----h-----
EASEMENT FOR CONSTR OF SLOPES	-----i-----
EASEMENT FOR CONSTR OF DRIVES	-----j-----

BEGIN LIMIT OF ACCESS.....BLA	-----k-----
END LIMIT OF ACCESS.....ELA	-----l-----
LIMIT OF ACCESS	-----m-----
REQ'D R/W & LIMIT OF ACCESS	-----n-----
ORANGE BARRIER FENCE	-----o-----
ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)	-----p-----

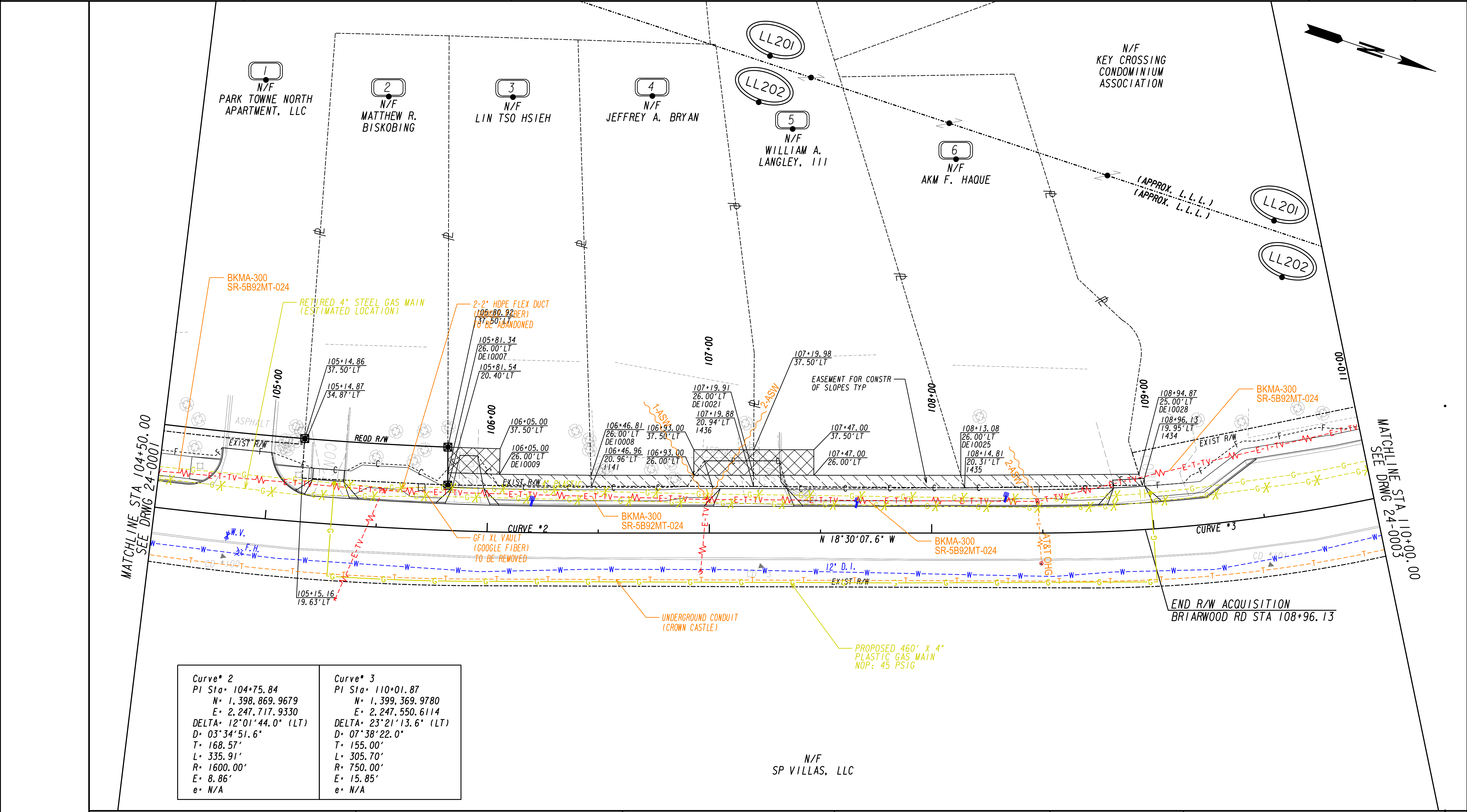
**INFRASTRUCTURE**  
CONSULTING & ENGINEERING



REVISION DATES	
8/16/2019	

**UTILITY PLANS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**  
**STA 100+36.07 TO STA 104+50.00**

CHECKED:	DATE:	DRAWING No. <b>24-0001</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	

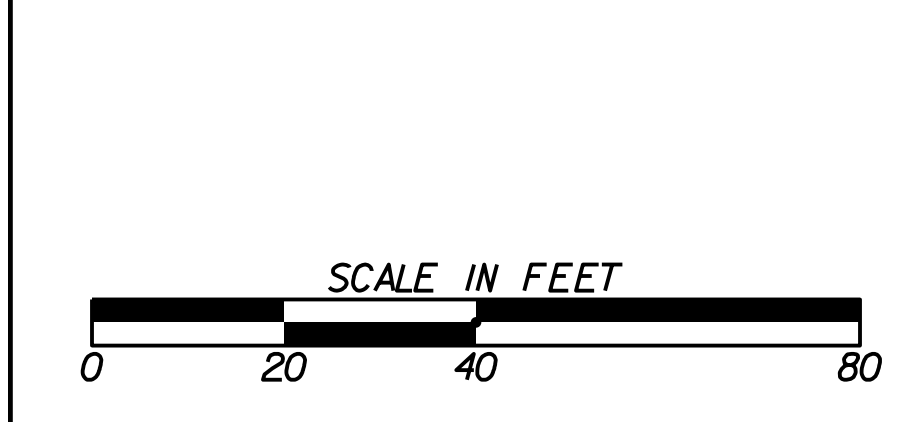


Curve # 2	Curve # 3
PI Sta: 104+75.84	PI Sta: 110+01.87
N = 1,398,869.9679	N = 1,399,369.9780
E = 2,247,717.9330	E = 2,247,550.6114
DELTA = 12°01'44.0" (LT)	DELTA = 23°21'13.6" (LT)
D = 03°34'51.6"	D = 07°38'22.0"
T = 168.57'	T = 155.00'
L = 335.91'	L = 305.70'
R = 1600.00'	R = 750.00'
E = 8.86'	E = 15.85'
e = N/A	e = N/A

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES

-----e-----  
 ---C---F---  
  
  
  
 BEGIN LIMIT OF ACCESS.....BLA  
 END LIMIT OF ACCESS.....ELA  
 LIMIT OF ACCESS  
 REQ'D R/W & LIMIT OF ACCESS  
 ORANGE BARRIER FENCE  
 ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)

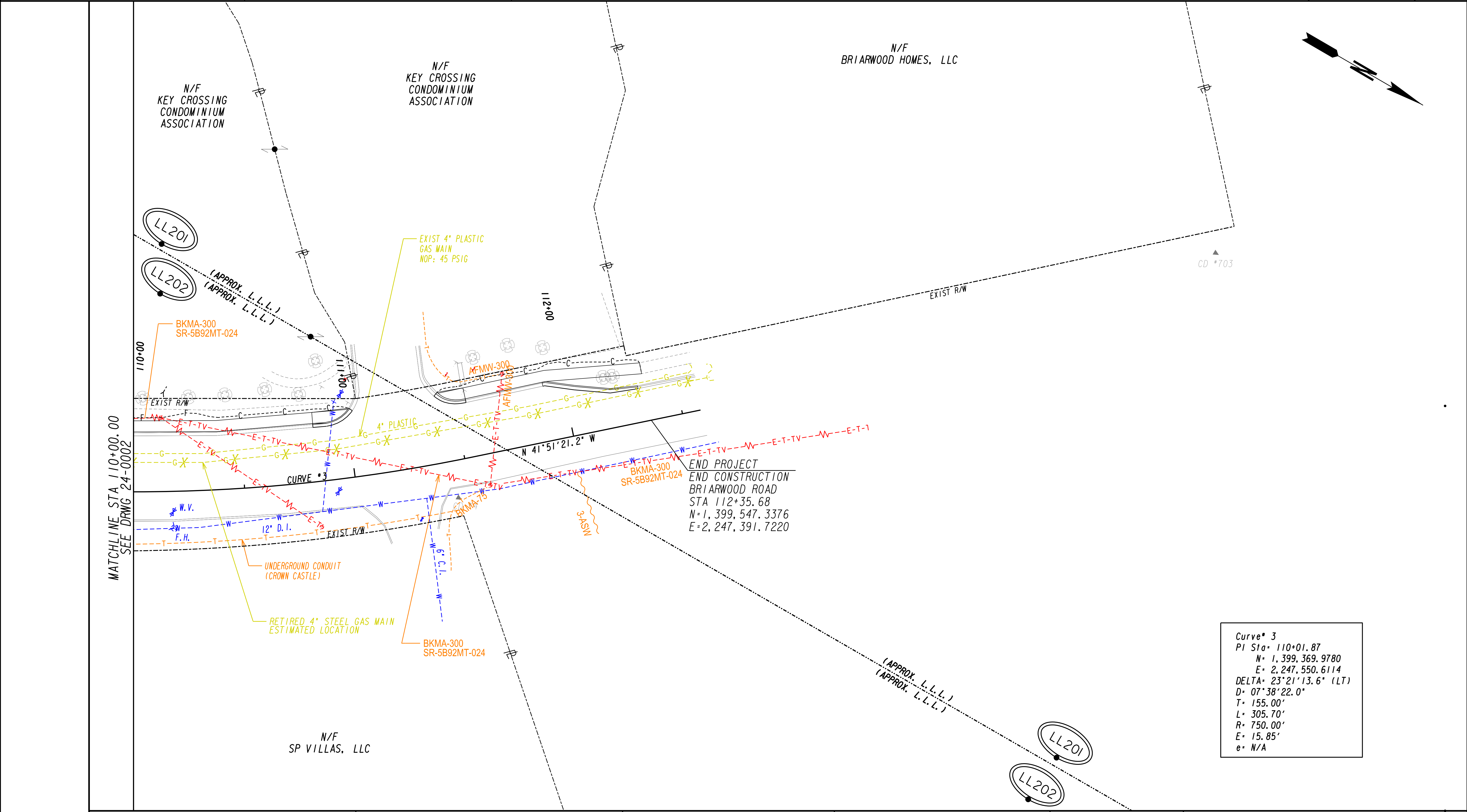
**INFRASTRUCTURE**  
 CONSULTING & ENGINEERING



REVISION DATES	

**UTILITY PLANS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**  
**STA 104+50.00 TO STA 110+00.00**

CHECKED:	DATE:	DRAWING No. <b>24-0002</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	



Curve# 3  
 PI Sta= 110+01.87  
 N= 1,399,369.9780  
 E= 2,247,550.6114  
 DELTA= 23°21'13.6" (LT)  
 D= 07°38'22.0"  
 T= 155.00'  
 L= 305.70'  
 R= 750.00'  
 E= 15.85'  
 e= N/A

END PROJECT  
 END CONSTRUCTION  
 BRIARWOOD ROAD  
 STA 112+35.68  
 N=1,399,547.3376  
 E=2,247,391.7220

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR  
 & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES

-----e-----  
 ---c---f---  
 [Hatched Box]  
 [Hatched Box]  
 [Hatched Box]

BEGIN LIMIT OF ACCESS.....BLA  
 END LIMIT OF ACCESS.....ELA  
 LIMIT OF ACCESS  
 REQ'D R/W & LIMIT OF ACCESS  
 ORANGE BARRIER FENCE  
 ESA - ENV. SENSITIVE AREA  
 (SEE ERIT TABLE)

**IE** INFRASTRUCTURE  
 CONSULTING & ENGINEERING

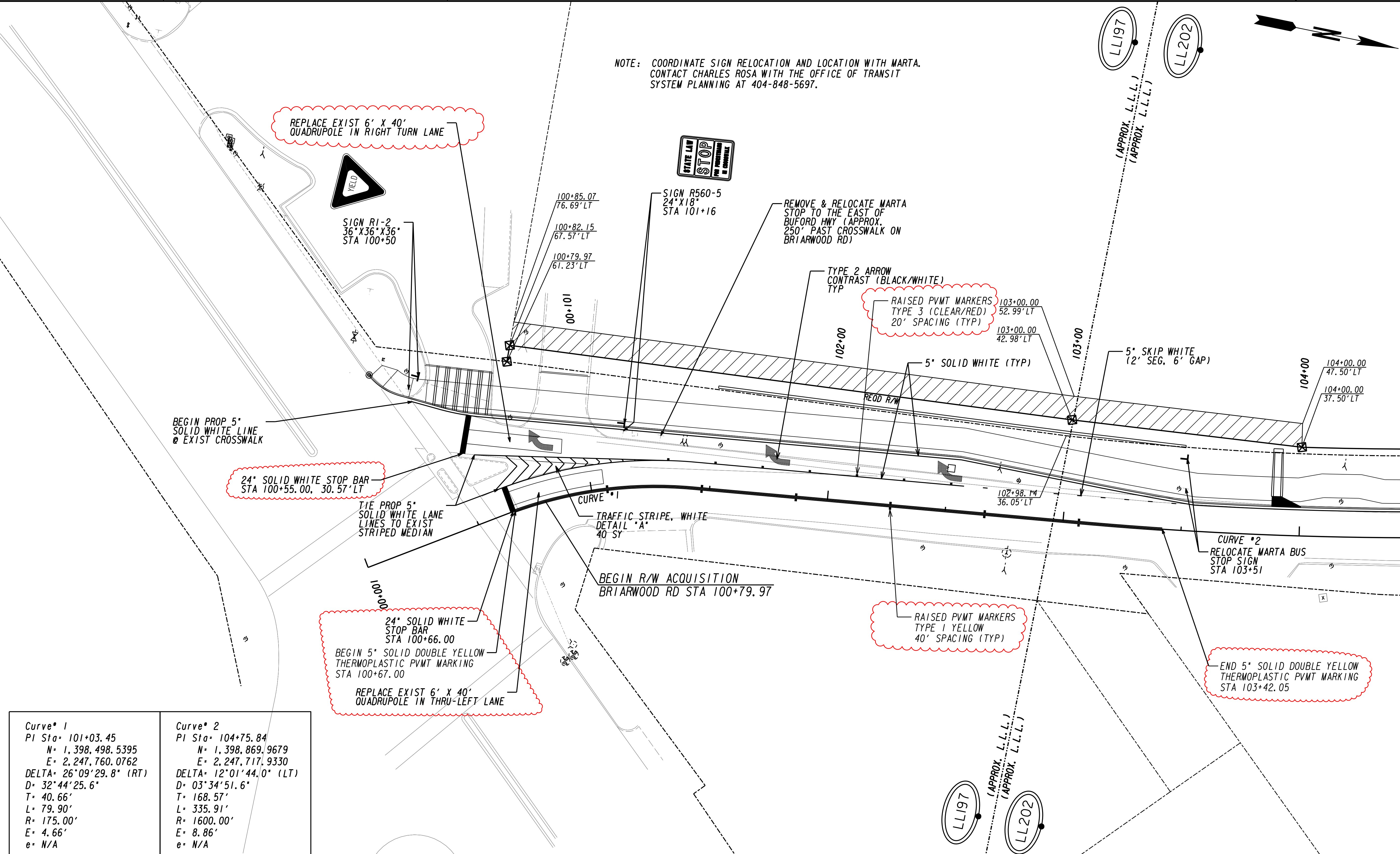


REVISION DATES	

**UTILITY PLANS  
 BRIARWOOD ROAD SIDEWALK  
 CITY OF BROOKHAVEN  
 STA 110+00.00 TO STA 112+35.68**

CHECKED:	DATE:	DRAWING No. <b>24-0003</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	

NOTE: COORDINATE SIGN RELOCATION AND LOCATION WITH MARTA. CONTACT CHARLES ROSA WITH THE OFFICE OF TRANSIT SYSTEM PLANNING AT 404-848-5697.

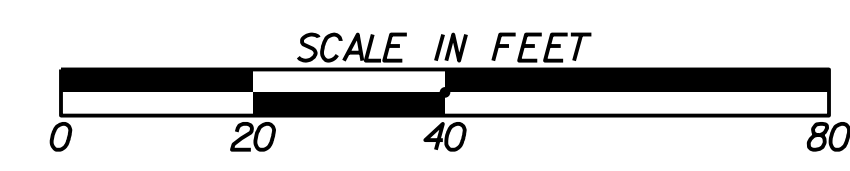


<p>Curve* 1          PI Sta= 101+03.45          N= 1,398,498.5395          E= 2,247,760.0762          DELTA= 26°09'29.8" (RT)          D= 32°44'25.6"          T= 40.66'          L= 79.90'          R= 175.00'          E= 4.66'          e= N/A</p>	<p>Curve* 2          PI Sta= 104+75.84          N= 1,398,869.9679          E= 2,247,717.9330          DELTA= 12°01'44.0" (LT)          D= 03°34'51.6"          T= 168.57'          L= 335.91'          R= 1600.00'          E= 8.86'          e= N/A</p>
---	--

<p>PROPERTY AND EXISTING R/W LINE          REQUIRED R/W LINE          CONSTRUCTION LIMITS          EASEMENT FOR CONSTR          &amp; MAINTENANCE OF SLOPES          EASEMENT FOR CONSTR OF SLOPES          EASEMENT FOR CONSTR OF DRIVES</p>	<p>-----e-----          -C-F-      </p>
---	---

<p>BEGIN LIMIT OF ACCESS.....BLA          END LIMIT OF ACCESS.....ELA          LIMIT OF ACCESS          REQ'D R/W &amp; LIMIT OF ACCESS          ORANGE BARRIER FENCE          ESA - ENV. SENSITIVE AREA          (SEE ERIT TABLE)</p>	<p>-----          - - - -    </p>
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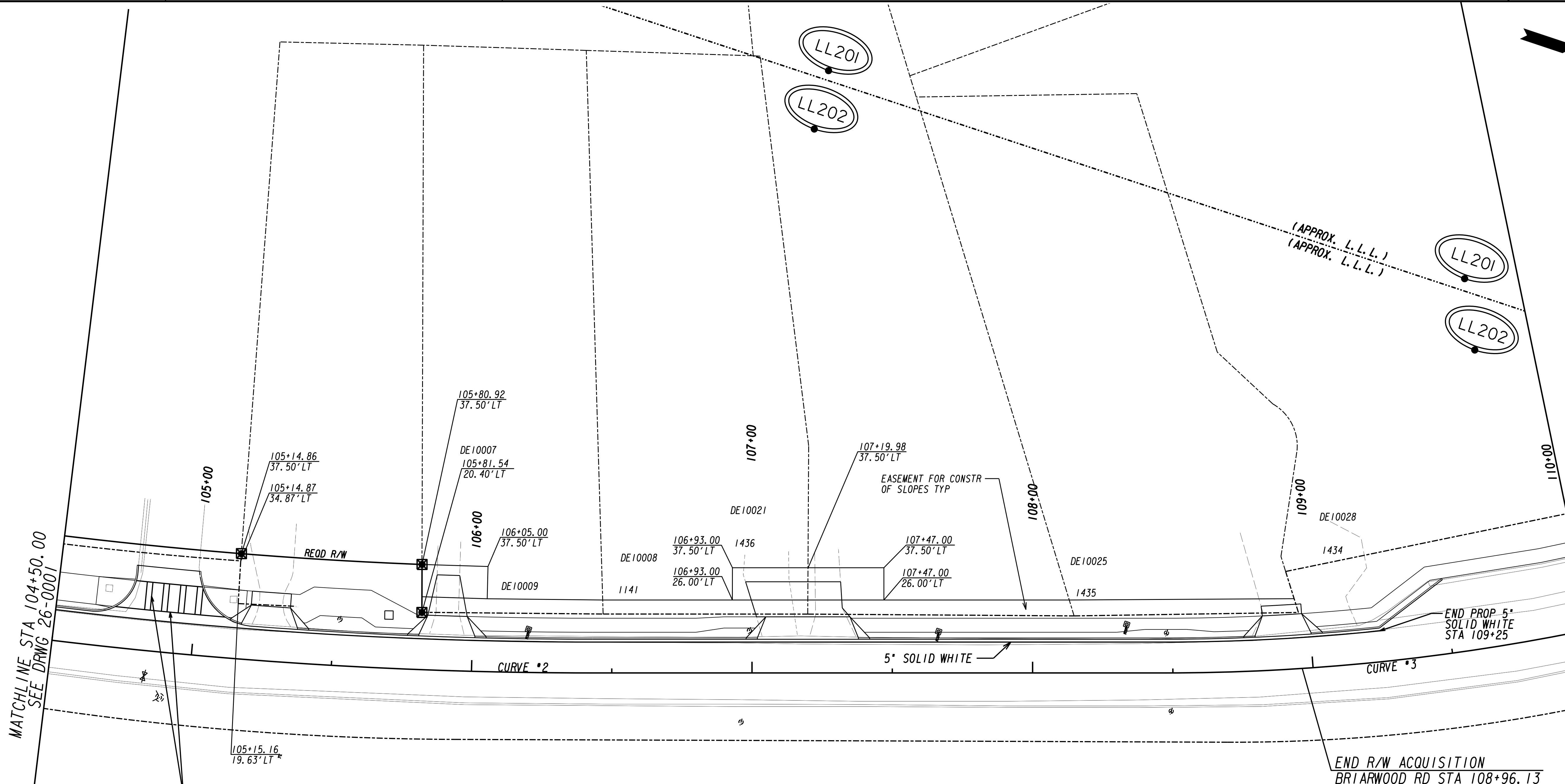
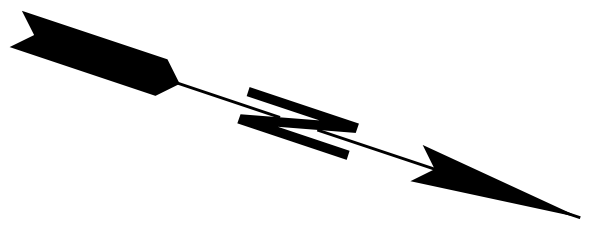
INFRASTRUCTURE CONSULTING & ENGINEERING



REVISION DATES	
8/16/2019	

<b>SIGNING AND MARKING PLANS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN STA 100+36.07 TO STA 104+50.00</b>	
CHECKED:	DATE:
BACKCHECKED:	DATE:
CORRECTED:	DATE:
VERIFIED:	DATE:
DRAWING No. <b>26-0001</b>	





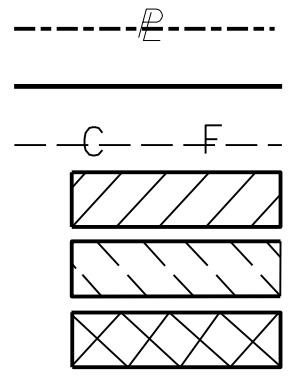
MATCHLINE STA 104+50.00  
SEE DRWG 26-0001

MATCHLINE STA 110+00.00  
SEE DRWG 26-0003

<p><b>Curve # 2</b>                  PI Sta: 104+75.84                  N= 1,398,869.9679                  E= 2,247,717.9330                  DELTA= 12°01'44.0" (LT)                  D= 03°34'51.6"                  T= 168.57'                  L= 335.91'                  R= 1600.00'                  E= 8.86'                  e= N/A</p>	<p><b>Curve # 3</b>                  PI Sta: 110+01.87                  N= 1,399,369.9780                  E= 2,247,550.6114                  DELTA= 23°21'13.6" (LT)                  D= 07°38'22.0"                  T= 155.00'                  L= 305.70'                  R= 750.00'                  E= 15.85'                  e= N/A</p>
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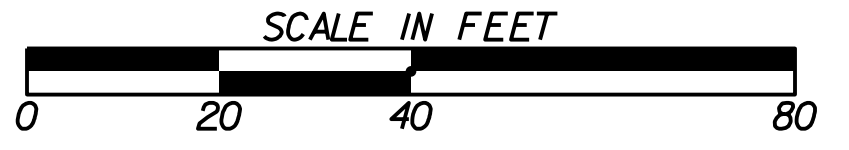
NOTE: COORDINATE SIGN RELOCATION AND LOCATION WITH MARTA.  
CONTACT CHARLES ROSA WITH THE OFFICE OF TRANSIT  
SYSTEM PLANNING AT 404-848-5697.

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR  
 & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES



BEGIN LIMIT OF ACCESS.....BLA  
 END LIMIT OF ACCESS.....ELA  
 LIMIT OF ACCESS  
 REQ'D R/W & LIMIT OF ACCESS  
 ORANGE BARRIER FENCE  
 ESA - ENV. SENSITIVE AREA  
 (SEE ERIT TABLE)

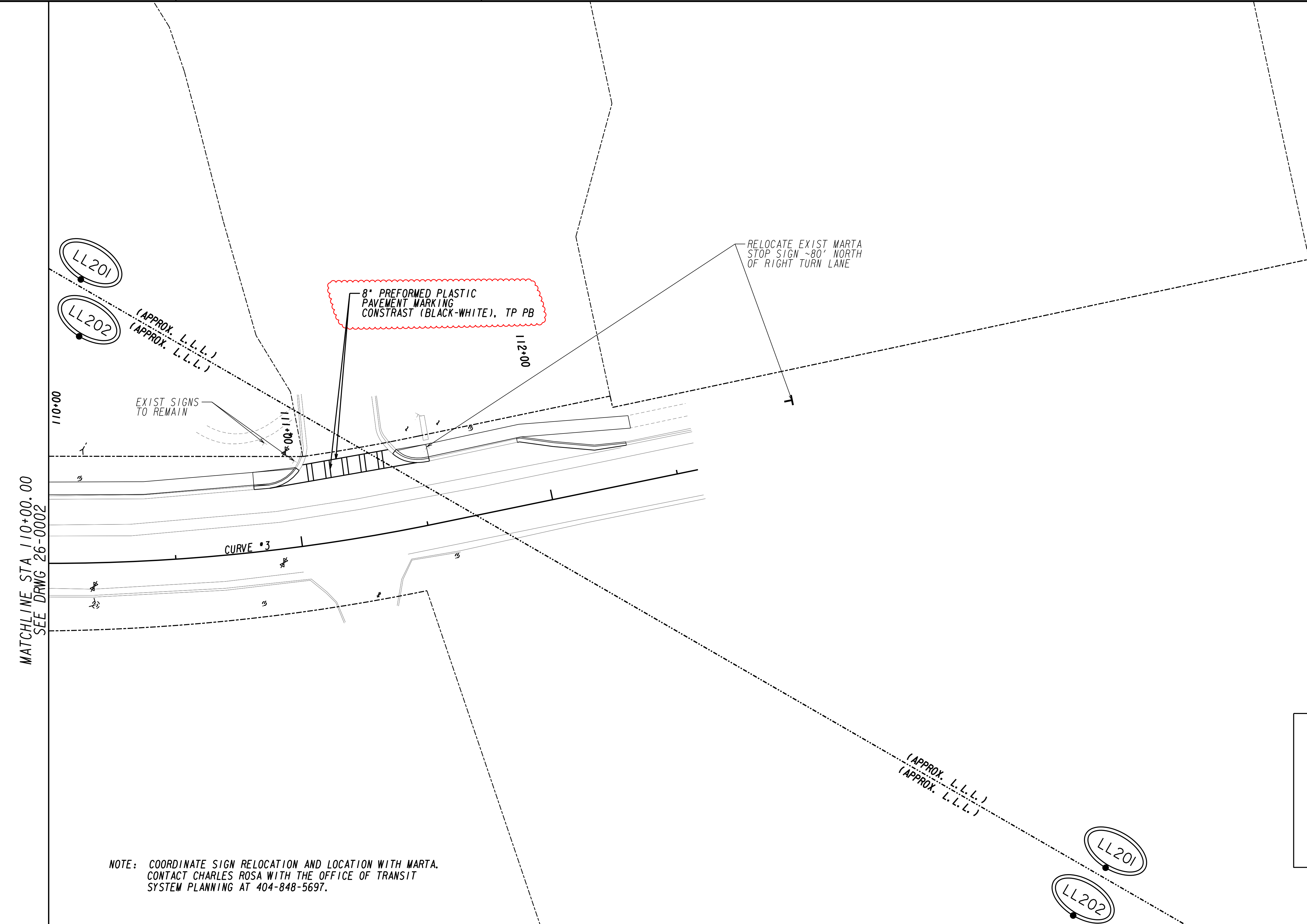
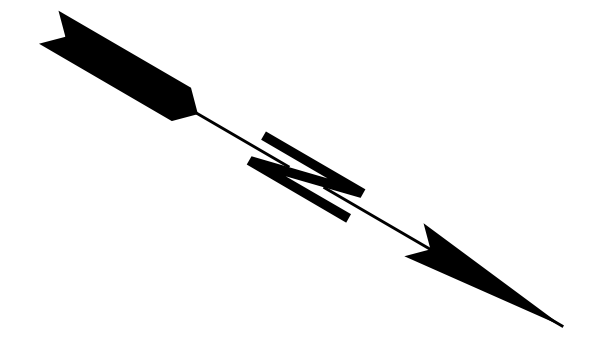
**INFRASTRUCTURE**  
CONSULTING & ENGINEERING



REVISION DATES	

**SIGNING AND MARKING PLANS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**  
**STA 104+50.00 TO STA 110+00.00**

CHECKED:	DATE:	DRAWING No. <b>26-0002</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	



8" PREFORMED PLASTIC  
PAVEMENT MARKING  
CONTRAST (BLACK-WHITE), TP PB

RELOCATE EXIST MARTA  
STOP SIGN ~80' NORTH  
OF RIGHT TURN LANE

EXIST SIGNS  
TO REMAIN

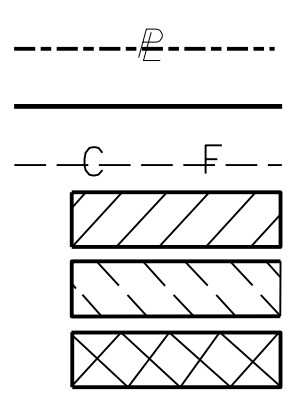
CURVE #3

MATCHLINE STA 110+00.00  
SEE DRWG 26-0002

NOTE: COORDINATE SIGN RELOCATION AND LOCATION WITH MARTA.  
CONTACT CHARLES ROSA WITH THE OFFICE OF TRANSIT  
SYSTEM PLANNING AT 404-848-5697.

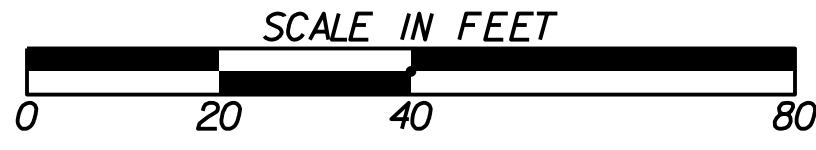
Curve # 3  
PI Sta= 110+01.87  
N= 1,399,369.9780  
E= 2,247,550.6114  
DELTA= 23°21'13.6" (LT)  
D= 07°38'22.0"  
T= 155.00'  
L= 305.70'  
R= 750.00'  
E= 15.85'  
e= N/A

PROPERTY AND EXISTING R/W LINE  
REQUIRED R/W LINE  
CONSTRUCTION LIMITS  
EASEMENT FOR CONSTR  
& MAINTENANCE OF SLOPES  
EASEMENT FOR CONSTR OF SLOPES  
EASEMENT FOR CONSTR OF DRIVES



BEGIN LIMIT OF ACCESS.....BLA  
END LIMIT OF ACCESS.....ELA  
LIMIT OF ACCESS  
REQ'D R/W & LIMIT OF ACCESS  
ORANGE BARRIER FENCE  
ESA - ENV. SENSITIVE AREA  
(SEE ERIT TABLE)

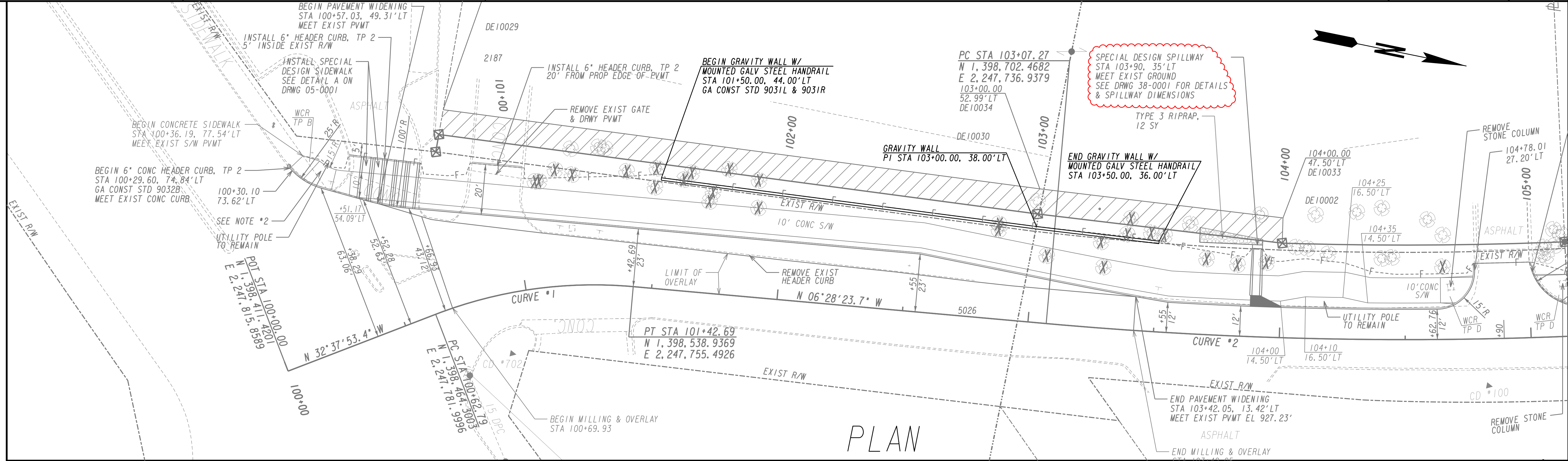
**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING



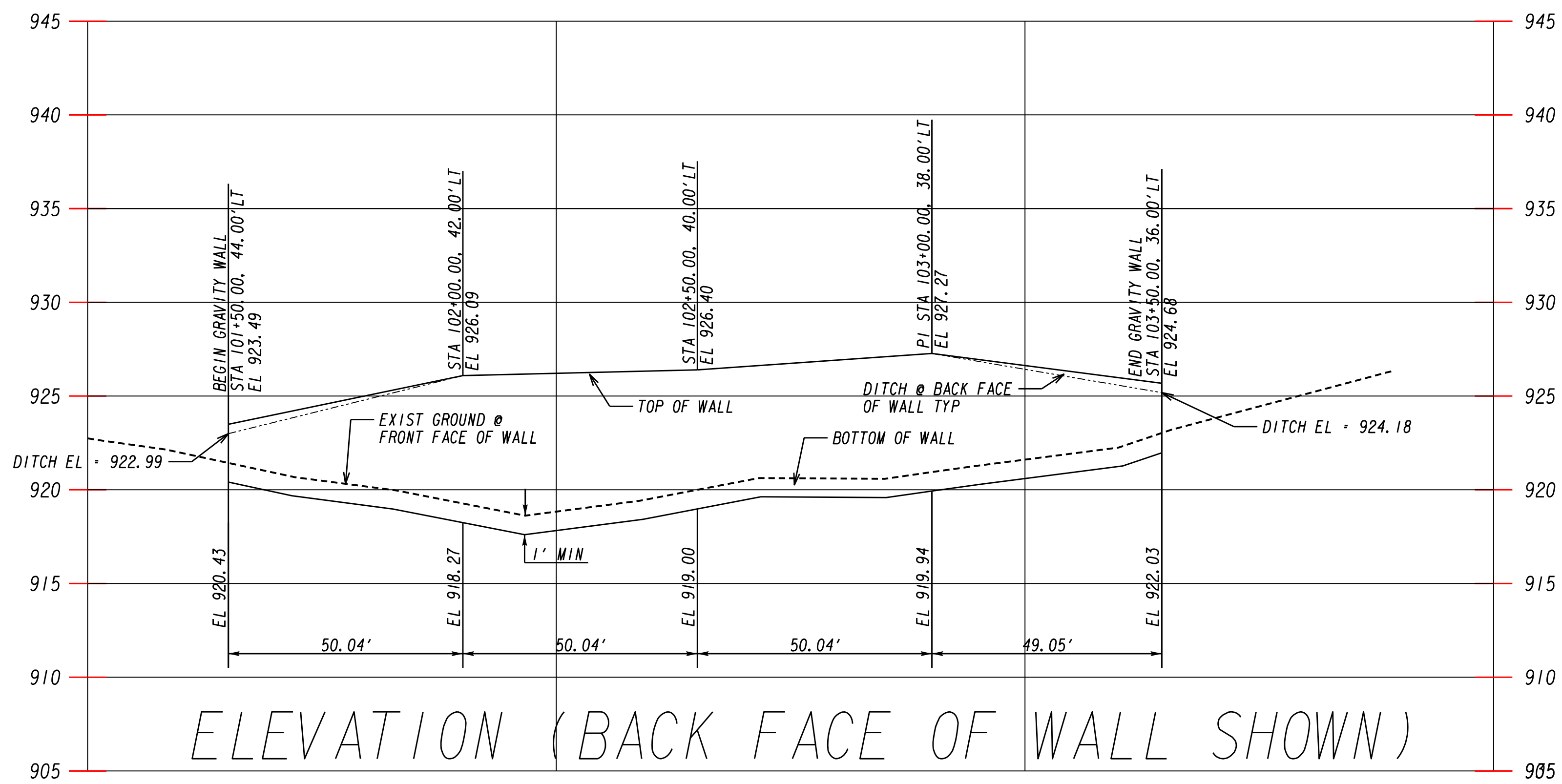
REVISION DATES	
8/16/2019	

**SIGNING AND MARKING PLANS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**  
**STA 110+00.00 TO STA 112+35.68**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	26-0003
CORRECTED:	DATE:	
VERIFIED:	DATE:	

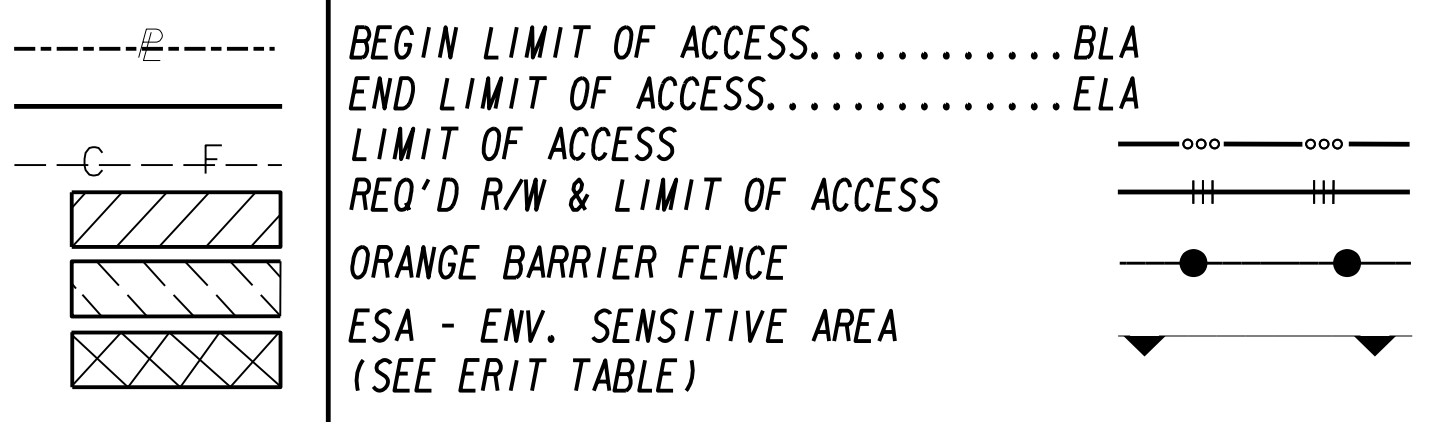


PLAN

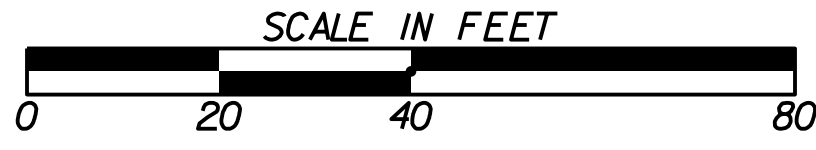


ELEVATION (BACK FACE OF WALL SHOWN)

PROPERTY AND EXISTING R/W LINE  
 REQUIRED R/W LINE  
 CONSTRUCTION LIMITS  
 EASEMENT FOR CONSTR  
 & MAINTENANCE OF SLOPES  
 EASEMENT FOR CONSTR OF SLOPES  
 EASEMENT FOR CONSTR OF DRIVES



**INFRASTRUCTURE**  
 CONSULTING & ENGINEERING



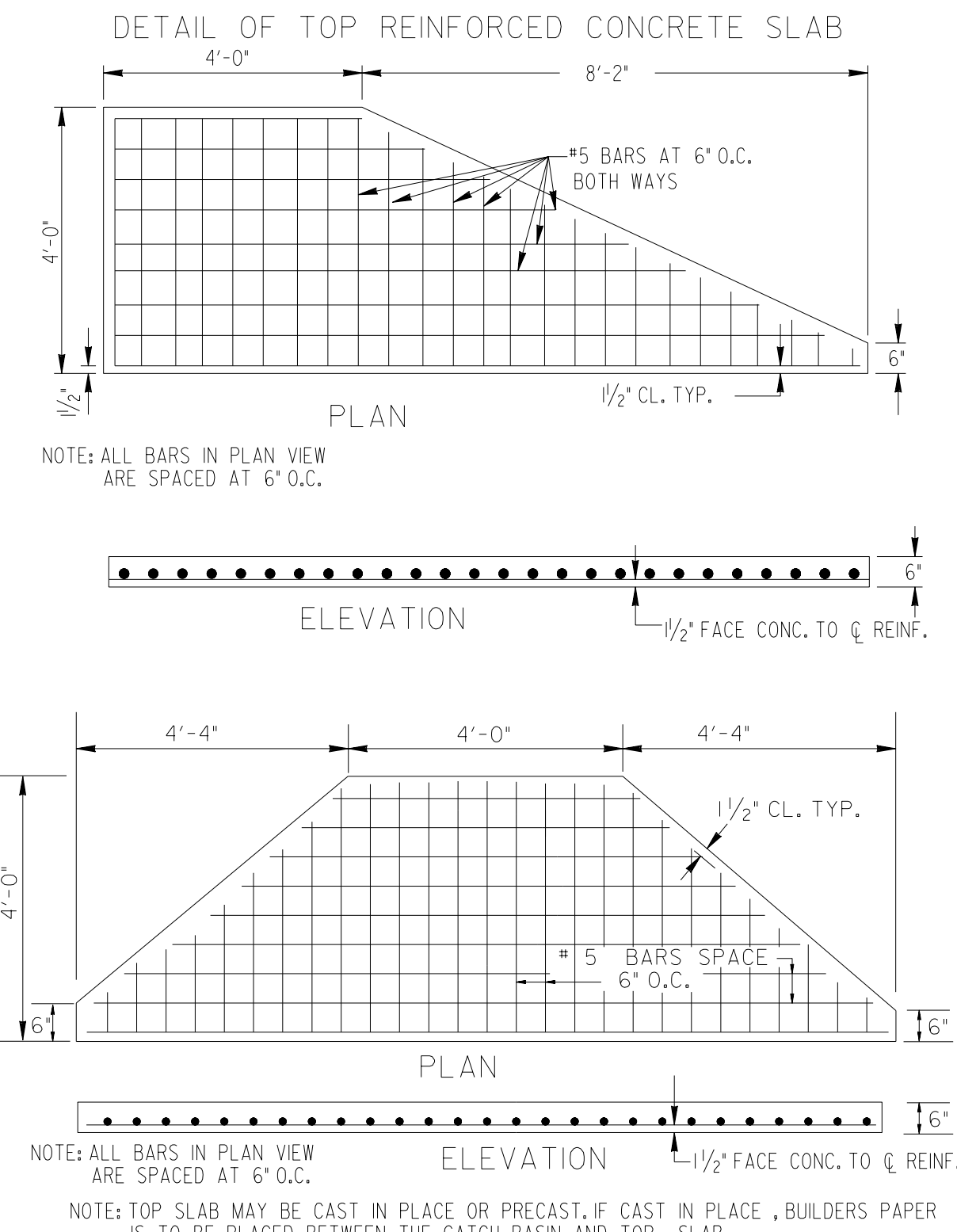
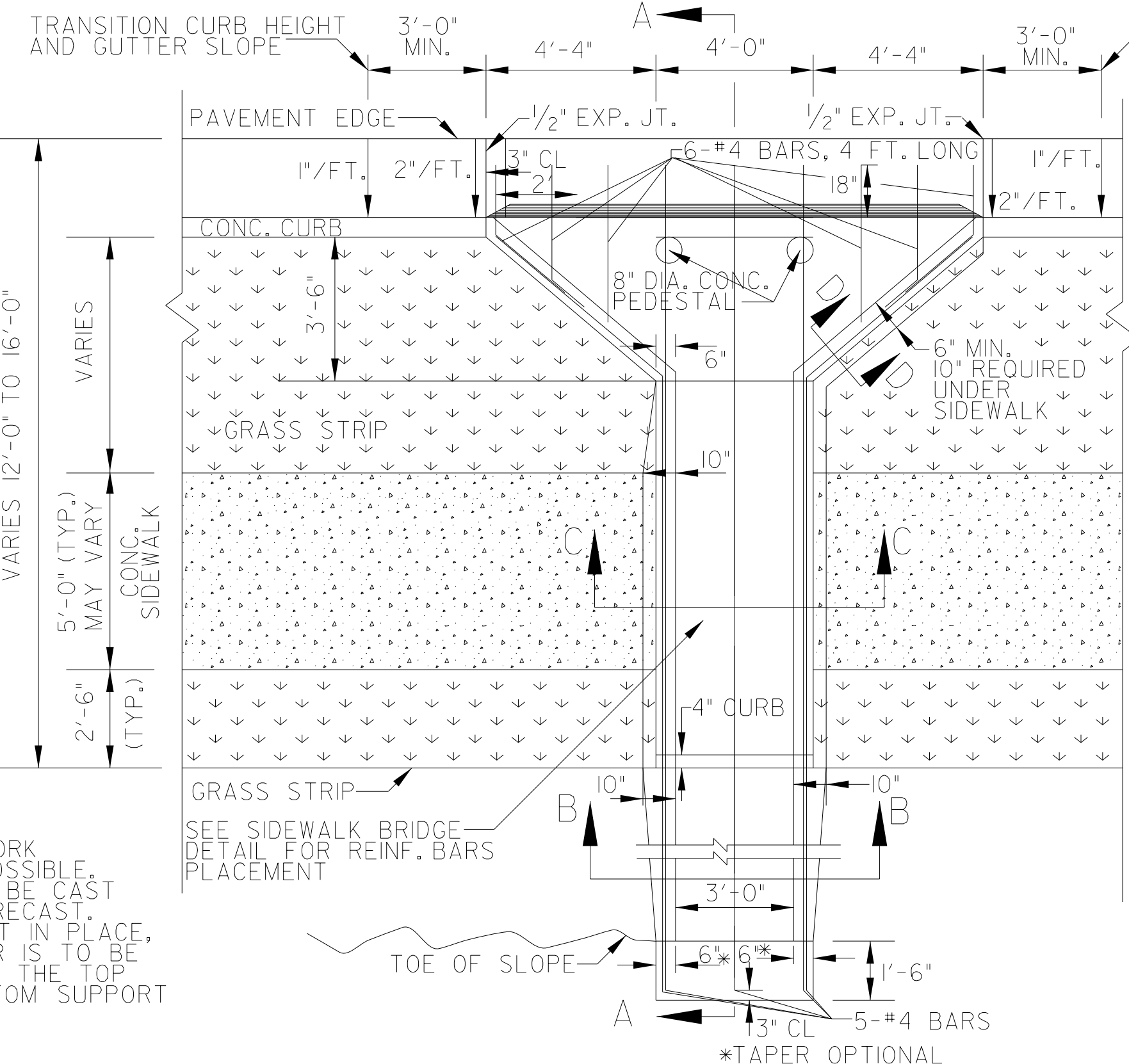
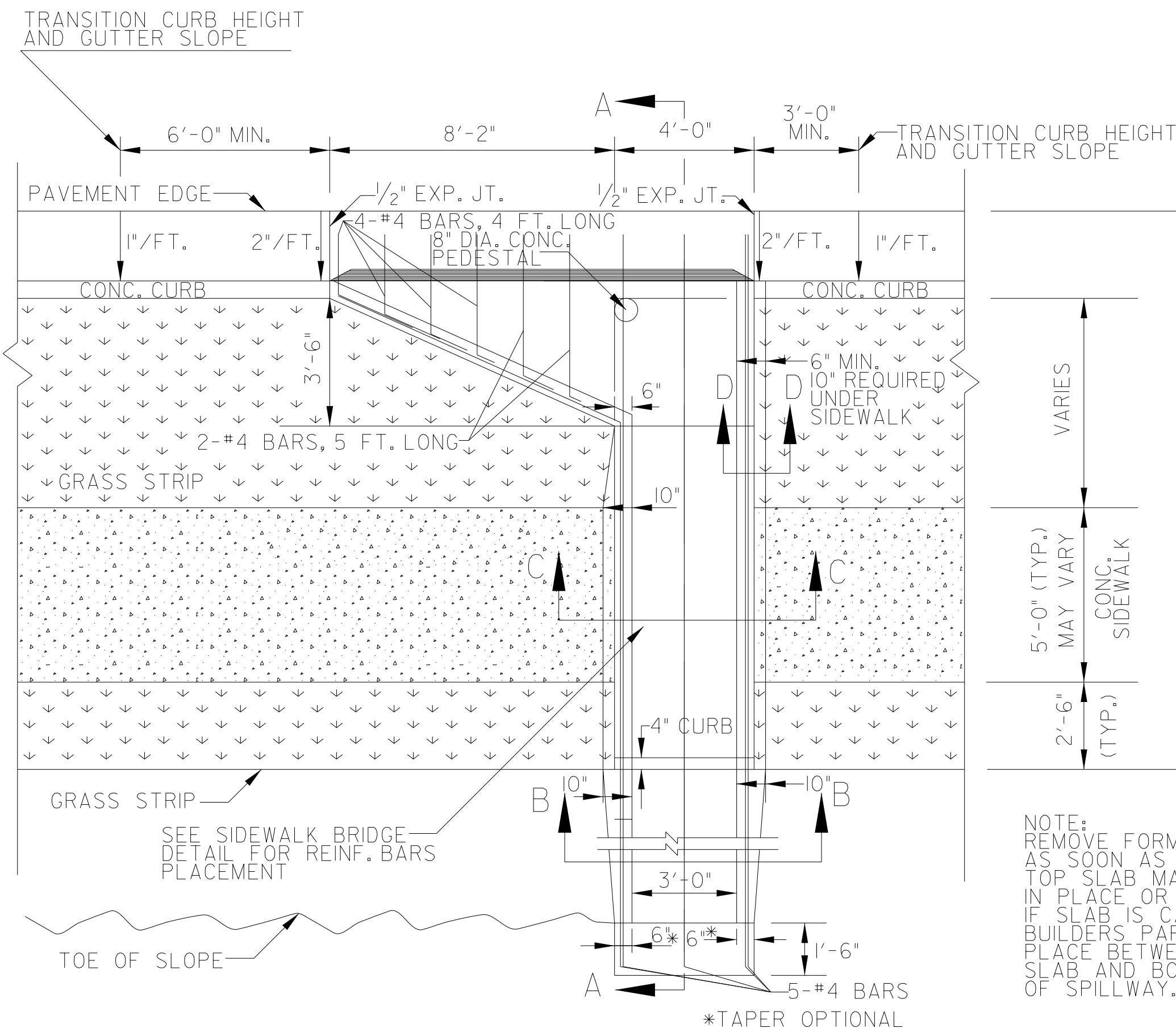
REVISION DATES	
8/16/2019	

**RETAINING WALL PLANS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	32-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

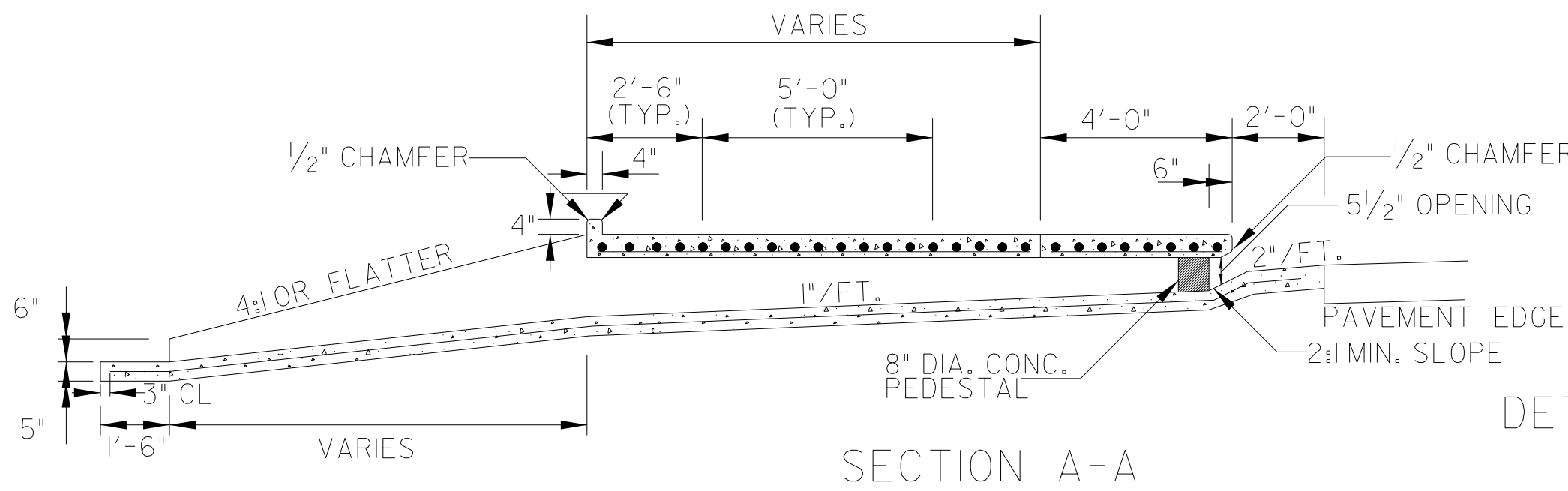
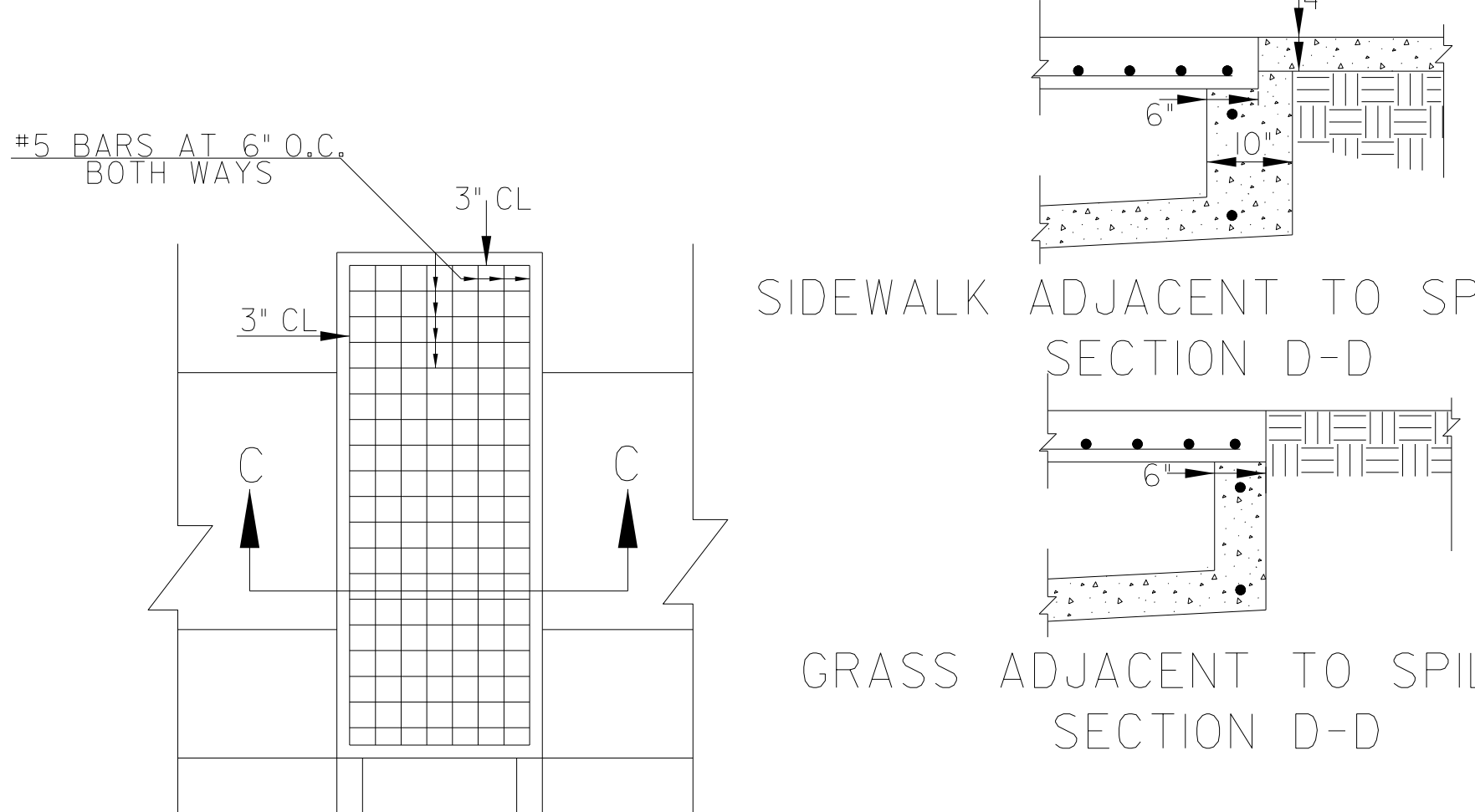
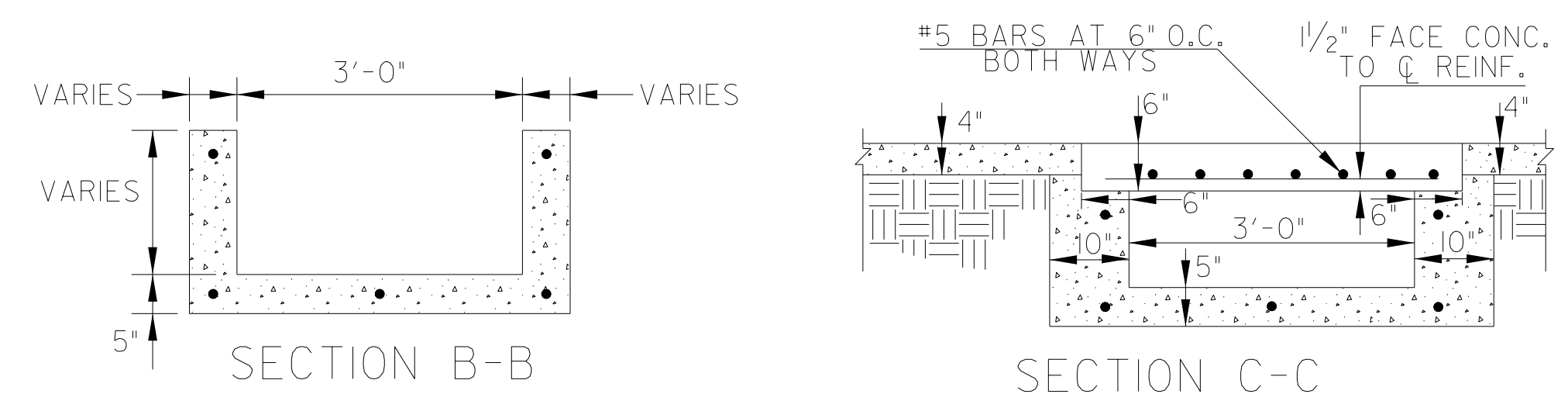
SPECIAL DESIGN SPILLWAY

SPECIAL DESIGN SPILLWAY  
LOW POINT



NOTE:  
REMOVE FORMWORK  
AS SOON AS POSSIBLE.  
TOP SLAB MAY BE CAST  
IN PLACE OR PRECAST.  
IF SLAB IS CAST IN PLACE,  
BUILDERS PAPER IS TO BE  
PLACED BETWEEN THE TOP  
SLAB AND BOTTOM SUPPORT  
OF SPILLWAY.

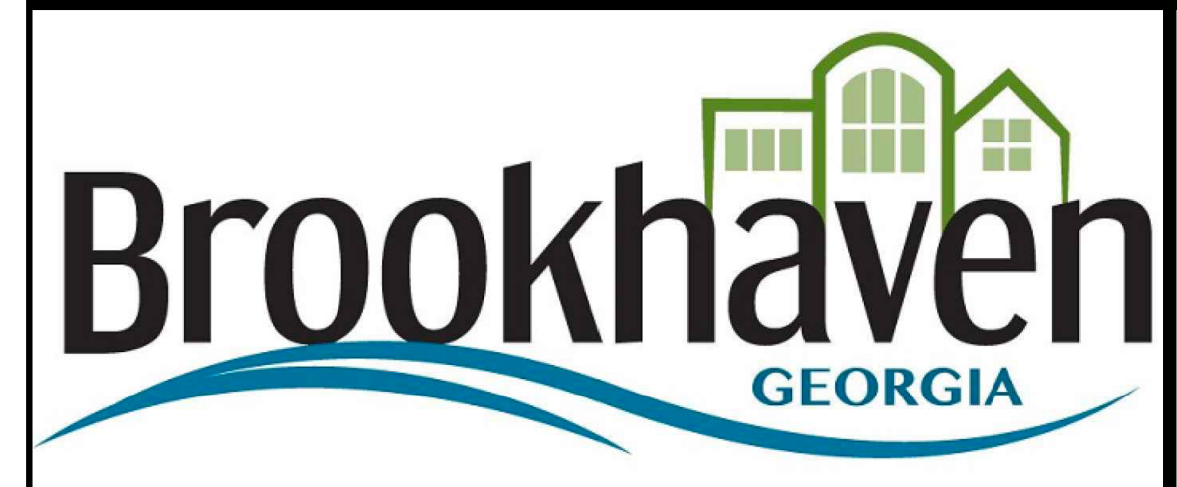
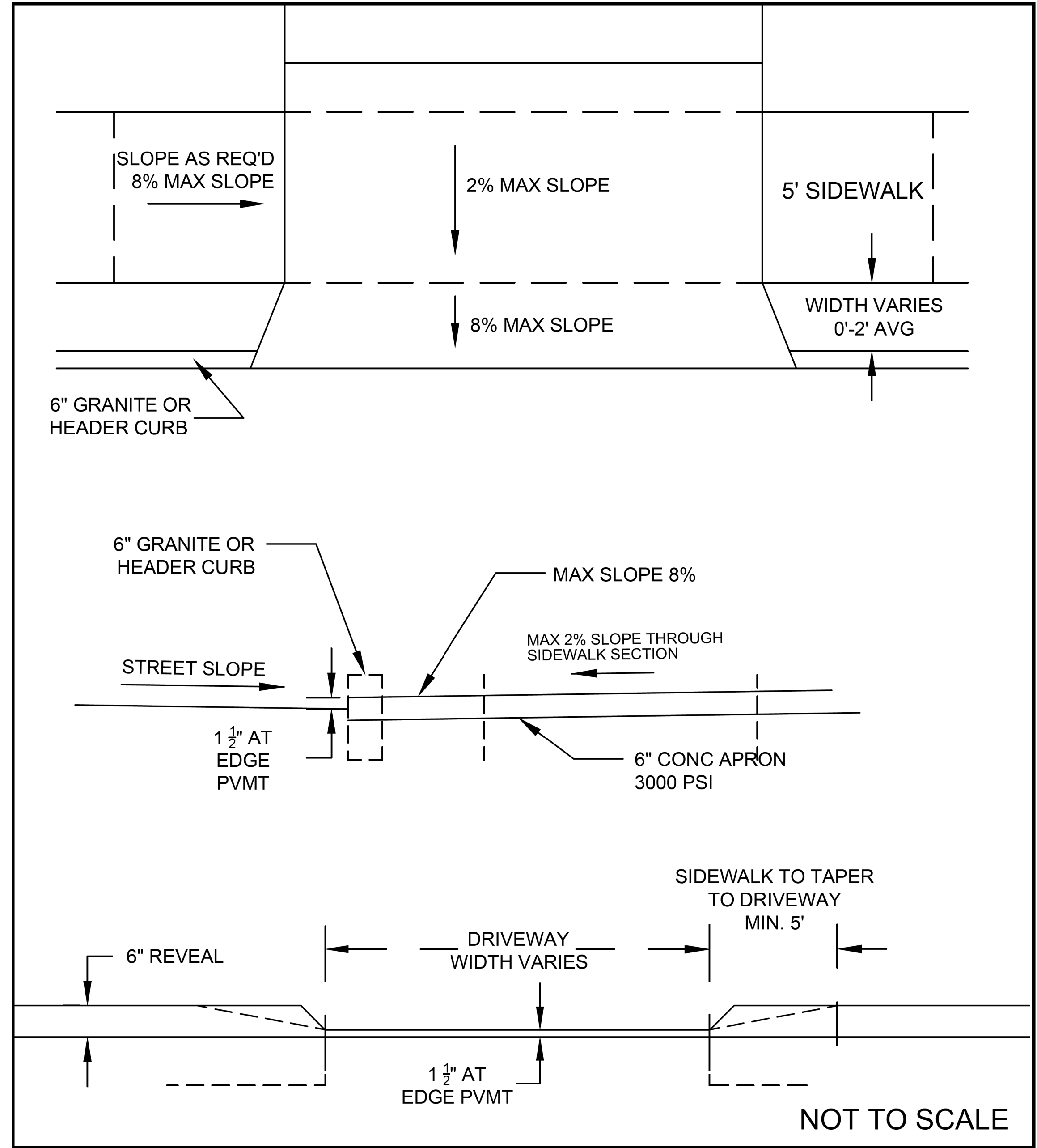
- GENERAL NOTE
- SEE GEORGIA STANDARD I033D AND I034D FOR ADDITIONAL INFORMATION ON CATCH BASIN TOP AND FORMING THE TRANSITION OF THE GUTTER.
  - CAST IRON MANHOLE RING AND COVER NOT REQUIRED.
  - CLASS "A" CONCRETE REQUIRED FOR ALL REINFORCED TOP SLABS. BOTTOM SECTION MAY BE CLASS "B" OR CLASS "A".
- MEASUREMENT: SPECIAL DESIGN SPILLWAY UNDER SIDEWALK WILL BE MEASURED BY THE UNIT.
- PAYMENT: SPECIAL DESIGN SPILLWAY UNDER SIDEWALK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH.
- PAYMENT WILL BE MADE UNDER:  
ITEM NO. 441, CONC. SPILLWAY, SPEC DES.



DETAIL OF SIDEWALK BRIDGE REINFORCED CONCRETE DETAIL

DATE		DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REVISION		SPECIAL DESIGN SPILLWAY UNDER SIDEWALK	
BY		NO SCALE	FEB. 2005

REVISION DATES		SPECIAL CONSTRUCTION DETAIL BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN	
CHECKED:	DATE:	CHECKED:	DATE:
BACKCHECKED:	DATE:	CORRECTED:	DATE:
CORRECTED:	DATE:	VERIFIED:	DATE:
VERIFIED:	DATE:	DRAWING No. 38-0001	



CITY OF BROOKHAVEN  
PUBLIC WORKS DEPARTMENT  
RESIDENTIAL DRIVEWAY DETAIL  
AT GRANITE HEADER CURB

**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING

NOT TO SCALE

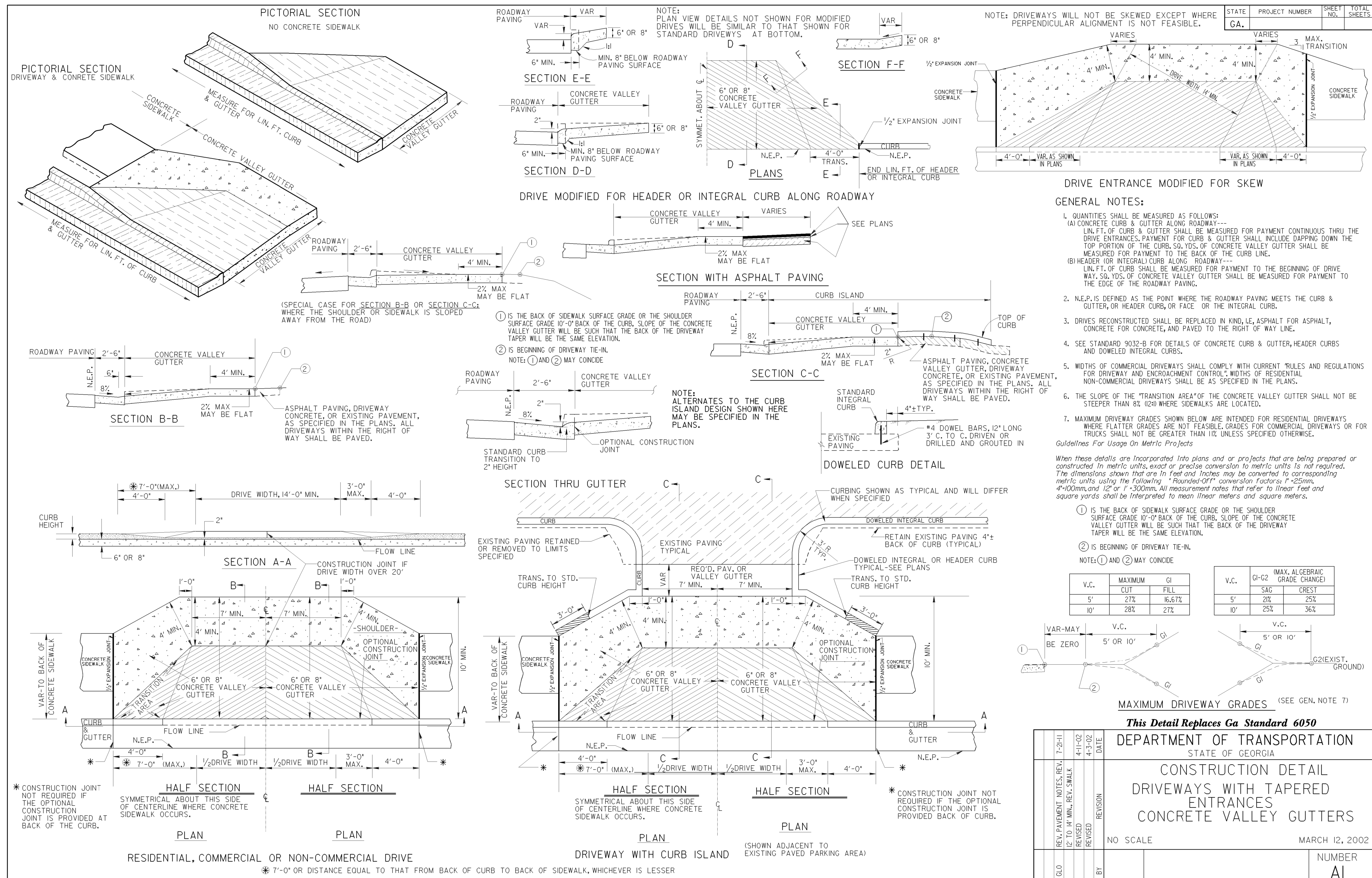
REVISION DATES

NO.	DATE	DESCRIPTION

SPECIAL CONSTRUCTION DETAIL  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	38-0002
CORRECTED:	DATE:	
VERIFIED:	DATE:	

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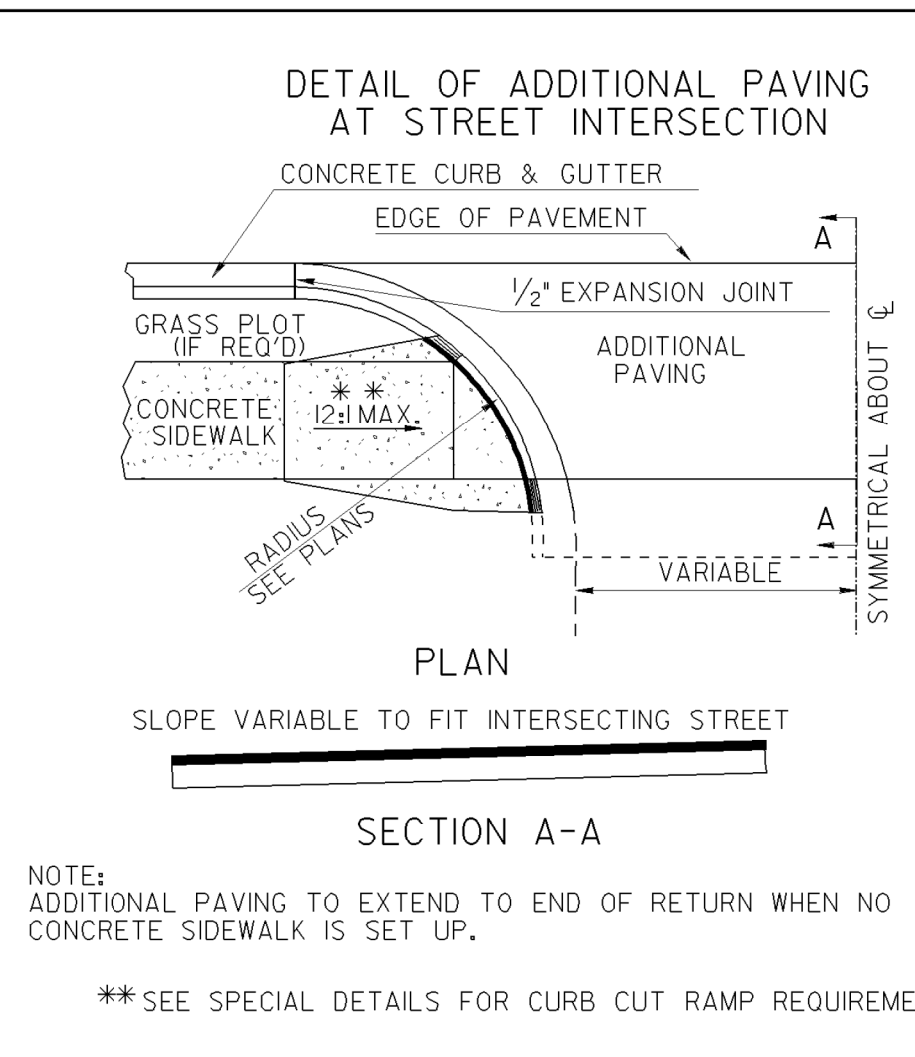
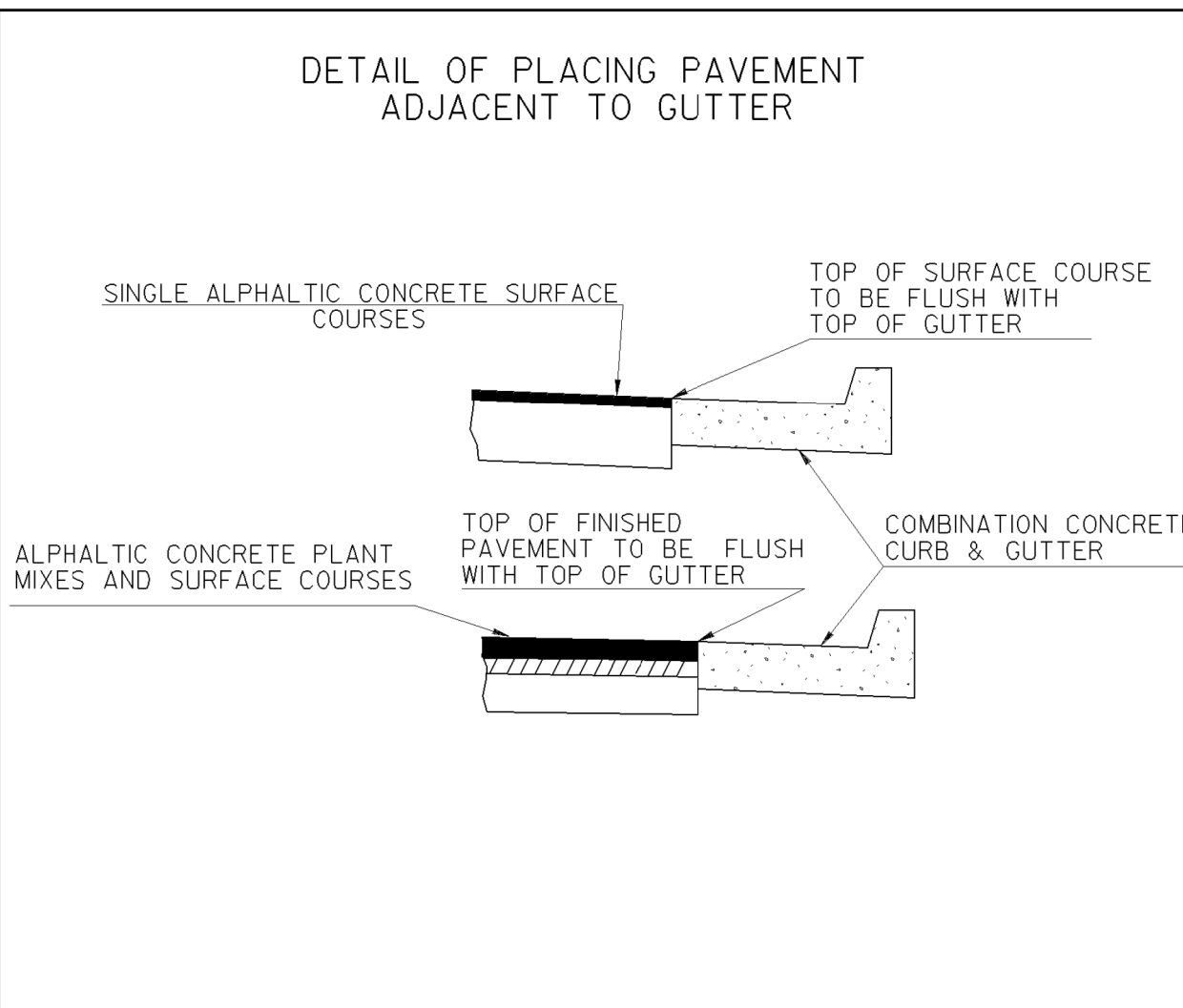
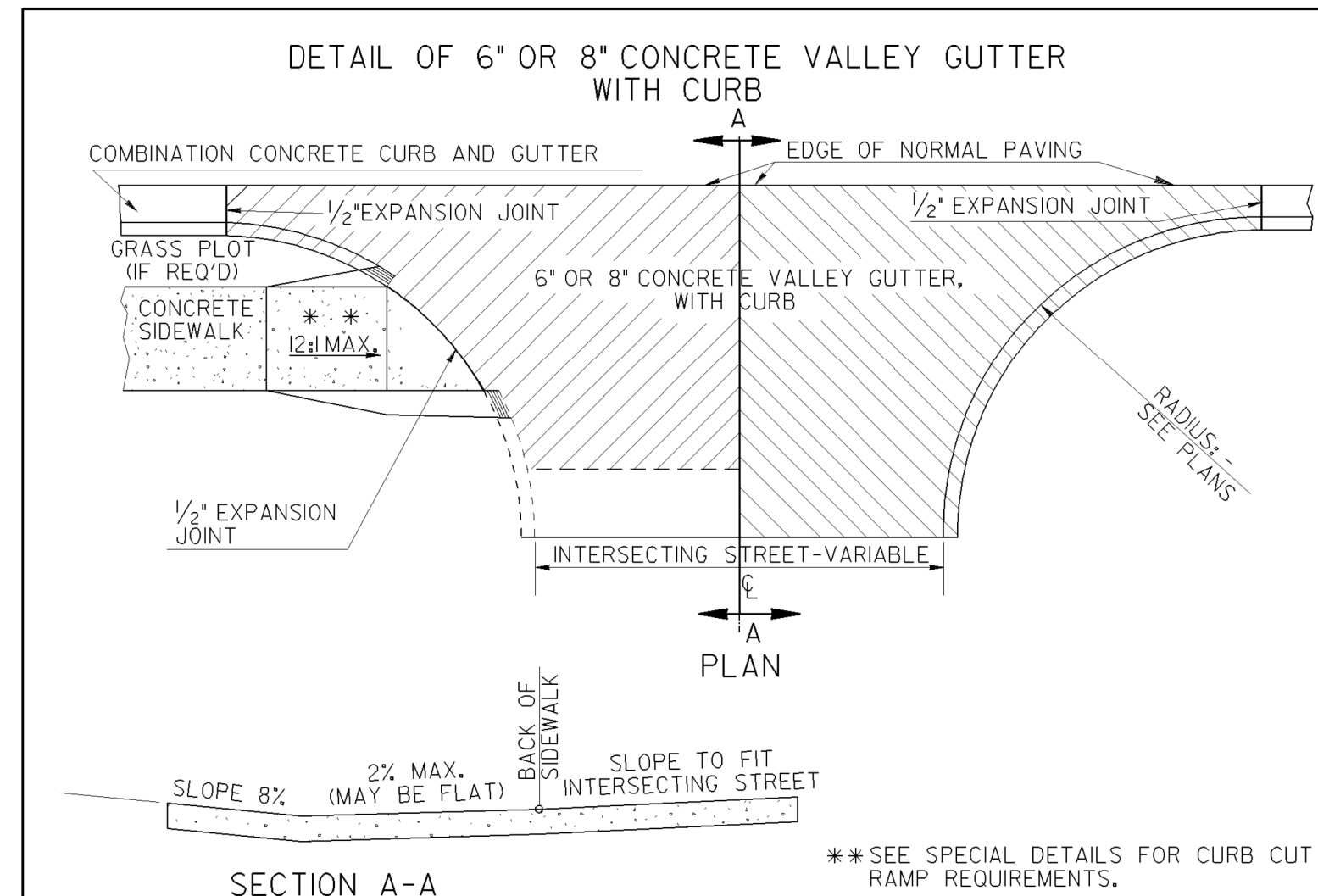


REVISION DATES

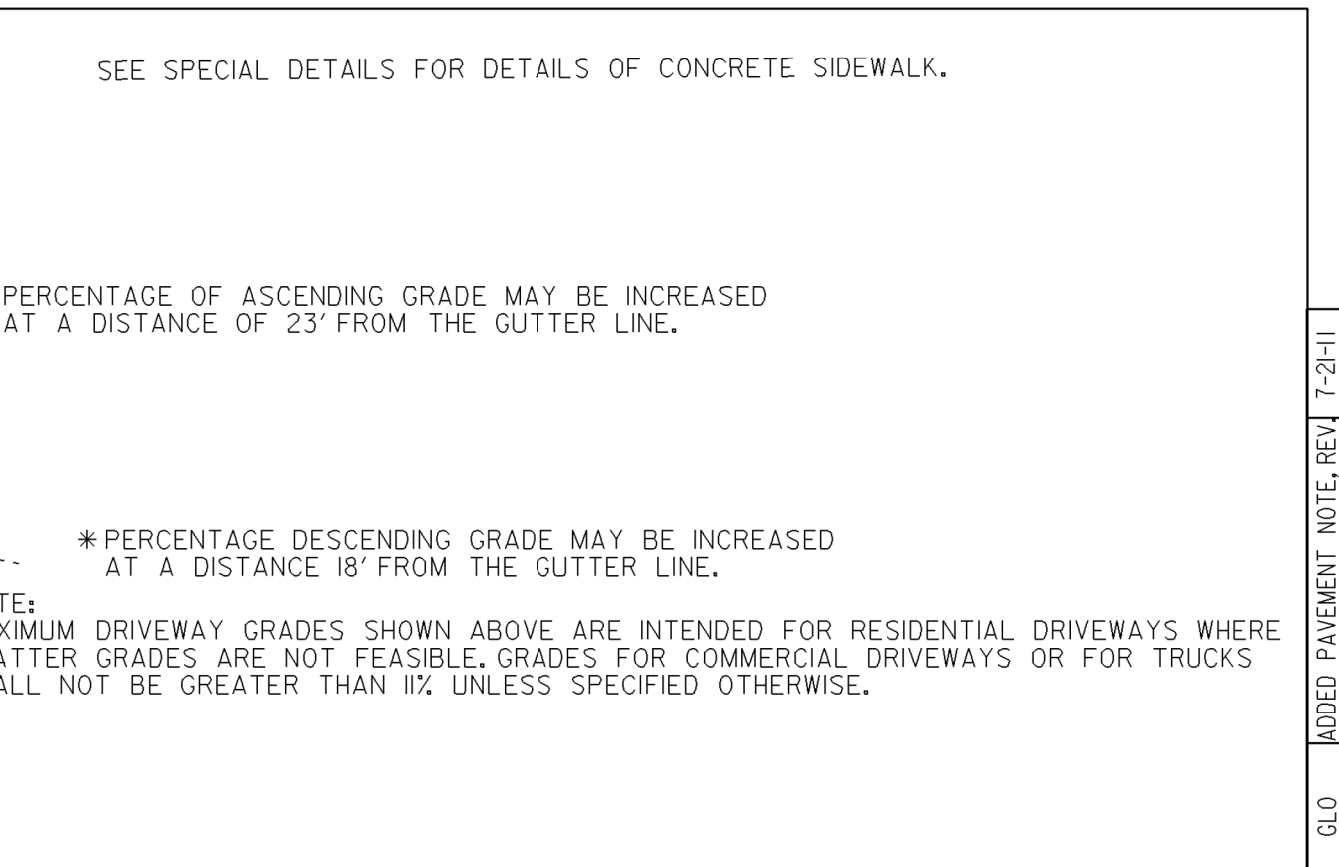
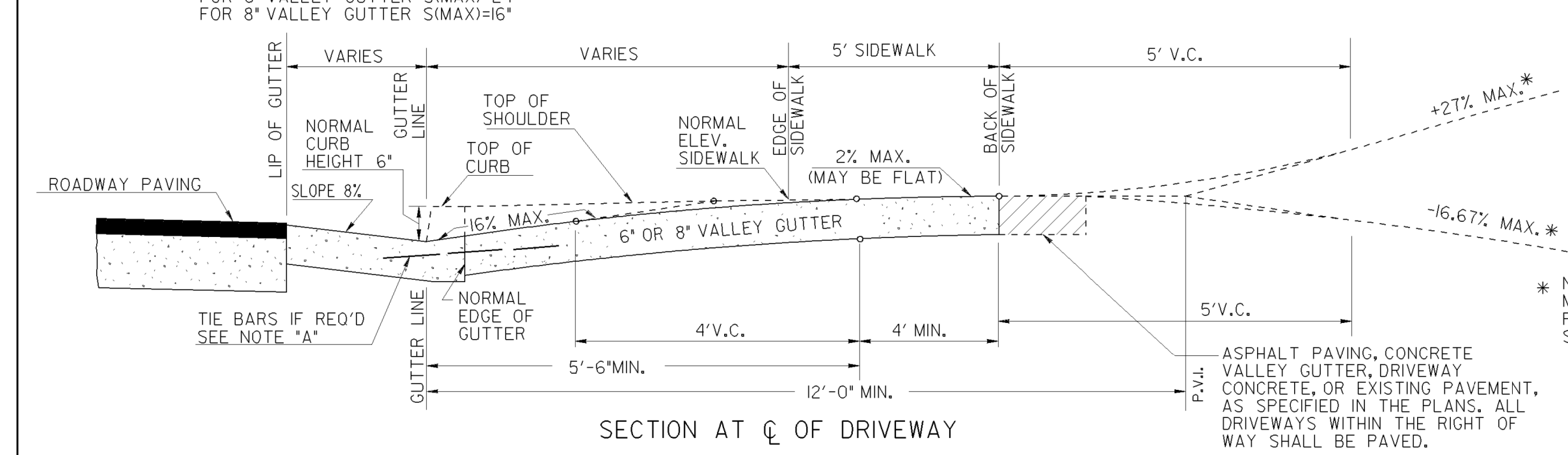
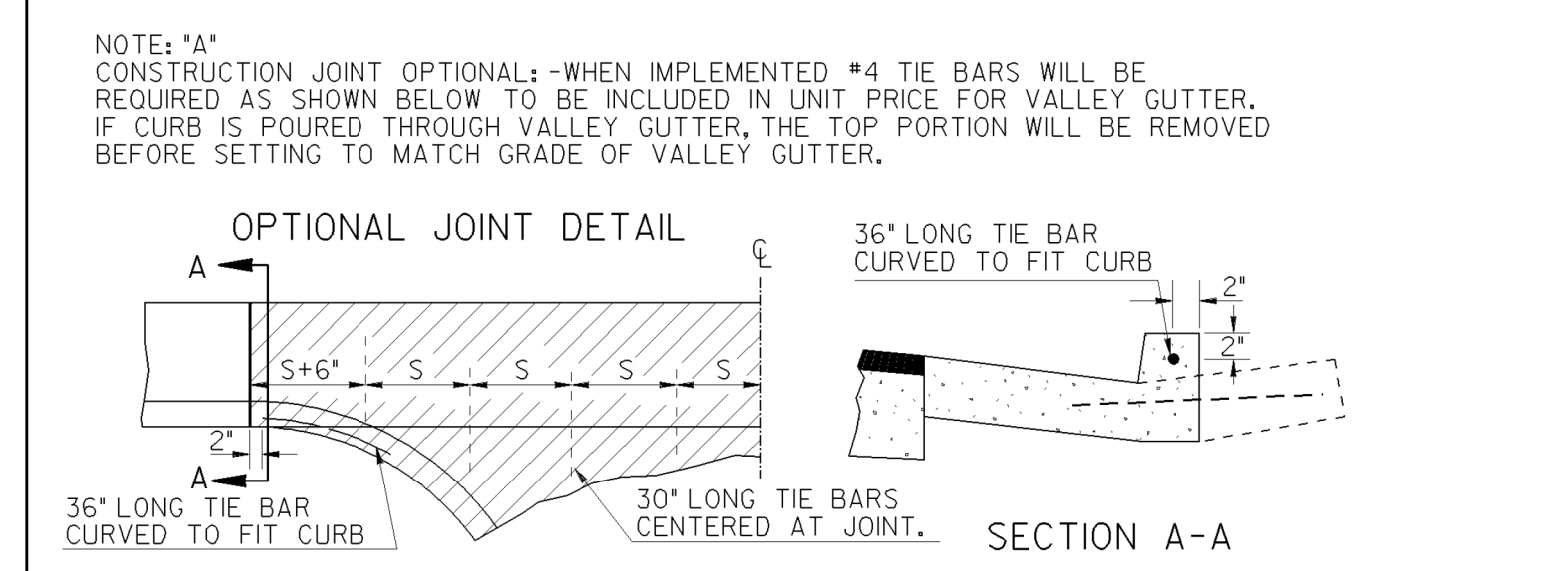
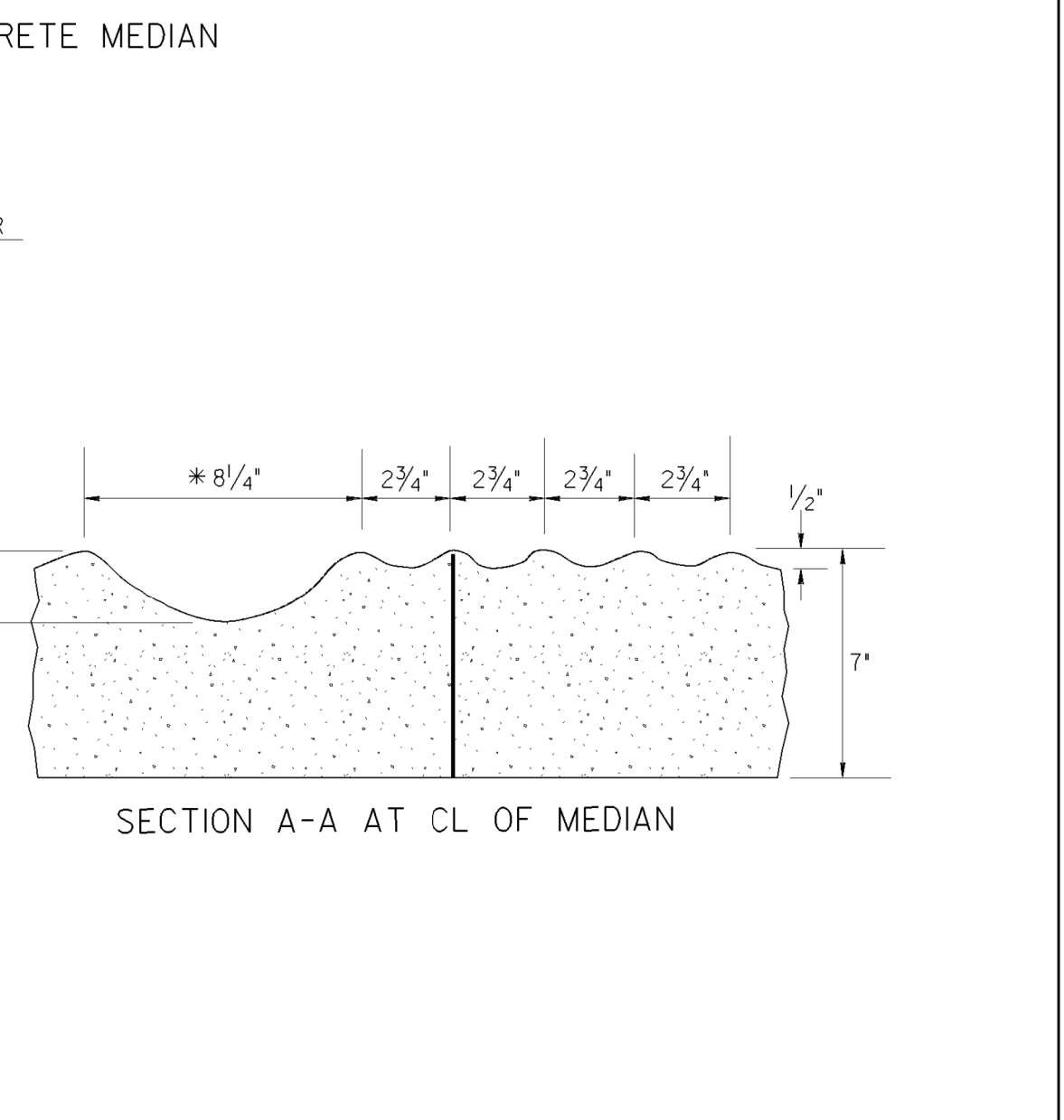
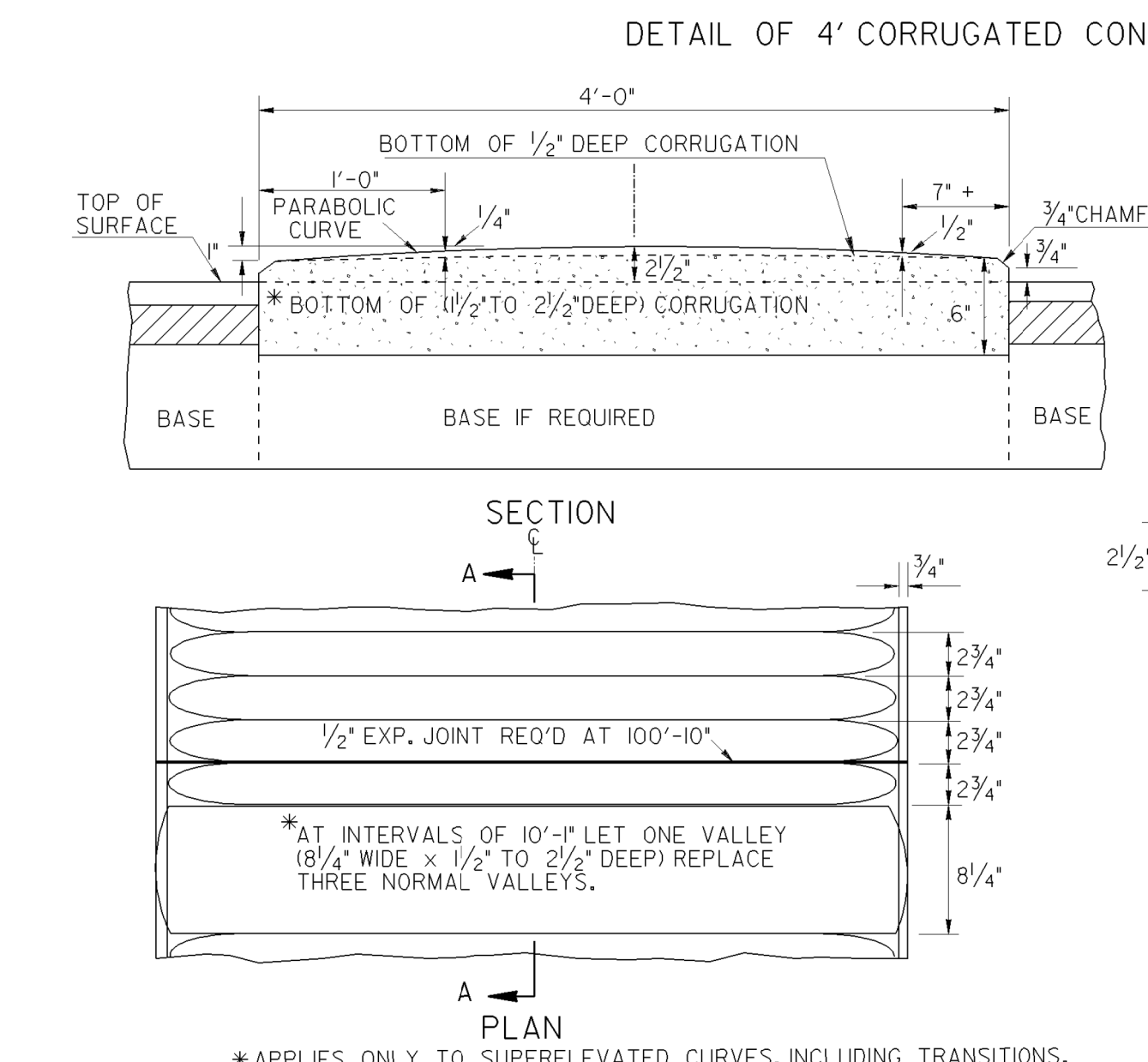
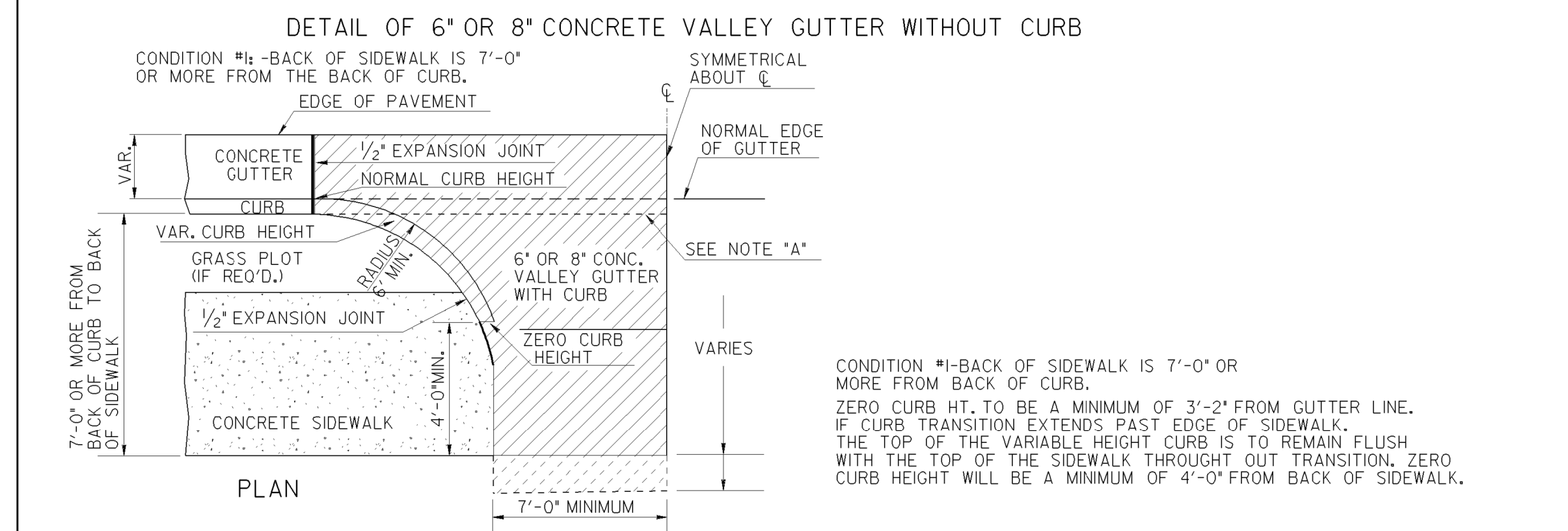
CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	40-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

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STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



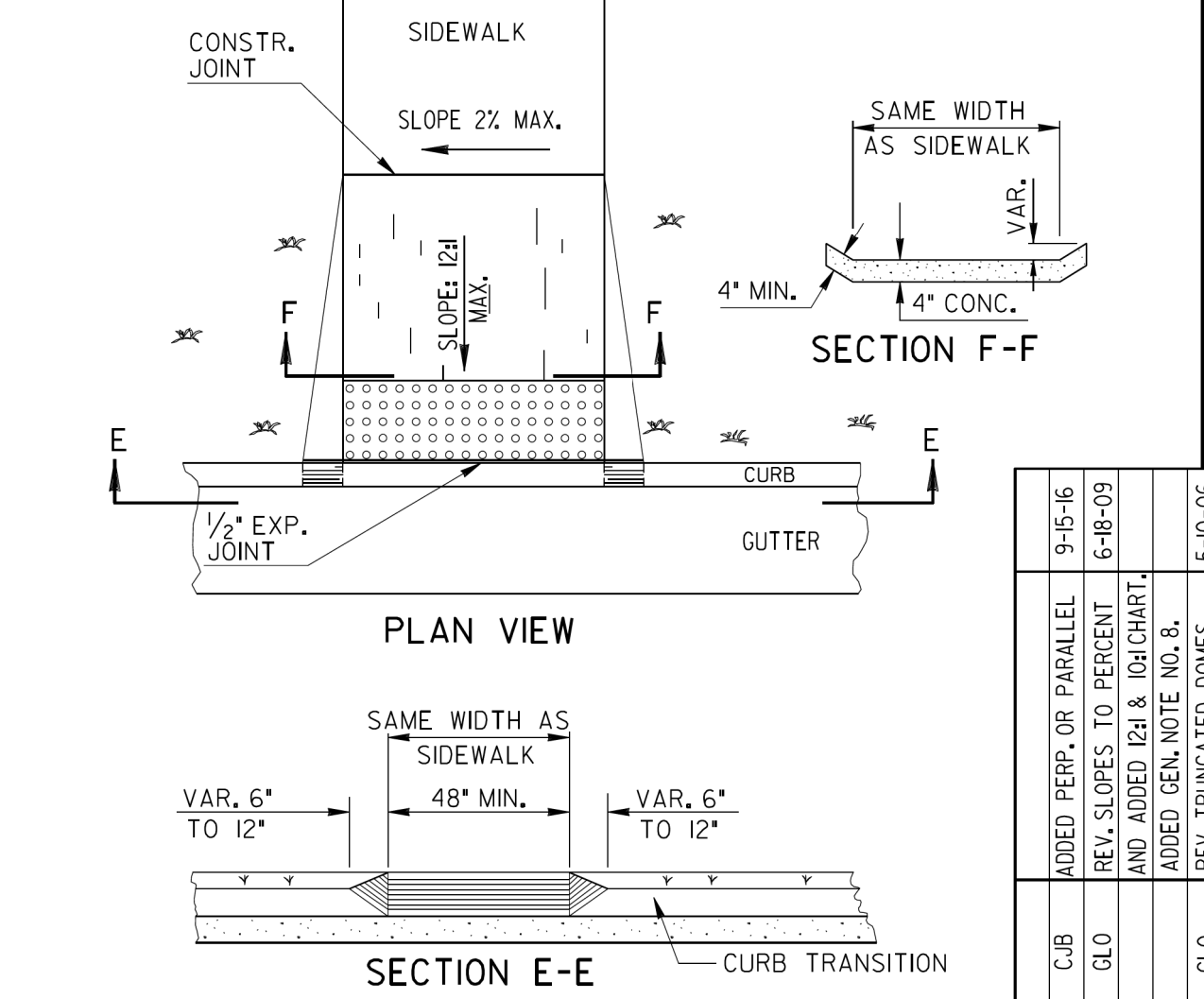
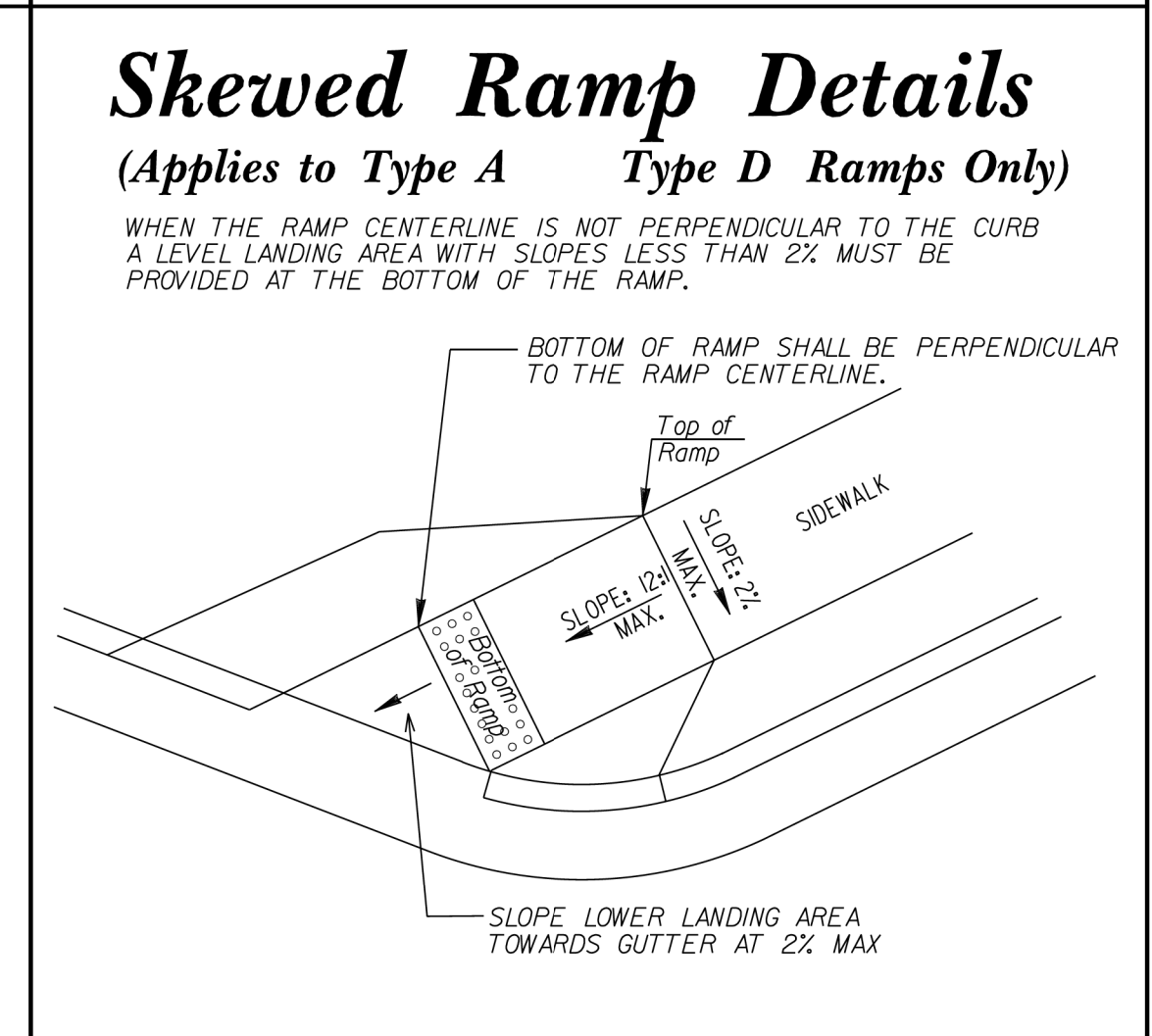
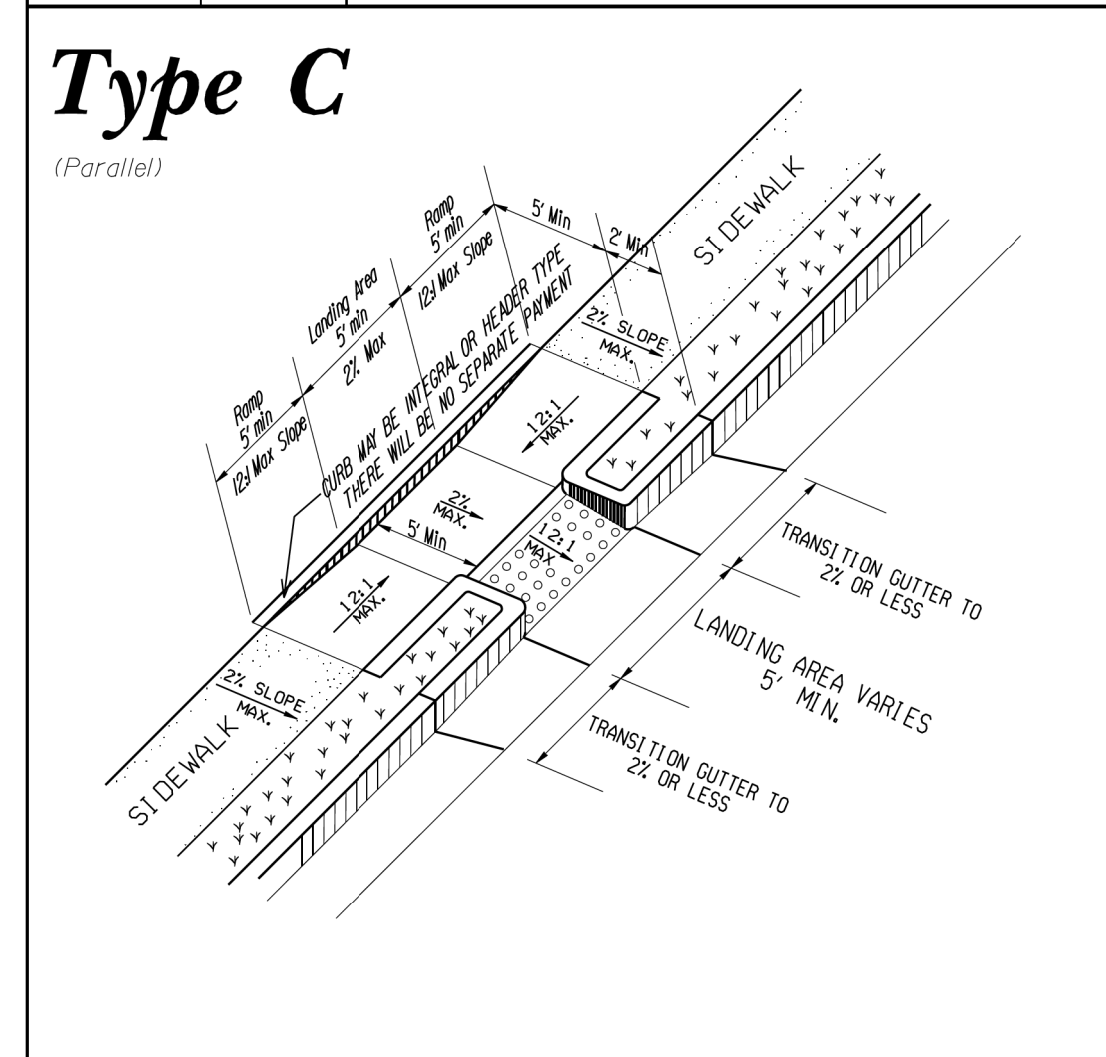
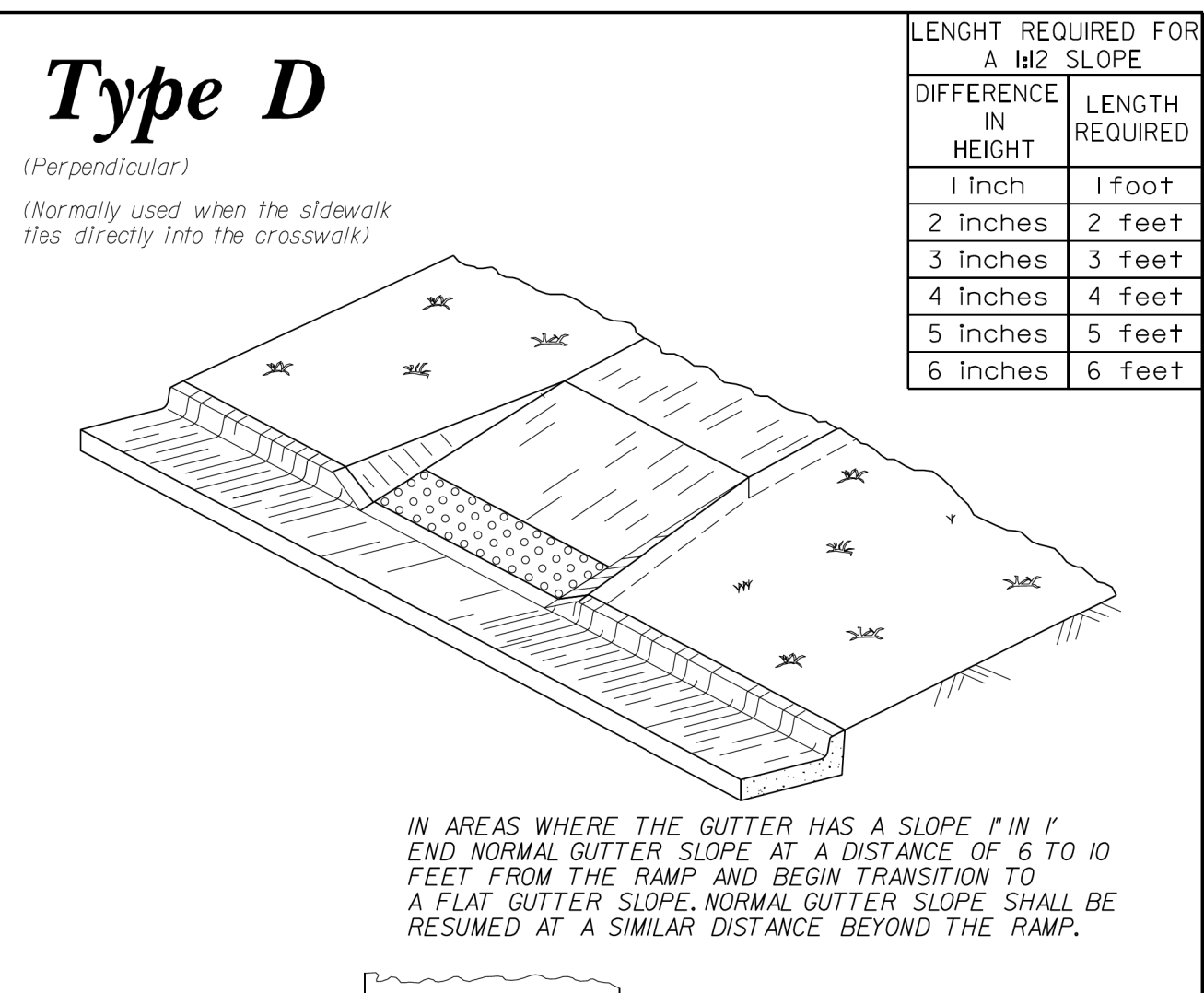
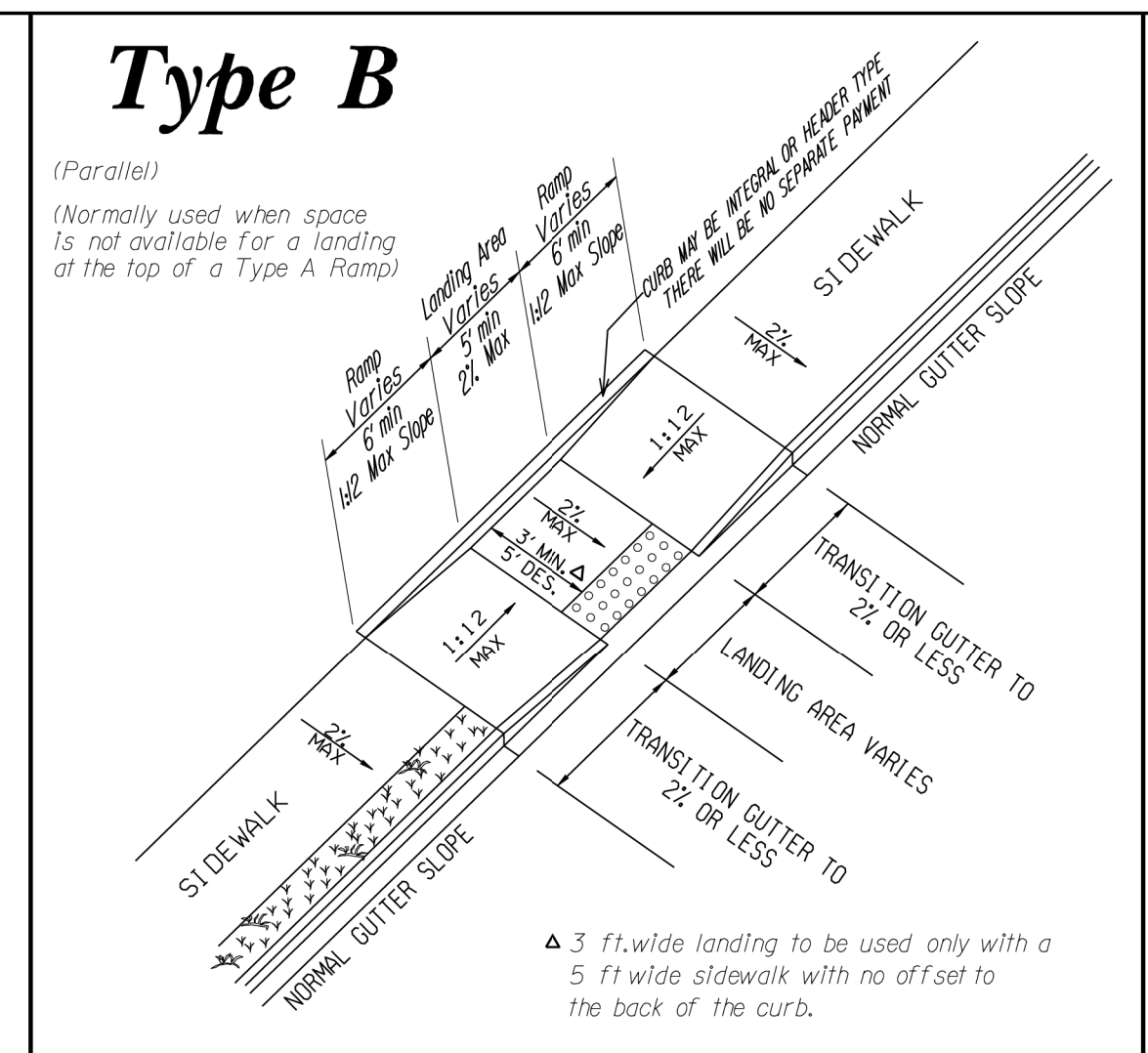
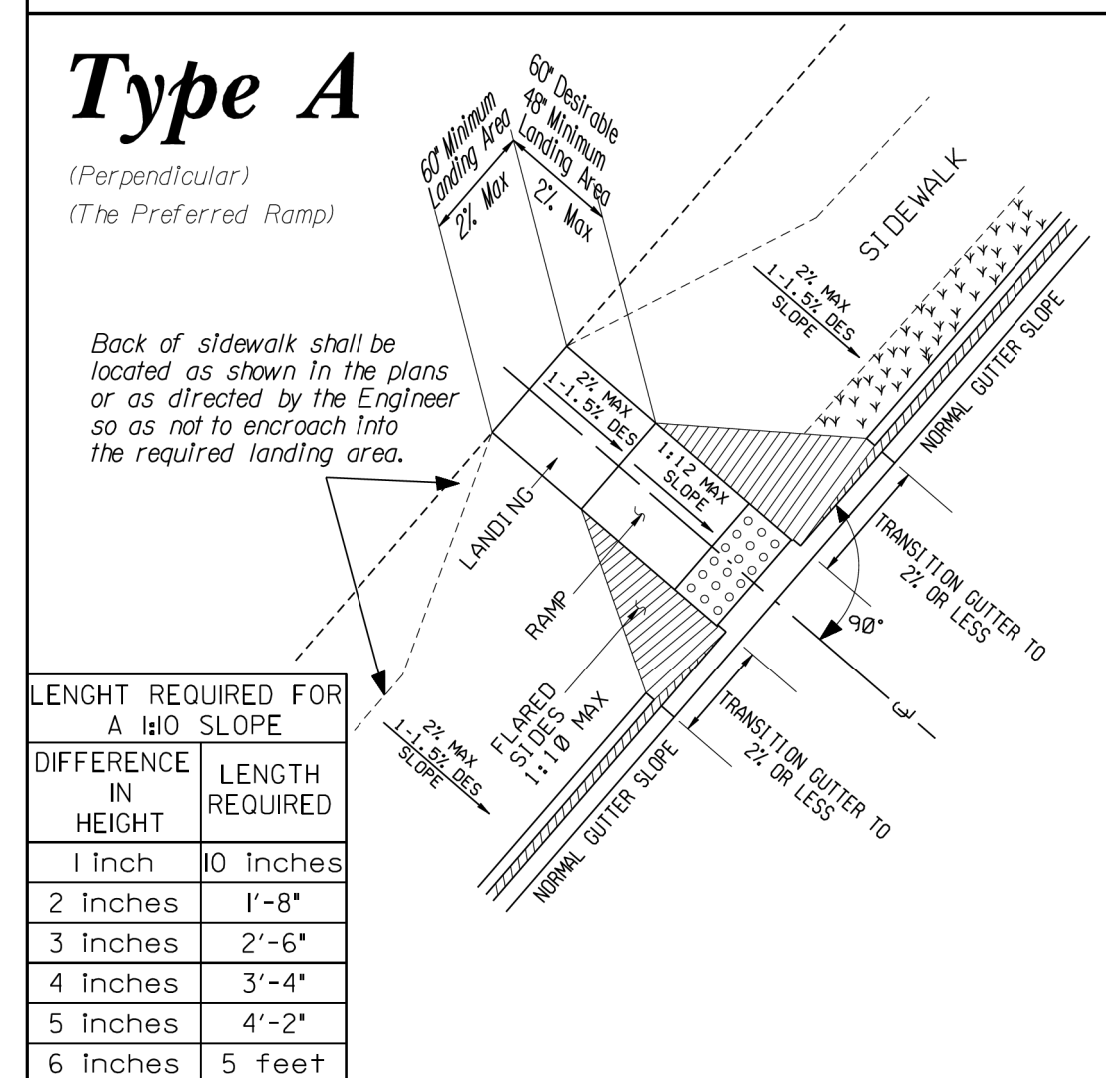
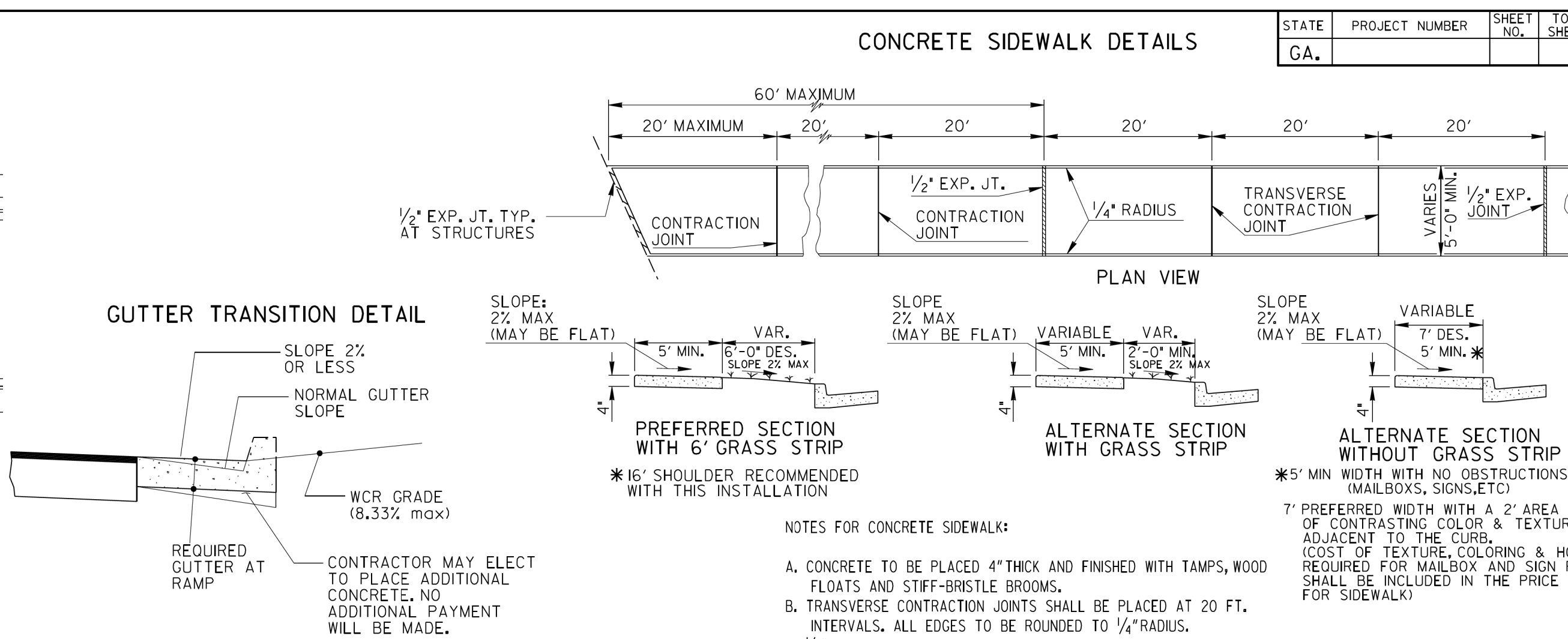
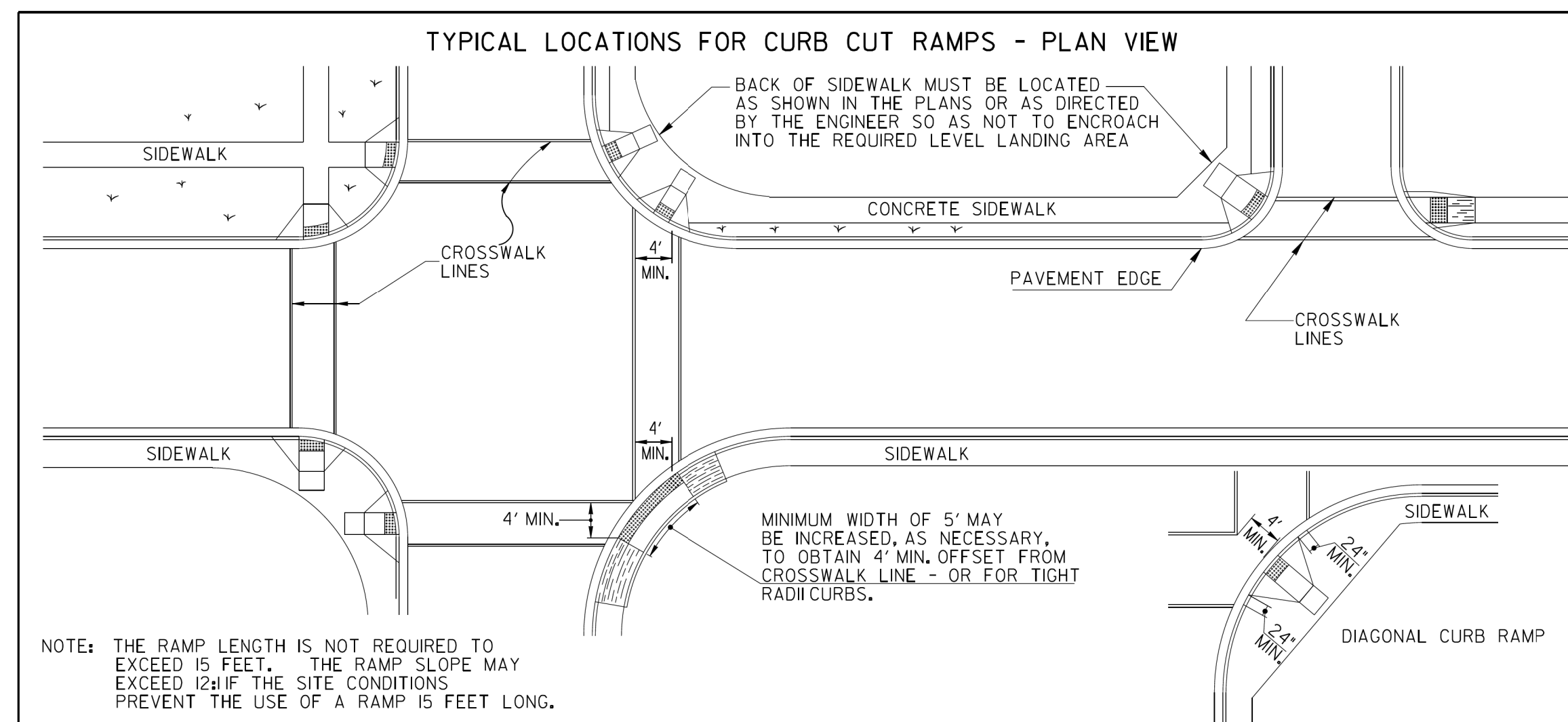
**This Detail Replaces Ga Standard 9031J**  
Guidelines For Usage On Metric Projects

When these details are incorporated into plans and or projects that are being prepared or constructed in metric units, exact or precise conversion to metric units is not required. The dimensions shown that are in feet and inches may be converted to corresponding metric units using the following: \*Rounded-Off\* conversion factors: 1" = 25mm, 4" = 100mm, and 12" or 1' = 300mm. All measurement notes that refer to linear feet and square yards shall be interpreted to mean linear meters and square meters.

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA			
<b>CONSTRUCTION DETAIL</b>			
CONCRETE VALLEY GUTTER AT STREET INTERSECTION 6' OR 8' CONCRETE VALLEY GUTTER AT DRIVE PLACING PAVEMENT ADJACENT TO GUTTER ADDITIONAL PAVING AT STREET INTERSECTION 4' CORRUGATED CONCRETE MEDIAN			
NO SCALE	MARCH 12, 2002		
NUMBER A2			

7/22/2011 2:16:33 PM \\G0DOT-D5N1\G0PLOT\G0F\G0\_K1p8000.qcf gowens V:\GARY\ra\15ad A-1, A-2\A-2.pr1

REVISION DATES		CONSTRUCTION DETAILS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN	
CHECKED:	DATE:	CHECKED:	DATE:
BACKCHECKED:	DATE:	CORRECTED:	DATE:
CORRECTED:	DATE:	VERIFIED:	DATE:
VERIFIED:	DATE:	DRAWING No. <b>40-0002</b>	



REVISION	DATE	DESCRIPTION
9-15-16	6-18-09	ADDED PERP. OR PARALLEL REV. SLOPES TO PERCENT AND ADDED I24 & I04 CHART.
7-29-02	5-29-02	REVISED
5-23-02	5-13-02	REVISED
4-29-02	4-11-02	REVISED
4-3-02	3-28-02	REVISED

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

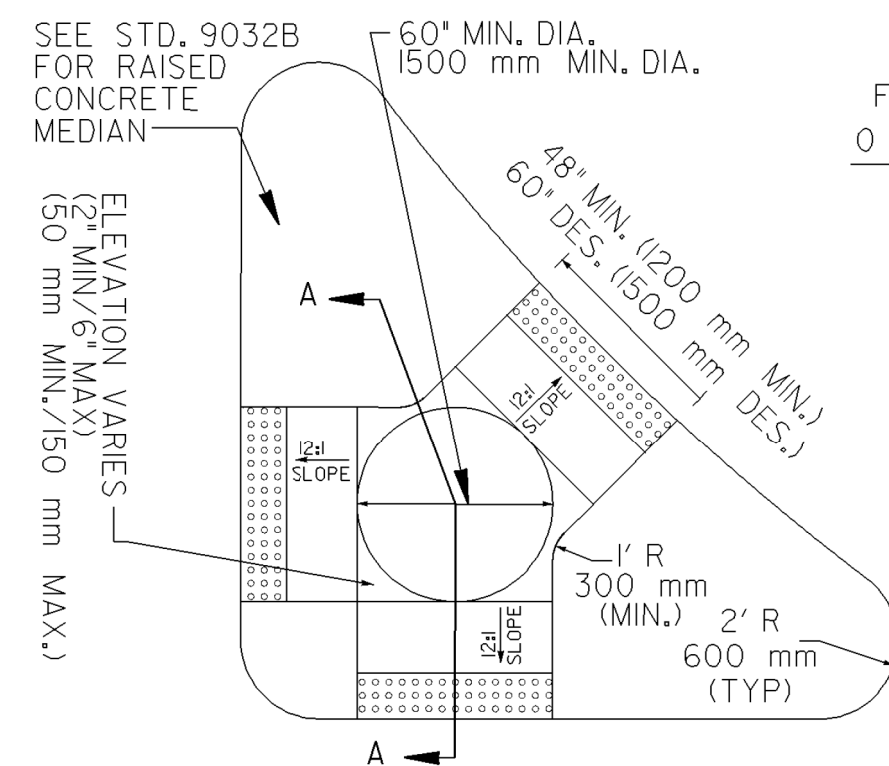
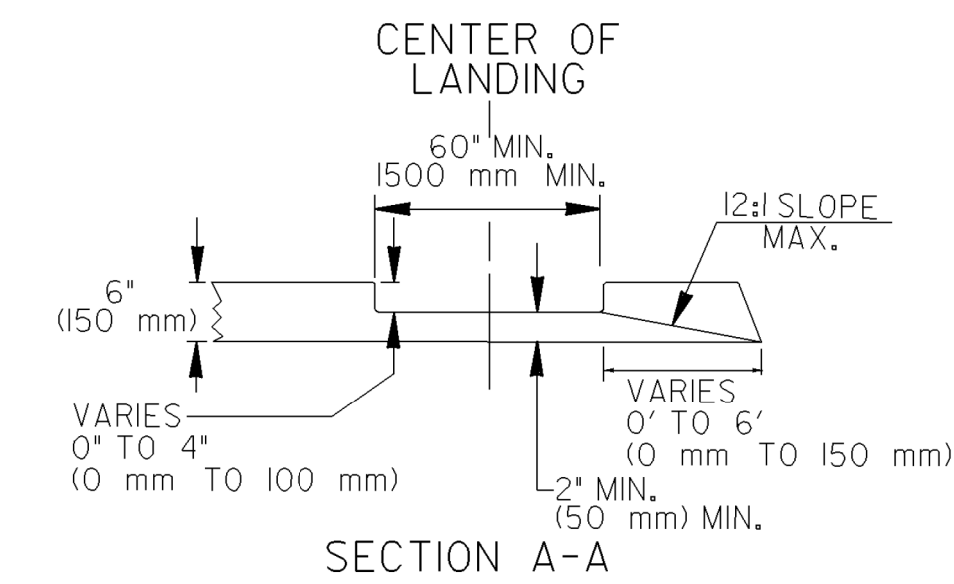
SPECIAL DETAIL  
CONCRETE SIDEWALK DETAILS  
CURB CUT (WHEELCHAIR) RAMPS

NO SCALE MARCH 12, 2002

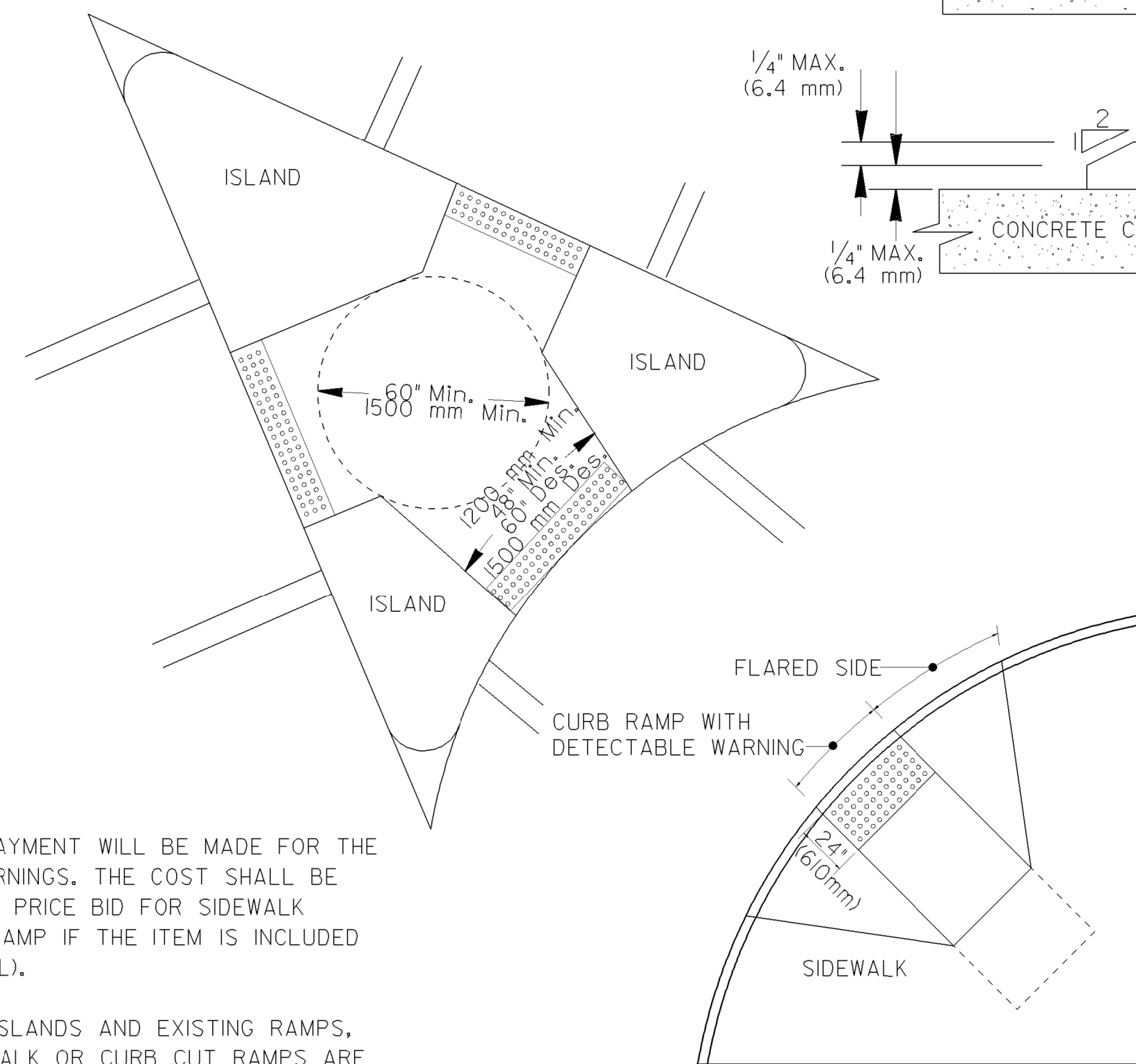
NUMBER  
A3



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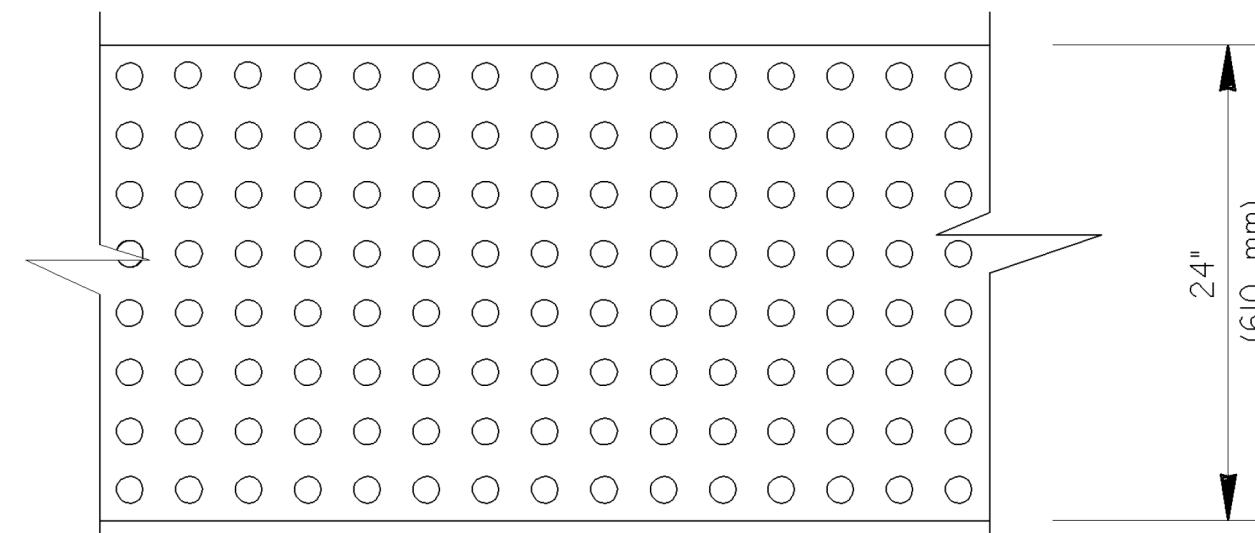
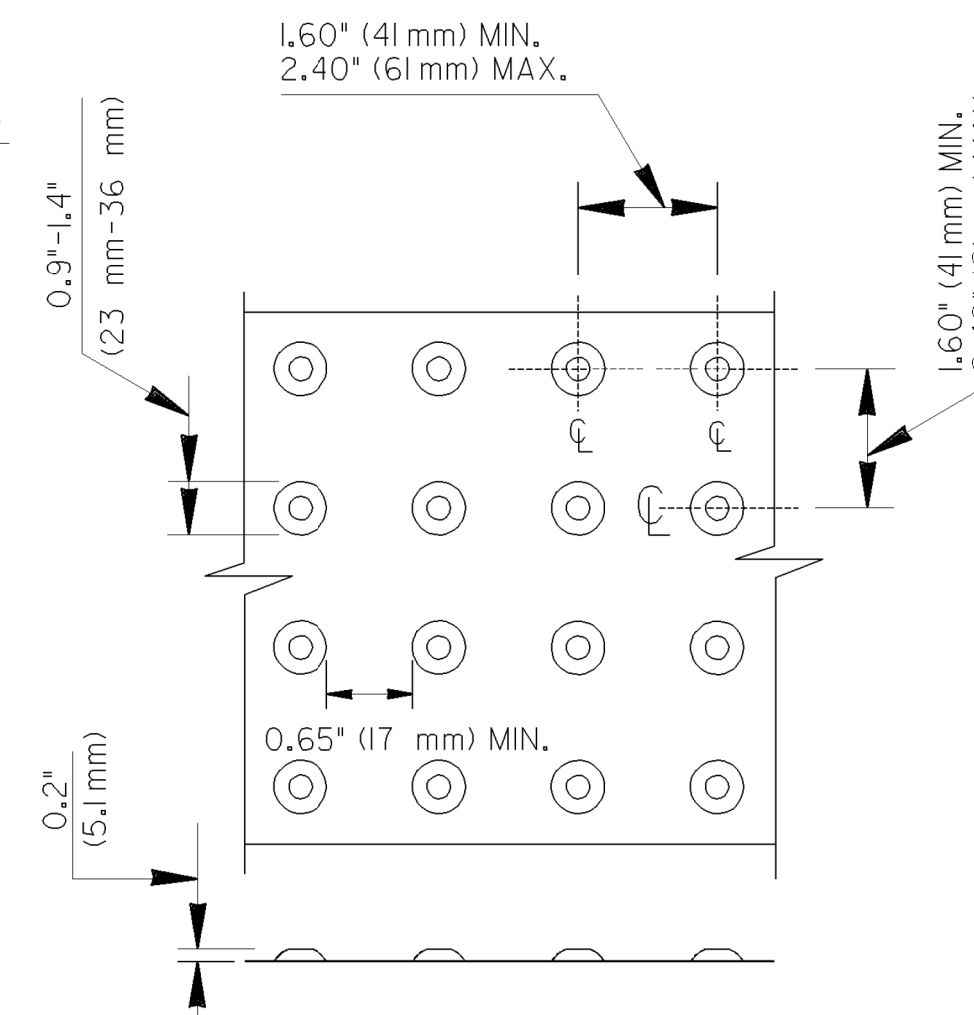
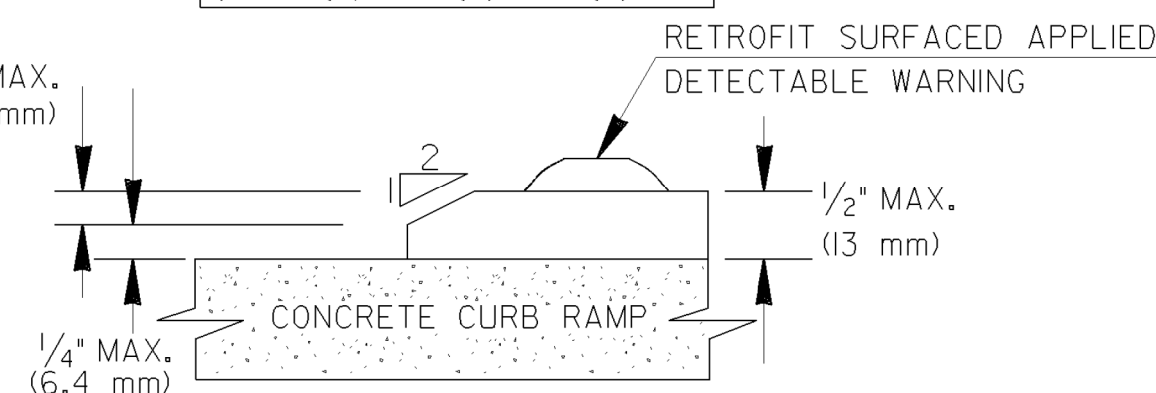
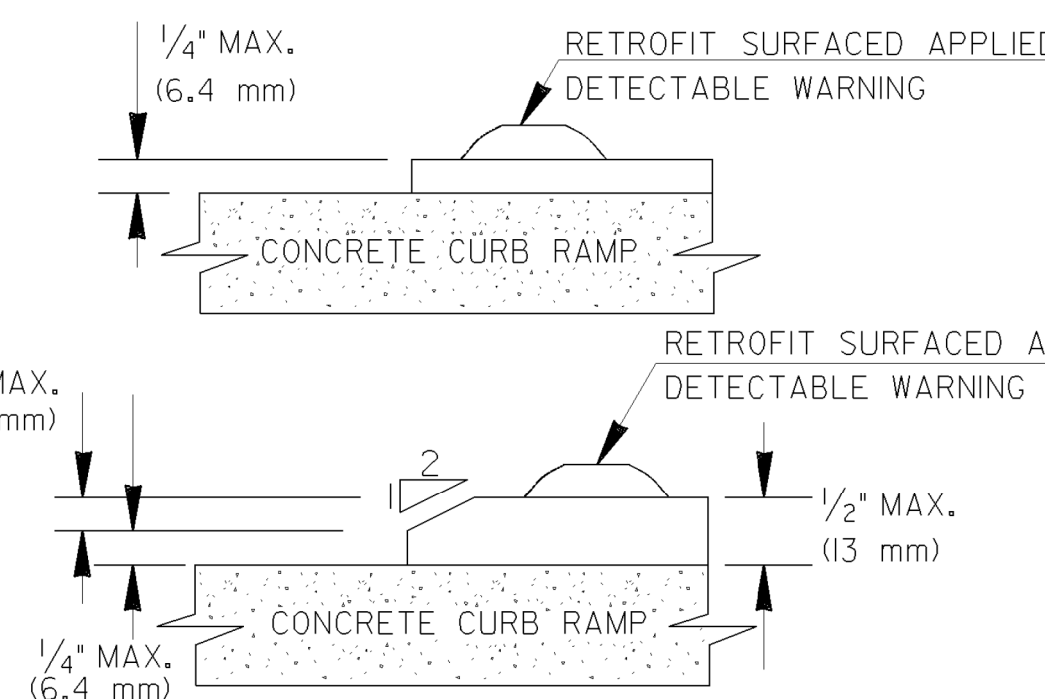
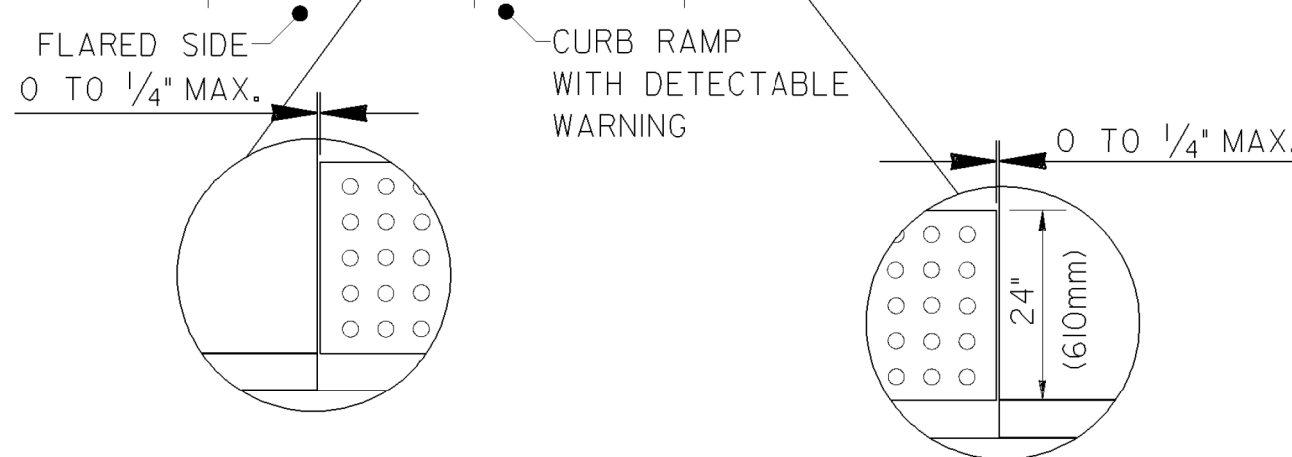
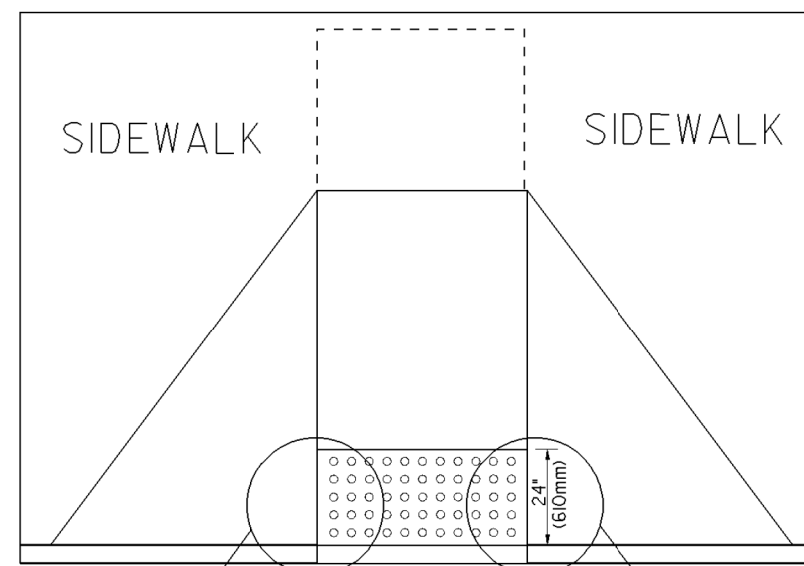
CONCRETE ISLAND WITH ELEVATED CUT THROUGH



DETAIL FOR DETECTABLE WARNING AT CUT-THRU CONCRETE ISLAND

NO SEPARATE PAYMENT WILL BE MADE FOR THE DETECTABLE WARNINGS. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR SIDEWALK (OR CURB CUT RAMP IF THE ITEM IS INCLUDED IN THE PROPOSAL).

FOR CUT-THRU ISLANDS AND EXISTING RAMPS, WHERE NO SIDEWALK OR CURB CUT RAMPS ARE IN THE PROPOSAL. THE COST OF THE DETECTABLE WARNINGS SHALL BE INCLUDED IN THE OVERALL BID PRICE SUBMITTED.



SIZE: DETECTABLE WARNINGS SHALL BE 24 INCHES (610 mm) IN THE DIRECTION OF PEDESTRAIN TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE.  
 LOCATION: THE DETECTABLE WARNING SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE OR OTHER POTENTIAL HAZARD, SUCH AS A REFLECTIVE POOL EDGE OR THE DYNAMIC ENVELOPE OF RAIL OPERATIONS.  
 DOME SIZE AND SPACING: TRUNCATED DOMES SHALL HAVE A BASE DIAMETER OF 0.9 INCH TO 1.4 INCH (23mm-36mm) AT THE BOTTOM, A DIAMETER OF 0.45 INCH TO 0.9 INCH (11mm-23mm) AT THE TOP, THE TOP DIAMETER SHALL BE A MINIMUM OF 50% AND A MAXIMUM OF 65% OF THE BASE DIAMETER, A HEIGHT OF 0.2 INCH (5.1mm) AND A CENTER-TO-CENTER SPACING OF 2.40 INCHES (61mm) DESIRABLE 1.60 INCHES (41mm) MINIMUM MEASURED ALONG ONE SIDE OF A SQUARE ARRANGEMENT. DOMES SHALL HAVE A SQUARE ARRANGEMENT. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.  
 VISUAL CONTRAST: DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH THE ADJACENT WALKING SURFACE EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT. THE MATERIAL USED TO PROVIDE VISUAL CONTRAST SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING SURFACE.

**MATERIALS:**

**NEW CONSTRUCTION**

THE DETECTABLE WARNINGS SHALL BE MADE OF MATERIALS SPECIFIED ON OPL 87.

**RETROFIT OF EXISTING RAMPS**

SURFACED APPLIED MATERIALS WILL ONLY BE APPROVED TO BE USED ON EXISTING WHEELCHAIR RAMPS.

**INSTALLATION:**

BRICK PAVERS SHALL BE SET IN A WET MORTAR BED. THE BED SHALL BE PLACED ON CONCRETE. THE CONCRETE SHALL BE A MINIMUM OF 4" THICK.

CERAMIC TILE SHALL BE EPOXIED IN PLACE OR SET IN A WET MORTAR BED. MANUFACTURER RECOMMEND ADHESIVE OR FASTENER SHALL BE USED IN THE INSTALLATION.

ALL OTHER MATERIALS SHALL BE INSTALLED ACCORDING TO MANUFACTURERS DETAILS OR INSTRUCTION.

**GENERAL NOTES:**

- RETROFIT SURFACED APPLIED MATERIALS ONLY:
1. CHANGES IN LEVEL OF 1/4" (6.4 mm) HIGH MAXIMUM SHALL BE PERMITTED VERTICALLY ON SURFACED APPLIED MATERIALS.
  2. CHANGES IN LEVEL BETWEEN 1/4" (6.4 mm) HIGH MINIMUM AND 1/2" (13mm) HIGH MAXIMUM SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 2:1.

6-18-09	ADDED RETROFIT DETAIL AND ADDED ALT. RAMP DETAIL AND GEN. NOTES	10-2-06	ADDED TOLERANCE TO DTL.	5-10-06	REVISED TRUNCATED DOMES AND NOTES.	11-14-02	REVISED	7-29-02	DATE	DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA
SPECIAL DETAIL										
DETECTABLE WARNING SURFACE TRUNCATED DOME SIZE, SPACING AND ALIGNMENT REQUIREMENTS										
NO SCALE										
MARCH 12, 2002										
NUMBER A4										

**REVISION DATES**


**CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN**

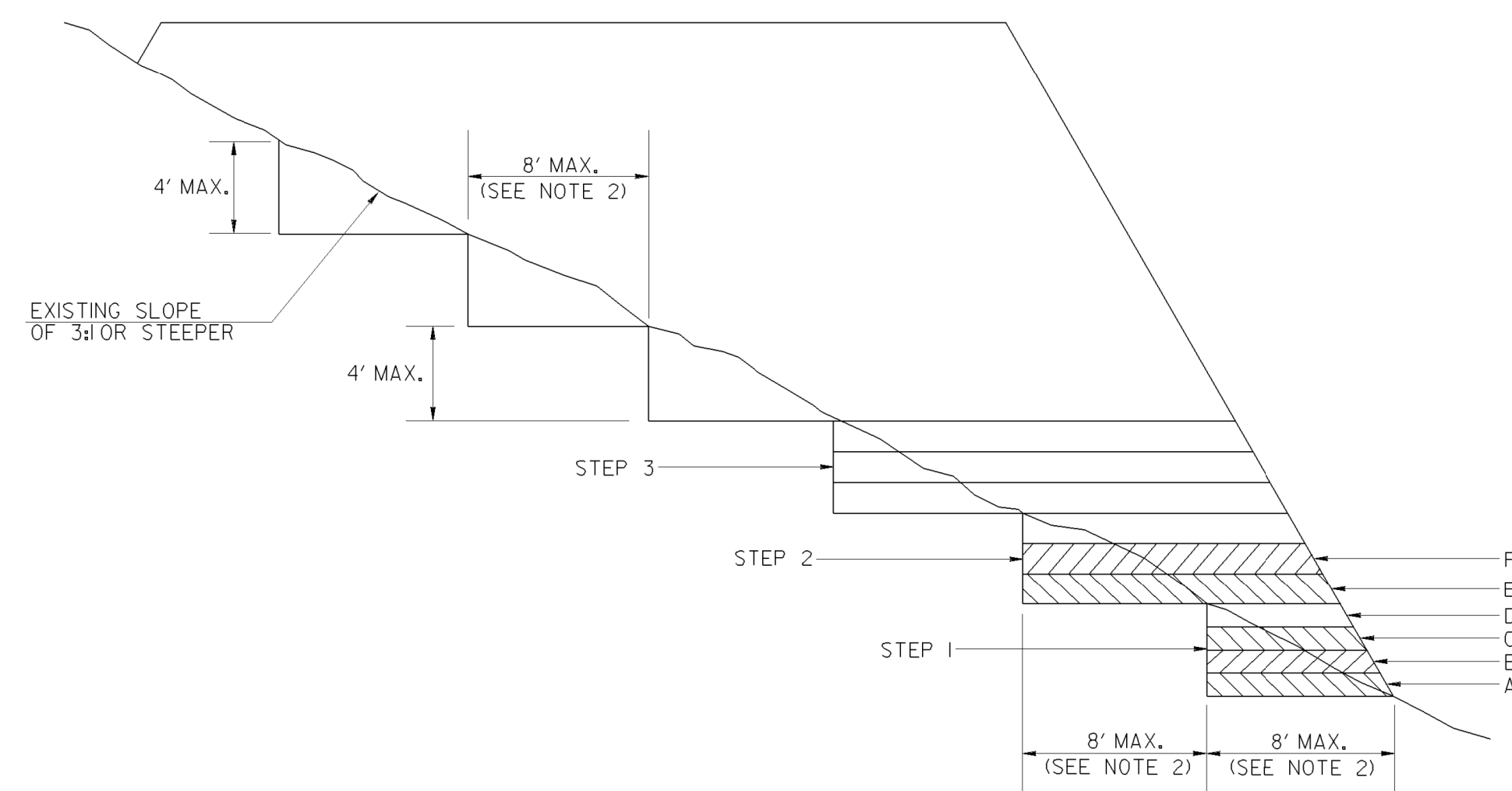
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CORRECTED:	DATE:	
VERIFIED:	DATE:	

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STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

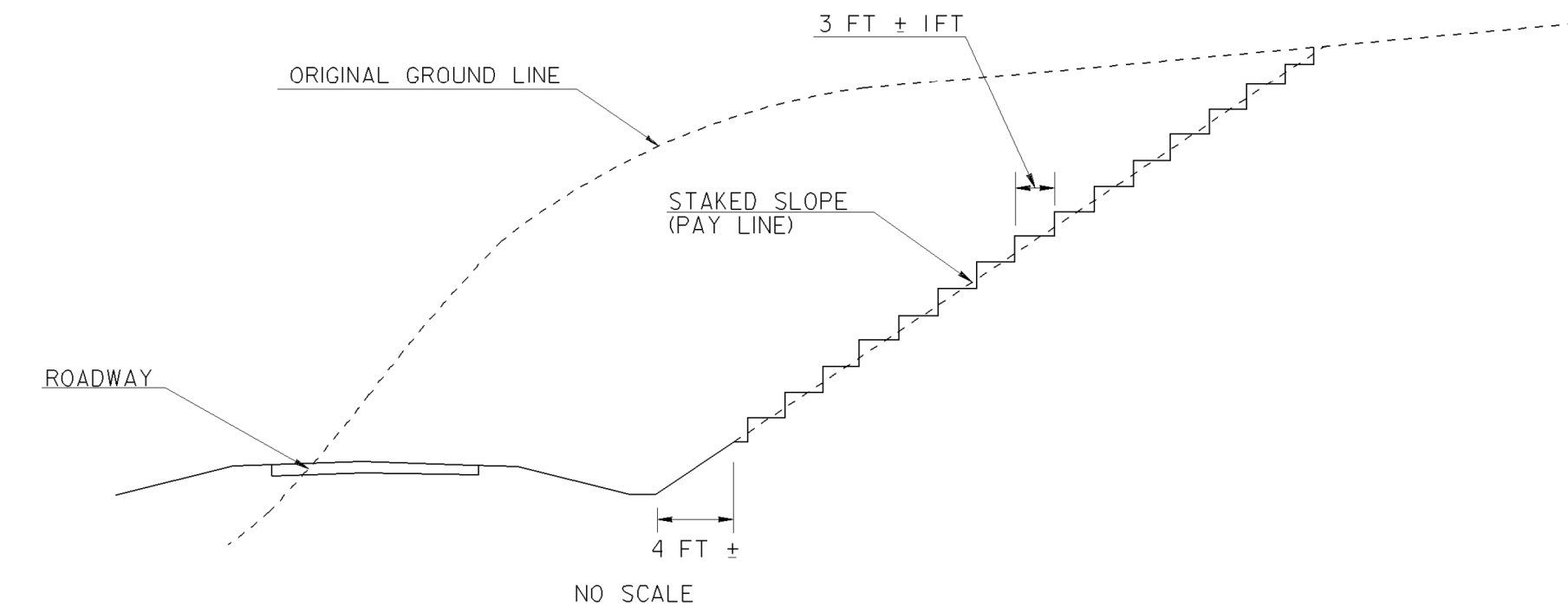
BENCHING DETAIL



NOTES :

- WHERE THE EMBANKMENT IS TO BE PLACED ON A HILLSIDE OR ANOTHER EXISTING EMBANKMENT HAVING A SLOPE OF 3 TO 1 OR STEEPER, THE FOUNDATION MUST BE BENCHING WHILE THE EMBANKMENT IS BEING MADE.(SEE DIAGRAM ABOVE)
- THE DIAGRAM SHOWS THAT BEFORE LAYER 'A' IS PLACED THE FIRST STEP (1) IS CUT INTO THE SLOPE A MAXIMUM DISTANCE OF ABOUT 8 FEET (ABOUT 3/4 THE WIDTH OF THE TYPICAL D-8 BULLDOZER BLADE). SUCCESSIVE LAYERS B, C, AND D ARE THEN PLACED BEFORE LAYER 'E' IS PLACED, THE SECOND STEP IS CUT 8 FEET INTO THE SLOPE AND SUCCESSIVE LAYERS ARE AGAIN PLACED. IF IT IS ANTICIPATED THAT THE VERTICAL PART OF THE STEP WILL EXCEED 4 FEET IF A 8 FEET HORIZONTAL CUT IS MADE, THEN THE ACTUAL CUT STOPS WHEN THE VERTICAL PART REACHES A MAXIMUM OF 4 FEET ALLOWING THE HORIZONTAL DISTANCE TO VARY.
- THE PROCESS OF BENCHING IS CONSIDERED INCIDENTAL TO THE ITEM OF UNCLASSIFIED EXCAVATION AND BORROW OR GRADING COMPLETE IN CONSTRUCTION OF THE EMBANKMENT AND NO ADDITIONAL MEASUREMENT OF QUANTITY OR PAYMENT WILL BE MADE FOR BENCHING.

SERRATED SLOPE DETAIL



NOTES :

- SLOPES STEEPER THAN 3:1 SHALL BE SERRATED.
- WIDTH OF STEP SHALL BE 3 FT +/- IFT.
- HEIGHT OF STEP IS A FUNCTION OF WIDTH AND STAKED SLOPE.
- TREAD OF STEP SHALL BE APPROXIMATELY HORIZONTAL.
- SERRATED SLOPES SHALL BE USED ON ALL PROJECTS IN DISTRICTS 1, 6, AND 7, EXCEPT WHERE SPECIFICALLY EXCEPTED BY THE GEOTECHNICAL BUREAU IN THE SOIL SURVEY REPORT. SERRATED SLOPES SHALL NOT BE USED IN DISTRICTS 2, 3, 4, AND 5, UNLESS REQUIRED BY THE SOIL SURVEY.

6-18-03		1-28-82		DATE		DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REVISED BENCHING DETAIL & SERRATED SLOPE DETAIL		GEN. REVISION		REVISION		CONSTRUCTION DETAIL SERRATED SLOPE DETAIL BENCHING DETAIL	
NO SCALE						JULY, 1981	
C.I.O.	R.M.U.	BY				NUMBER S-7	

6/18/2009 7:38:27 PM \\GDOT-DSN1\GDOT\GCF\gpo\_1111\_output.qcf gowens V:\BARY\REVISED S-7\S-7.pr1

**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING

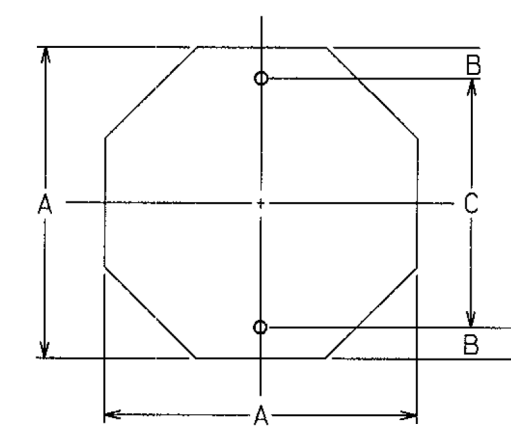
NOT TO SCALE

REVISION DATES


**CONSTRUCTION DETAILS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

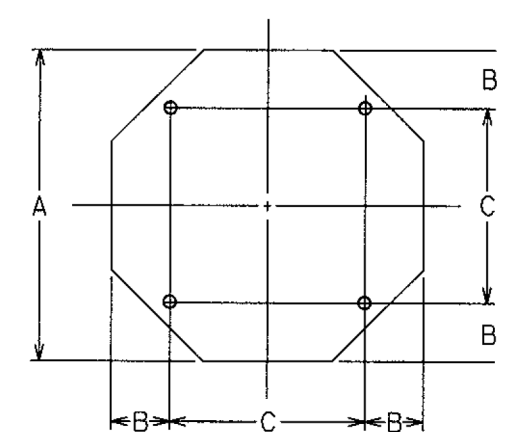
CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	40-0005
CORRECTED:	DATE:	
VERIFIED:	DATE:	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

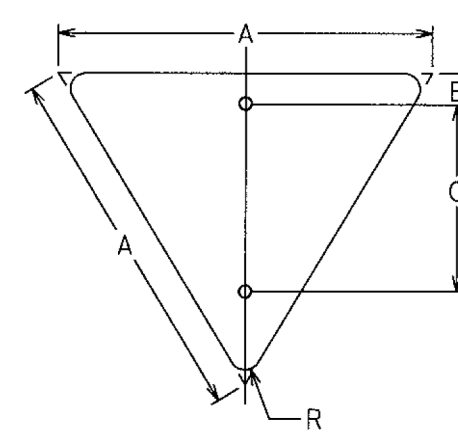


OCTAGON

A	B	C
24	3	18
30	3	24
36	3	30

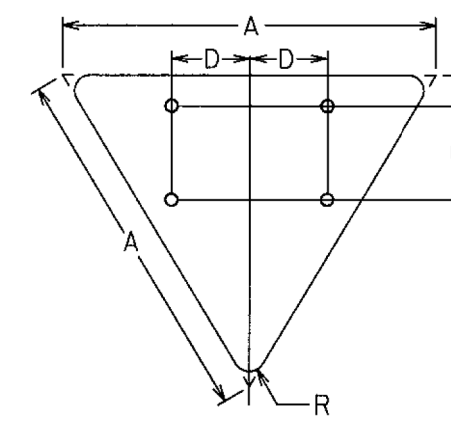


A	B	C
48	9	30

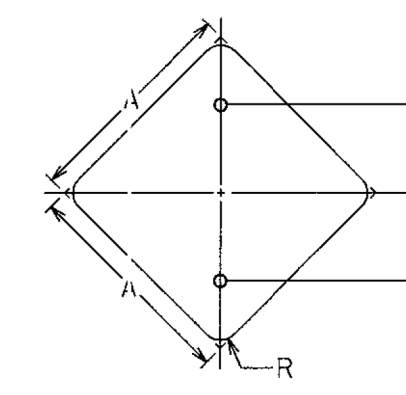


EQUILATERAL TRIANGLE

A	B	C	R
30	3	18	1 1/2
36	3	21	2
48	3	27	3

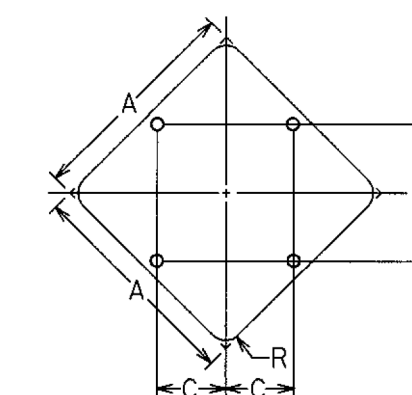


A	B	C	D	R
60	3	18	15	3



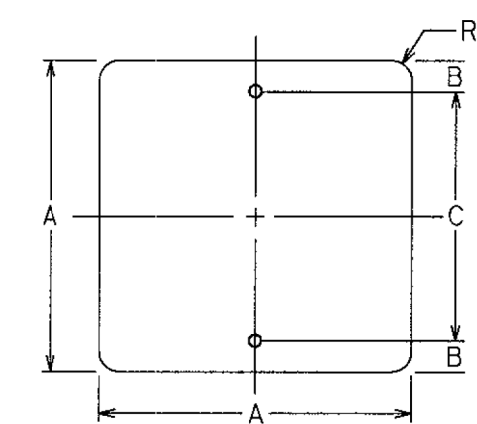
DIAMOND

A	B	R
24	12	1 1/2
30	15	1 7/8
36	18	2 1/4



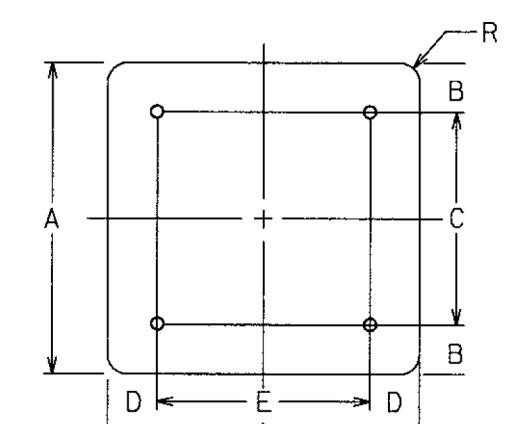
A	B	C	R
36	10	10	2 1/4
48	15	15	3
60	18	18	3 3/4

\* FOR TWO POST ERECTION

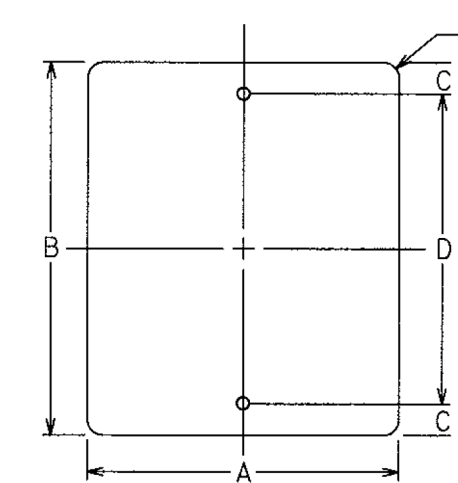


SQUARE

A	B	C	R
18	3	12	1 1/2
24	3	18	1 1/2
30	3	24	1 7/8

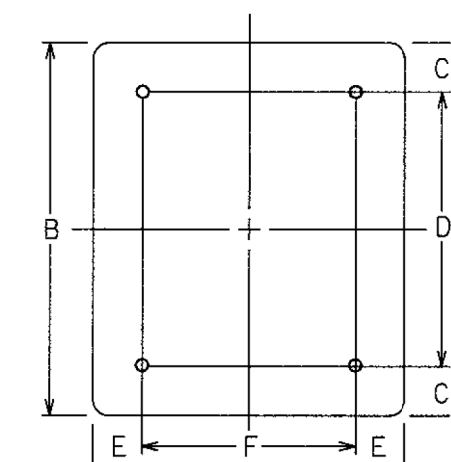


A	B	C	D	E	R
36	6	24	6	24	2 1/4
48	6	36	6	36	3

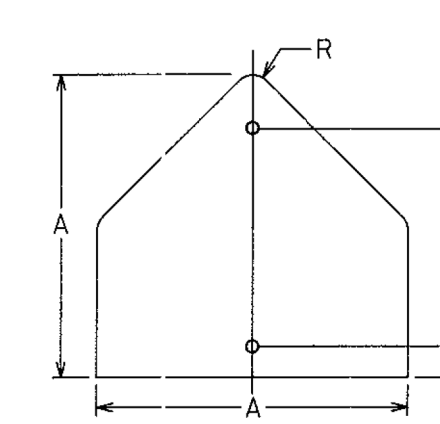


VERTICAL RECTANGLE

A	B	C	D	R
12	18	1 1/2	15	1 1/2
18	24	3	18	1 1/2
24	30	3	24	1 1/2
30	36	3	30	1 7/8

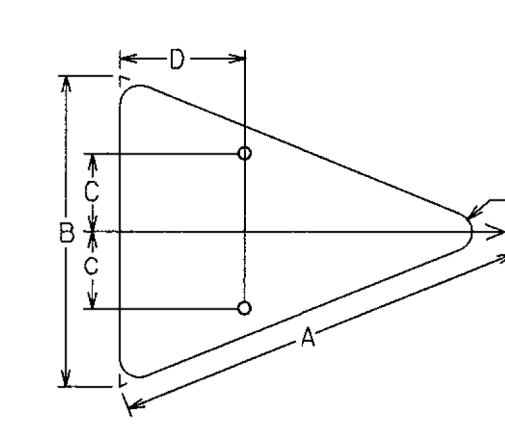


A	B	C	D	E	F	R
36	48	6	36	6	24	2 1/4
48	60	6	48	9	30	3



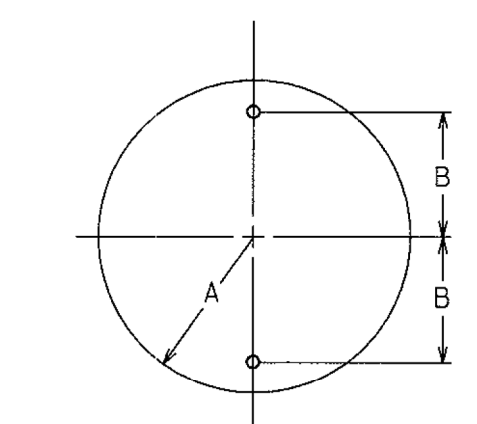
PENTAGON

A	B	C	R
30	21	3	1 7/8
36	24	3	2 1/4



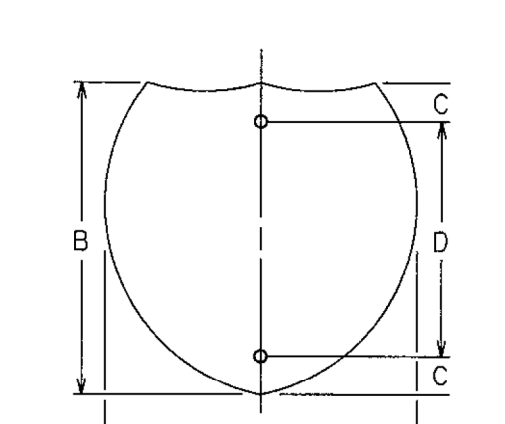
ISOSCELES TRIANGLE

A	B	C	D	R
40	30	7 1/2	12	1 7/8
48	36	9	15	2 1/4



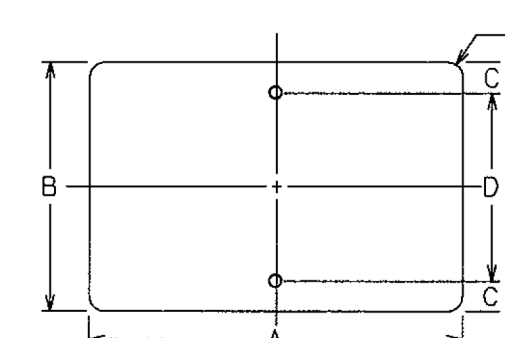
CIRCLE

A	B
15	12
18	15



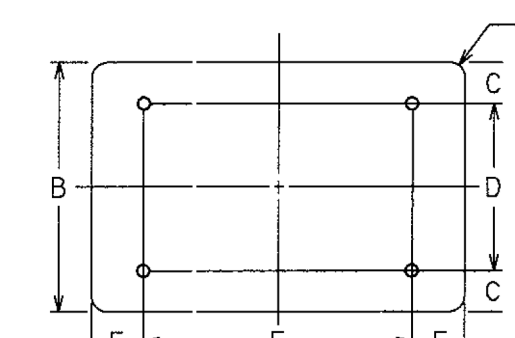
INTERSTATE SHIELD

A	B	C	D
24	24	3	18
30	24	3	18
36	36	6	24
45	36	6	24



HORIZONTAL RECTANGLE

A	B	C	D	R
21	15	1 1/2	12	1 1/2
24	12	1 1/2	9	1 1/2
24	18	3	12	1 1/2
30	15	1 1/2	12	1 1/2
30	24	3	18	1 1/2
36	12	1 1/2	9	1 1/2
36	24	3	18	1 1/2
48	12	1 1/2	9	1 1/2
48	24	3	18	1 7/8



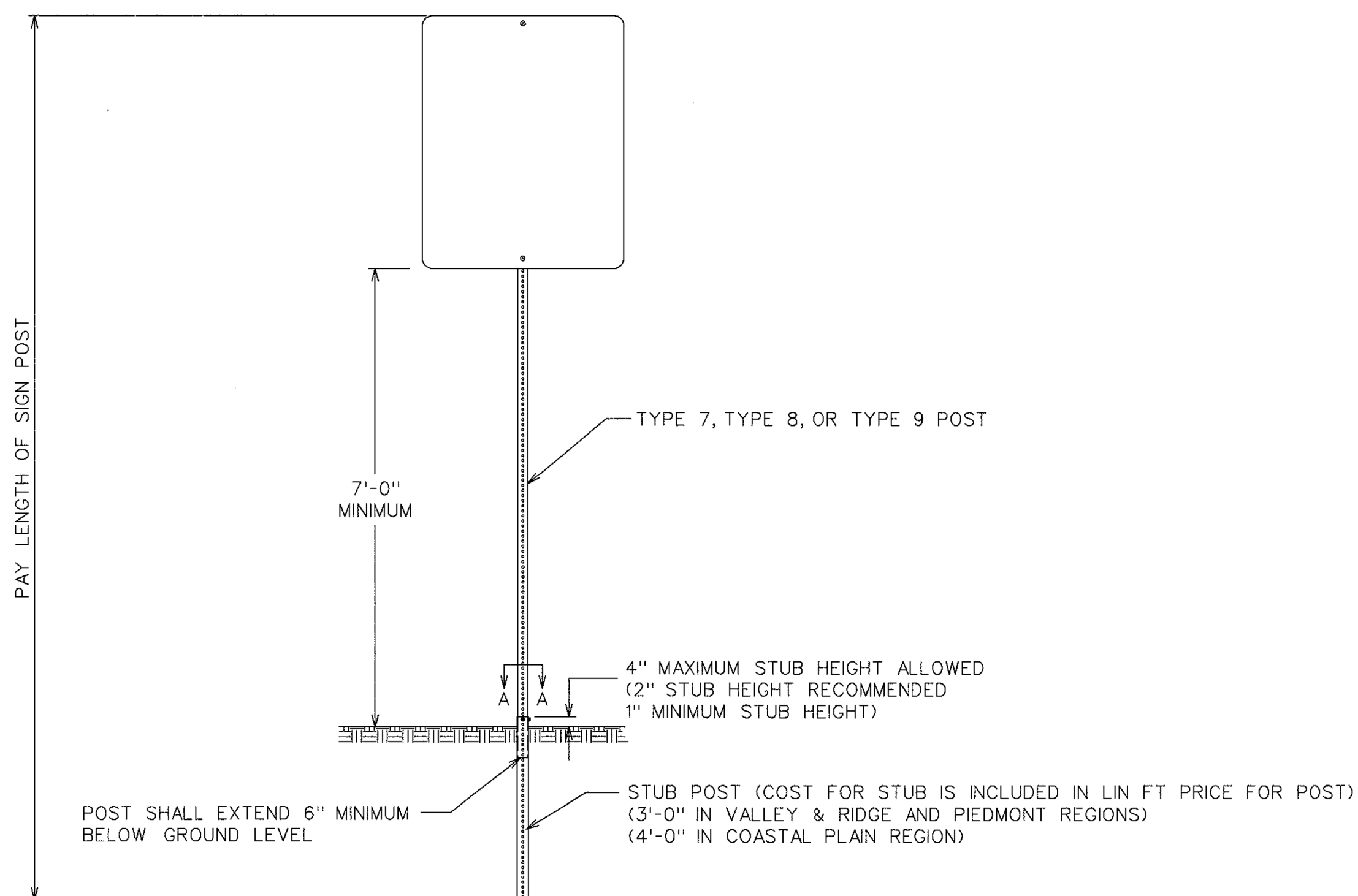
A	B	C	D	E	F	R
48	36	6	24	9	30	2 1/4
60	24	3	18	12	36	1 1/2
60	36	6	24	12	36	2 1/4

DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
		DETAILS OF SIGN PLATES
		NO SCALE JANUARY 2000

PC807B

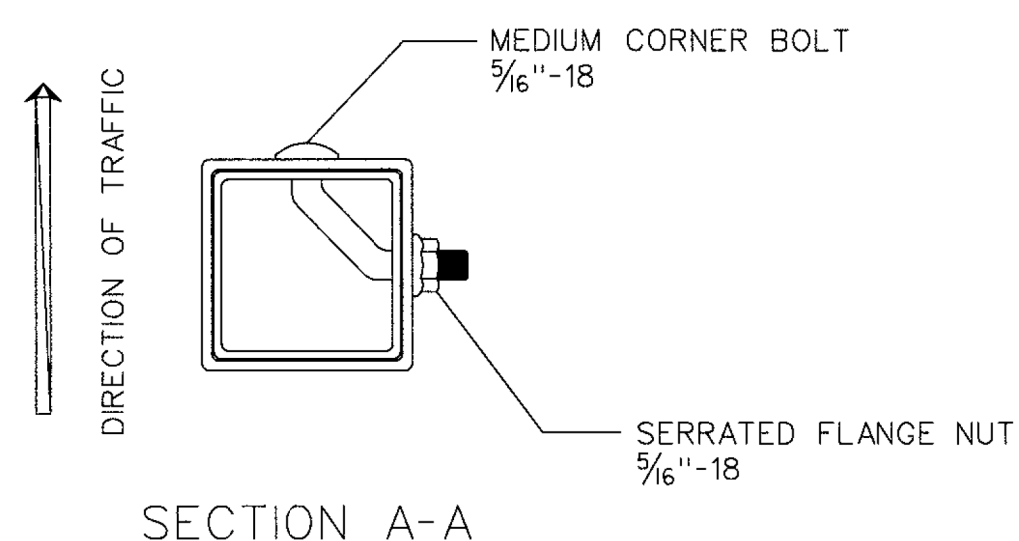
T-1

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



FRONT VIEW

POST	STUB SIZE
TYPE 7	2 1/4" x 2 1/4"
TYPE 8	2 3/4" x 2 3/4"
TYPE 9	2 1/2" x 2 1/2"



SECTION A-A

SIGN POST SELECTION CHART

70 MPH Wind Load Chart • 15% Gust Factor

Sign Centroid	SLIP BASE NOT REQUIRED				GROUND MOUNTED BREAKAWAY SIGN SUPPORT REQUIRED				
	TYPE 7 2" x 14 ga.		TYPE 9 2-1/4" x 14 ga.	TYPE 8 2-1/2" x 12 ga.	TYPE 8 2-1/2" x 12 ga.		TYPE 8 w / TYPE 9 Insert* 2-1/2" x 12 ga. W / 2-1/4" x 14 ga.		
	1 Post	2 Post	1 Post	1 Post	2 Post	3 Post	1 Post	2 Post	3 Post
	SQUARE FOOTAGE				SQUARE FOOTAGE				
6'	13.50	27.00	19.25	30.00	60.00	90.00	49.25	98.50	147.75
7'	11.60	23.20	16.50	25.75	51.50	77.25	42.25	84.50	126.75
8'	10.15	20.30	14.45	22.55	45.10	67.65	37.00	74.00	111.00
9'	9.00	18.00	12.85	20.00	40.00	60.00	32.85	65.70	98.55
10'	8.10	16.20	11.55	18.00	36.00	54.00	29.55	59.10	88.65
11'	7.40	14.80	10.50	16.40	32.80	49.20	26.90	53.80	80.70
12'	6.80	13.60	9.65	15.00	30.00	45.00	24.65	49.30	73.95
13'	6.25	12.50	8.90	13.85	27.70	41.55	22.75	45.50	68.25
14'	5.80	11.60	8.25	12.90	25.80	38.70	21.15	42.30	63.45
15'	5.00	10.00	6.45	10.10	20.20	30.30	16.55	33.10	49.65
16'	4.70	9.40	6.05	9.45	18.90	28.35	15.50	31.00	46.50
17'	4.40	8.80	5.70	8.90	17.80	26.70	14.60	29.20	43.80
18'	4.15	8.30	5.40	8.40	16.80	25.20	13.80	27.60	41.40
19'	3.95	7.90	5.10	7.95	15.90	23.85	13.05	26.10	39.15
20'	3.75	7.50	4.85	7.55	15.10	22.65	12.40	24.80	37.20

SIGN CENTROID IS DISTANCE FROM GROUND LEVEL TO BOTTOM OF SIGN PLUS HALF THE HEIGHT OF SIGN.  
EXAMPLE: 24" X 48" SIGN THAT IS 7 FEET FROM GROUND TO BOTTOM OF SIGN. ADD HALF OF 48" (24" OR 2 FT) PLUS 7 FT. = 9' CENTROID.

SIGN PLATE SHALL NOT EXCEED 48" IN WIDTH ON A SINGLE POST.

TYPE 9 INSERT SHALL BE A CONTINUOUS POST INSERTED INTO THE TYPE 8 POST WHERE REQUIRED. THE INSERT POST SHALL EXTEND FROM THE BOTTOM OF THE SLIP BASE UPPER ASSEMBLY TO 4" BELOW THE BOTTOM OF THE SIGN. THE INSERT POST SHALL NOT EXTEND ABOVE THE BOTTOM OF THE SIGN. PAYMENT FOR THE INSERT POST SHALL BE PER LINEAR FOOT OF TYPE 9 POST.

GROUND MOUNTED BREAKAWAY SIGN SUPPORT WILL BE MEASURED AND PAID FOR SEPARATELY. THE COST FOR THIS WORK SHALL INCLUDE THE UPPER AND LOWER ASSEMBLY, STUB POST, CLASS "A" CONCRETE, ALL HARDWARE NECESSARY TO COMPLETE THE INSTALLATION, AND BE INCLUDED IN THE BID PRICE SUBMITTED FOR ITEM 636-3010.

DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
		TYPE 7, 8, AND 9 SQUARE TUBE POST INSTALLATION DETAIL
		NO SCALE JULY 2002

T-3A

INFRASTRUCTURE CONSULTING & ENGINEERING

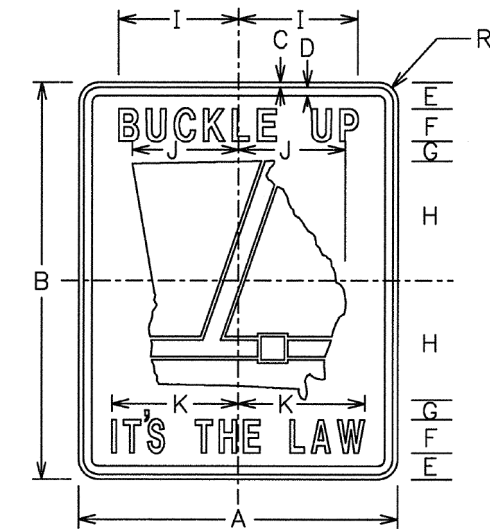
NOT TO SCALE

REVISION DATES

CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	40-0007
CORRECTED:	DATE:	
VERIFIED:	DATE:	

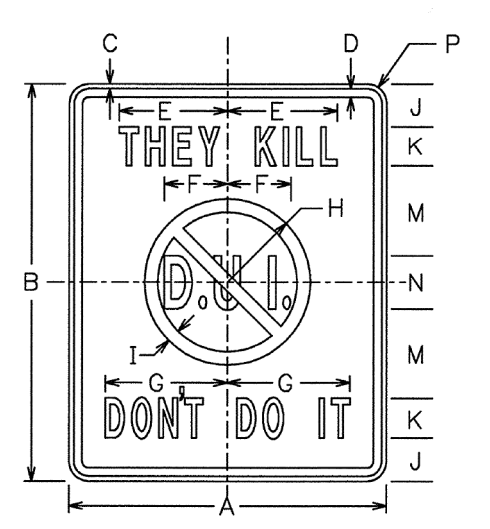
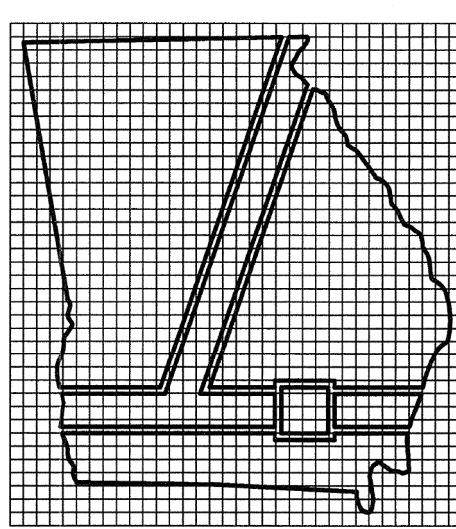
STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



R560-1

SIGN	DIMENSIONS (INCHES)											
	A	B	C	D	E	F	G	H	I	J	K	R
MIN & STD	30	36	1/2	3/4	2 1/2	3C	1 1/2	11	8 1/2	10	11	1 7/8
FWY	48	60	3/4	1 1/4	4	5C	3	18	15 1/2	16	18	3

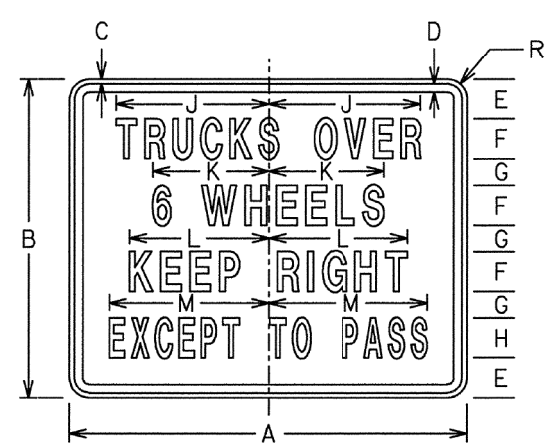
COLORS  
STATE SHIELD & BORDER - RED (REFL)  
LEGEND & BELT - BLACK (NON-REFL)  
BACKGROUND - WHITE (REFL)



R560-2

SIGN	DIMENSIONS (INCHES)														
	A	B	C	D	E	F	G	H	I	J	K	L	M	N	P
MIN & STD	30	36	1/2	3/4	11	5	12 1/2	7 1/2	1 1/4	4	4C	3	4 1/2	5C	1 7/8
FWY	48	60	3/4	1 1/4	16 1/2	2 1/2	18 1/2	12 1/2	2	6 1/2	6C	5	8 1/2	8C	3

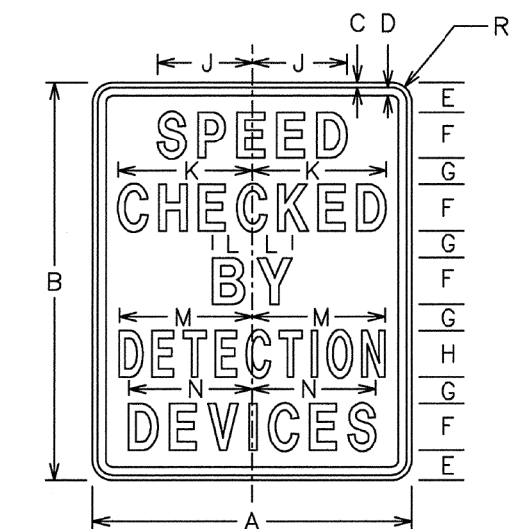
COLORS  
CIRCLE & DIAGONAL - RED (REFL)  
LEGEND & BORDER - BLACK (NON-REFL)  
BACKGROUND - WHITE (REFL)



R560-3

SIGN	DIMENSIONS (INCHES)												
	A	B	C	D	E	F	G	H	J	K	L	M	R
MIN & STD	36	30	1/2	3/4	3 1/2	3 1/2	3	3 1/2	14	10	11 1/2	14 1/2	1 7/8
FWY	60	48	3/4	1 1/4	6	6C	4	6B	23	17 1/2	21	24	3

COLORS  
LEGEND & BORDER - BLACK (NON-REFL)  
BACKGROUND - WHITE (REFL)



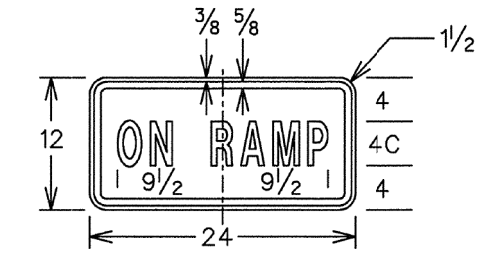
1550-1

SIGN	DIMENSIONS (INCHES)													
	A	B	C	D	E	F	G	H	J	K	L	M	N	R
MIN & STD	30	36	1/2	3/4	4	4D	2	4C	8 3/8	11 1/8	3 7/8	11 1/8	10 5/8	1 1/8
FWY	48	60	3/4	1 1/4	4 1/2	7D	4	7C	14 5/8	20 3/8	6	20	18 5/8	3

COLORS  
LEGEND & BORDER - BLACK (NON-REFL)  
BACKGROUND - WHITE (REFL)

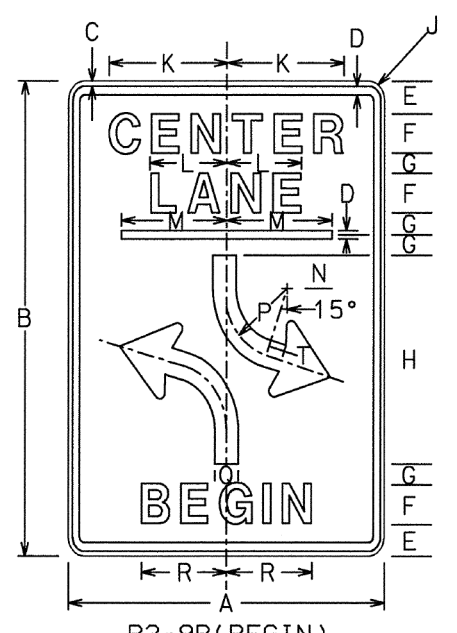
THE 1550-1 SIGN SHALL BE ERECTED:

- ON EVERY HIGHWAY THAT COMPRISES A PART OF THE STATE HIGHWAY SYSTEM AT THAT POINT ON THE HIGHWAY WHICH INTERSECTS THE STATE LINE.
- AT THE TERMINI OF EVERY HIGHWAY THAT COMPRISES A PART OF THE STATE HIGHWAY SYSTEM WHICH BEGINS OR ENDS WITHIN THE STATE BOUNDARIES.
- ON EVERY HIGHWAY THAT COMPRISES A PART OF THE STATE HIGHWAY SYSTEM AT THAT POINT ON THE HIGHWAY WHERE TRAFFIC FROM OUTSIDE THE COUNTY FIRST ENTERS A COUNTY THAT HAS A PERMIT TO OPERATE SPEED DETECTION DEVICES, AND
- ON EVERY HIGHWAY THAT COMPRISES A PART OF THE STATE HIGHWAY SYSTEM AT THAT POINT ON THE HIGHWAY WHERE TRAFFIC FIRST ENTERS THE CORPORATE LIMITS OF ANY MUNICIPALITY THAT HAS A PERMIT TO OPERATE SPEED DETECTION DEVICES.



R8-3aP

COLORS  
LEGEND & BORDER - RED (REFL)  
BACKGROUND - WHITE (REFL)



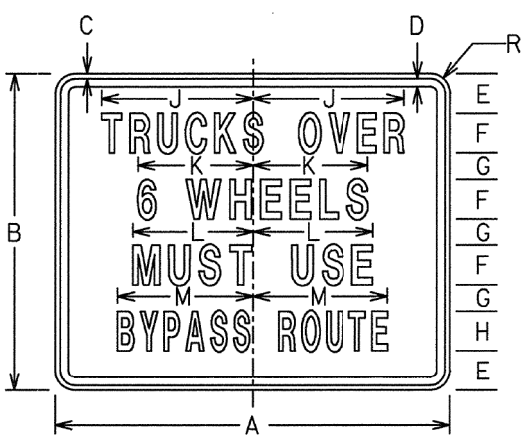
R3-9B(BEGIN)



R3-9B(END)

SIGN	DIMENSIONS (INCHES)																		
	A	B	C	D	E	F	G	H	J	K	L	M	N	P	Q	R	S	T	
STD & MIN	24	36	3/8	5/8	2 1/2	3E	1 1/2	16	1 1/2	8 3/8	5 3/4	8	2 1/2	6	2	6 1/8	4 3/8	1 1/2	
SPECIAL	36	48	5/8	7/8	3 1/2	5E	1 1/2	20	2 1/4	14 1/8	9 1/2	12	3	8	3	7 1/8	4 5/8	2	

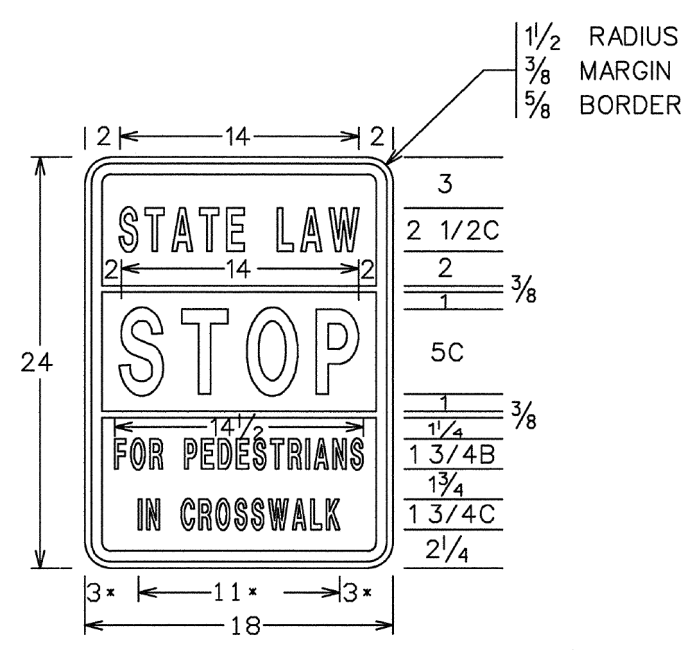
COLORS  
LEGEND & BORDER - BLACK (NON-REFL)  
BACKGROUND - WHITE (REFL)



R560-4

SIGN	DIMENSIONS (INCHES)												
	A	B	C	D	E	F	G	H	J	K	L	M	R
MIN & STD	36	30	1/2	3/4	3 1/2	3 1/2	3	3 1/2	14	10	10	12 1/2	1 7/8
FWY	60	48	3/4	1 1/4	6	6C	4	6C	23	17 1/2	17 1/2	25 1/2	3

COLORS  
LEGEND & BORDER - BLACK (NON-REFL)  
BACKGROUND - WHITE (REFL)



R560-5

COLORS  
LEGEND & BORDER - BLACK (NON-REFL)  
WORD "STOP" - RED (REFL)  
BACKGROUND - WHITE (REFL)

DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
5-24-00	ADDED R3-8 DETAIL	DETAILS OF REGULATORY SIGNS SHEET 1 OF 2
1-21-03	DELETED R1-4 SIGNS	
1-21-03	REV SIGN CODES FOR R3-9 SIGNS	
		NO SCALE
		JANUARY 2000

PC8078

T-5A

**INFRASTRUCTURE**  
CONSULTING & ENGINEERING

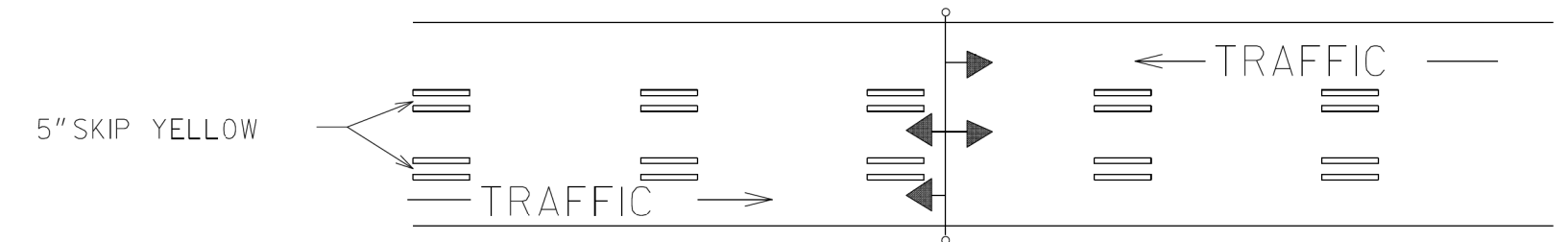
NOT TO SCALE

REVISION DATES

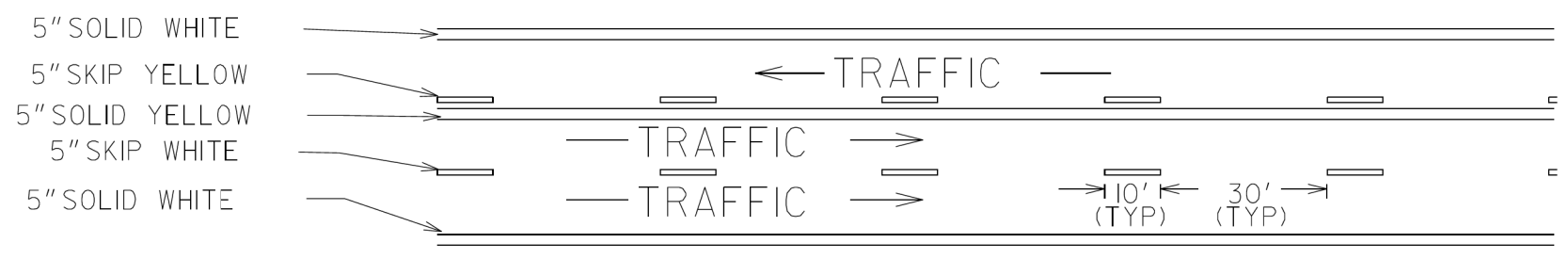
**CONSTRUCTION DETAILS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	40-0008
CORRECTED:	DATE:	
VERIFIED:	DATE:	

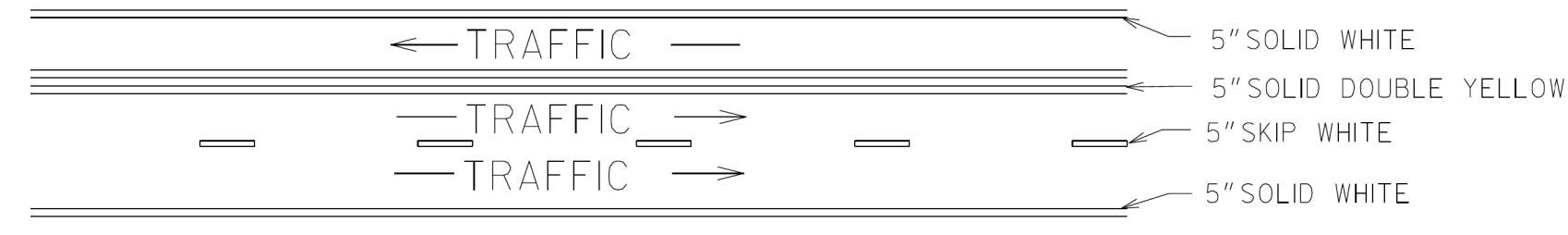
STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



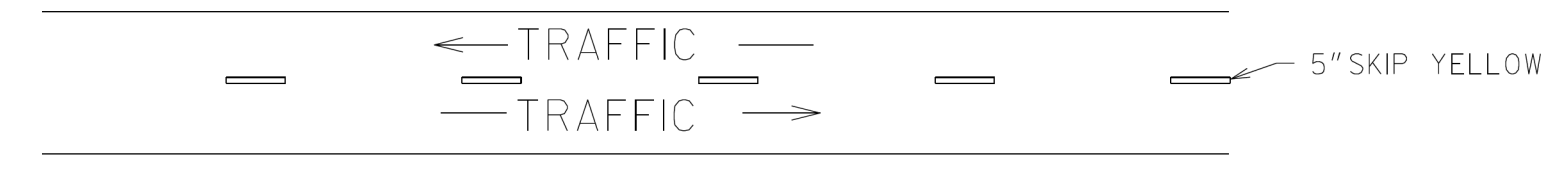
REVERSIBLE LANE SIGN OR SIGNAL SYSTEM REQUIRED  
TWO-WAY TRAFFIC WITH A REVERSIBLE CENTER LANE



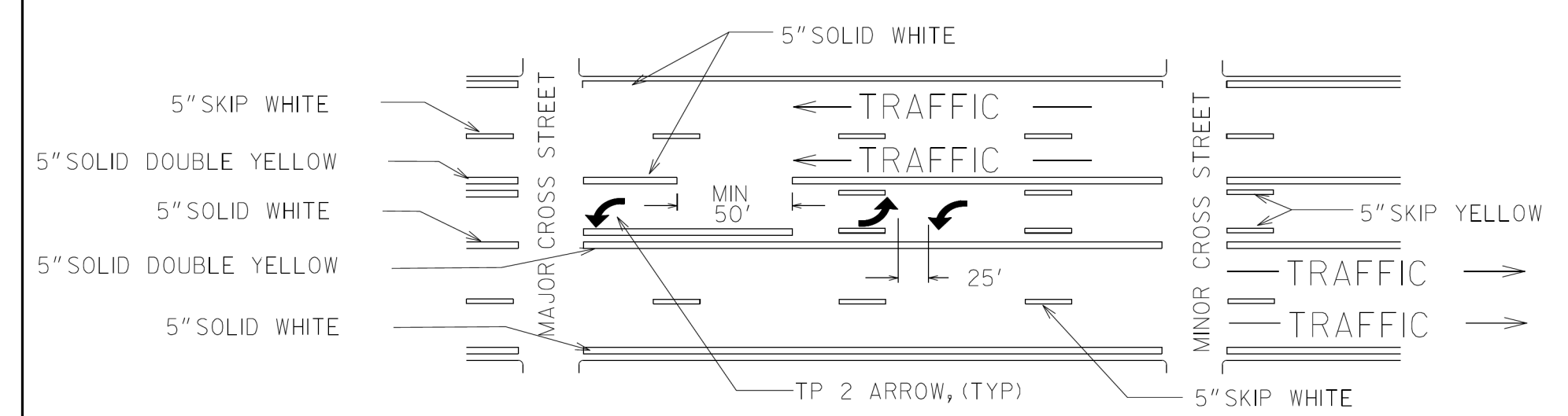
TWO-WAY TRAFFIC WHERE MOTORISTS IN A SINGLE LANE ARE PERMITTED TO PASS



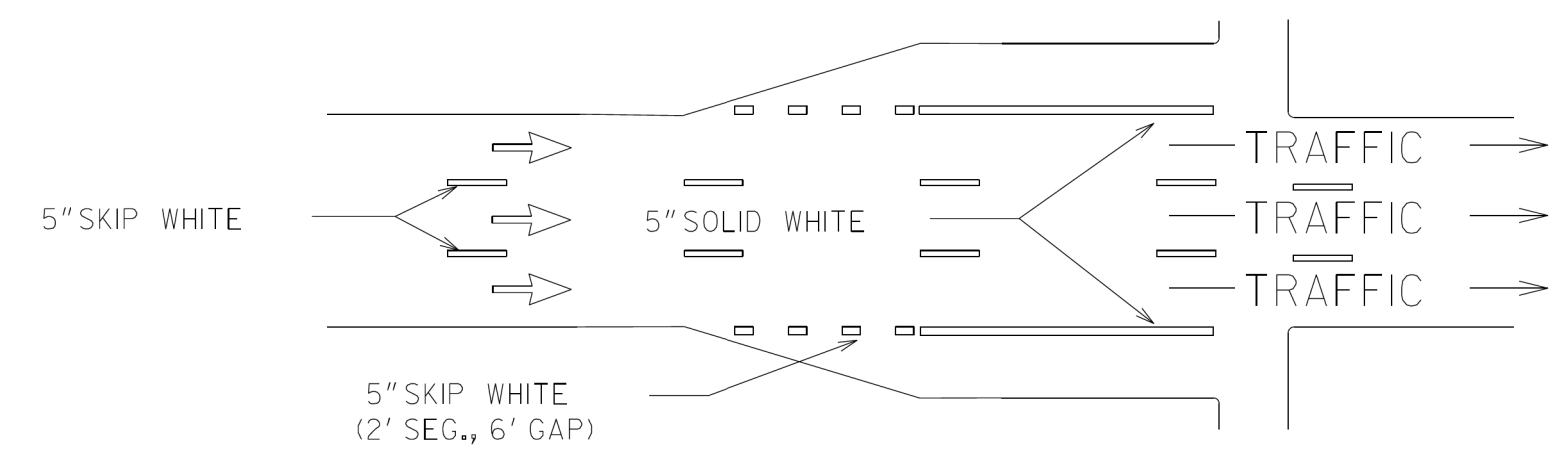
TWO-WAY TRAFFIC WHERE MOTORISTS IN A SINGLE LANE ARE NOT PERMITTED TO PASS



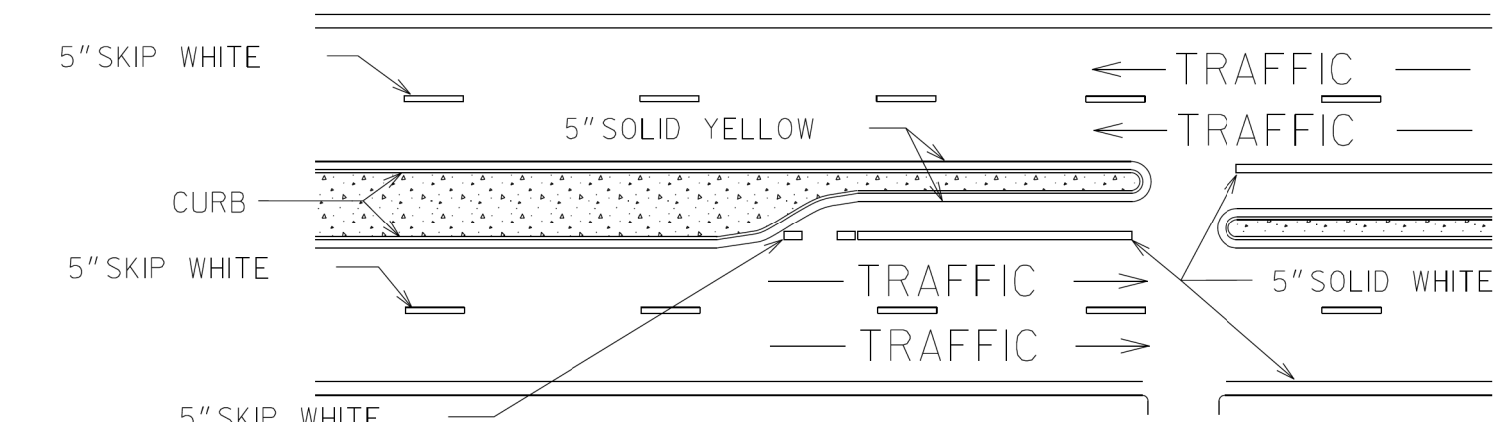
TWO-LANE, TWO-WAY TRAFFIC WITH PASSING PERMITTED



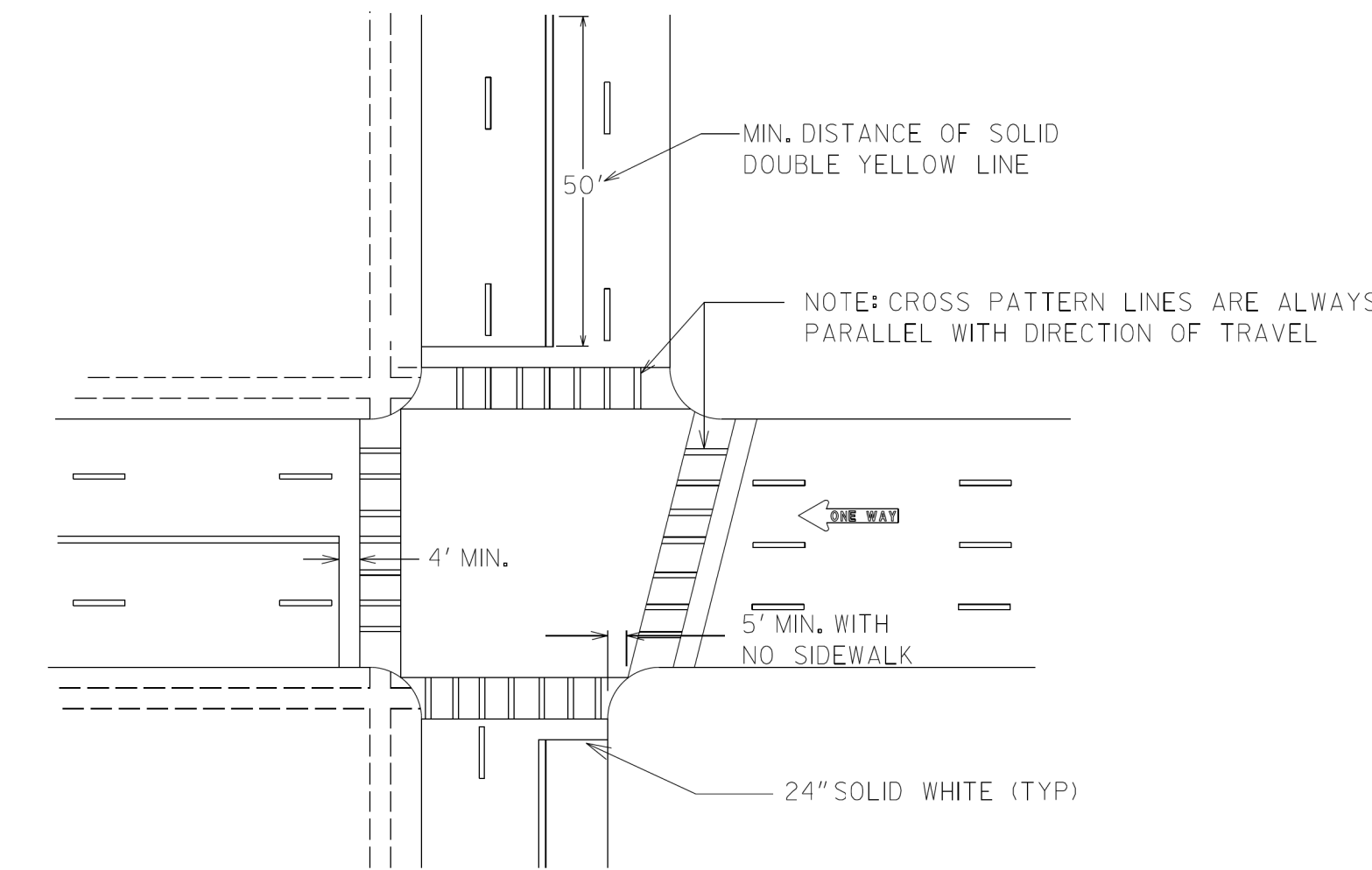
MULTI-LANE, TWO-WAY TRAFFIC WITH SINGLE LANE, TWO-WAY LEFT TURN CHANNELIZATION



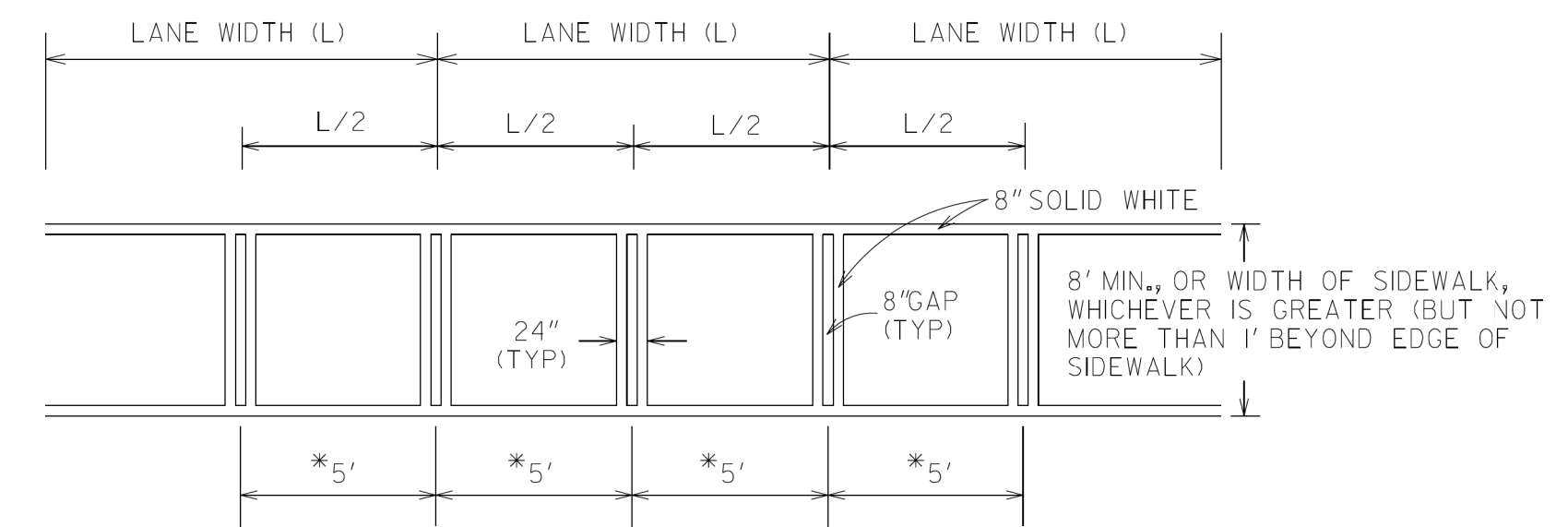
ONE-WAY TRAFFIC WITH ADDED TURN LANES



DIVIDED HIGHWAY WITH RAISED MEDIAN



TYPICAL LOCATION OF CROSSWALKS AND STOP BARS



\*USE WHERE THE LANE WIDTH EXCEEDS 12'  
OR WHERE LANE LINES HAVE BEEN OMITTED

CROSSWALK DETAIL

- GENERAL NOTES:
1. SPACING BETWEEN DOUBLE LINES SHALL BE EQUAL TO THE LINE WIDTH.
  2. EDGE LINES SHALL BE PLACED A MINIMUM OF 4 INCHES FROM THE NORMAL EDGE OF PAVEMENT.
  3. CONTRAST MARKINGS FOR SKIP STRIPING SHALL BE AS SHOWN IN DETAIL T-IIA.

9-05-16		DATE		DEPARTMENT OF TRANSPORTATION	
9-05-16		DATE		STATE OF GEORGIA	
ADDED GENERAL NOTE 3		REVISION		CONSTRUCTION DETAILS	
ADDED GENERAL NOTE 3		REVISION		PAVEMENT MARKING PLACEMENT	
ADDED GENERAL NOTE 3		REVISION		NON-LIMITED ACCESS ROADWAY	
NO SCALE		JANUARY 2000		NUMBER	
COR BY		DESIGNED		DRAWN	
COR BY		TRACED		CHECKED	
COR BY		CHECKED		T-IIA	



NOT TO SCALE

REVISION DATES

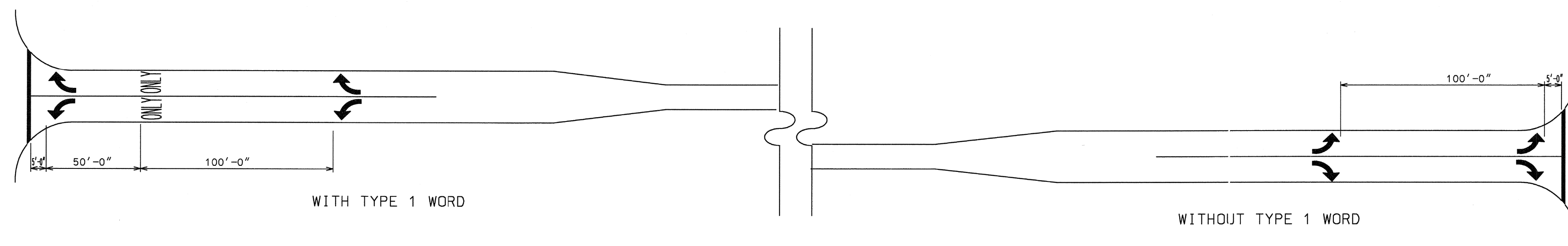
NO.	DATE	DESCRIPTION

CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

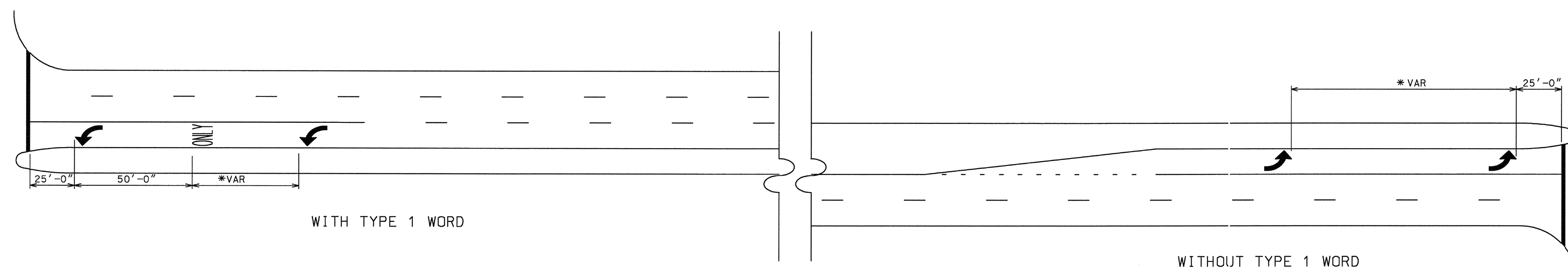
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CORRECTED:	DATE:	
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STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

EXIT RAMP

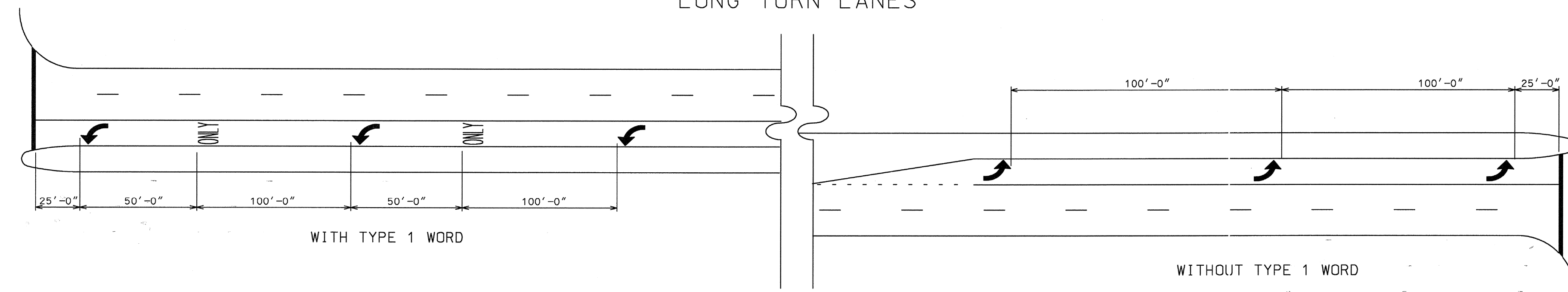


SHORT TURN LANES



\* ADJUST TO MEET LOCAL CONDITIONS (NOT LESS THAN 50' NOR MORE THAN 100')

LONG TURN LANES



GENERAL NOTES:

1. SPACING OF TYPE 2 ARROW IS REPRESENTATIVE OF SPACING FOR TYPE 1, TYPE 3, TYPE 4, & TYPE 5 ARROWS.
2. ALL TURNING LANES SHALL HAVE A MINIMUM OF 2 ARROWS.
3. GROUND MOUNTED OR OVERHEAD SIGNING SHALL BE SUPPLEMENTED BY TYPE 1 WORD.

DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
		DETAILS OF PAVEMENT MARKING ARROW LOCATION
		NO SCALE JANUARY 2000

PC807B

T-12A

**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING

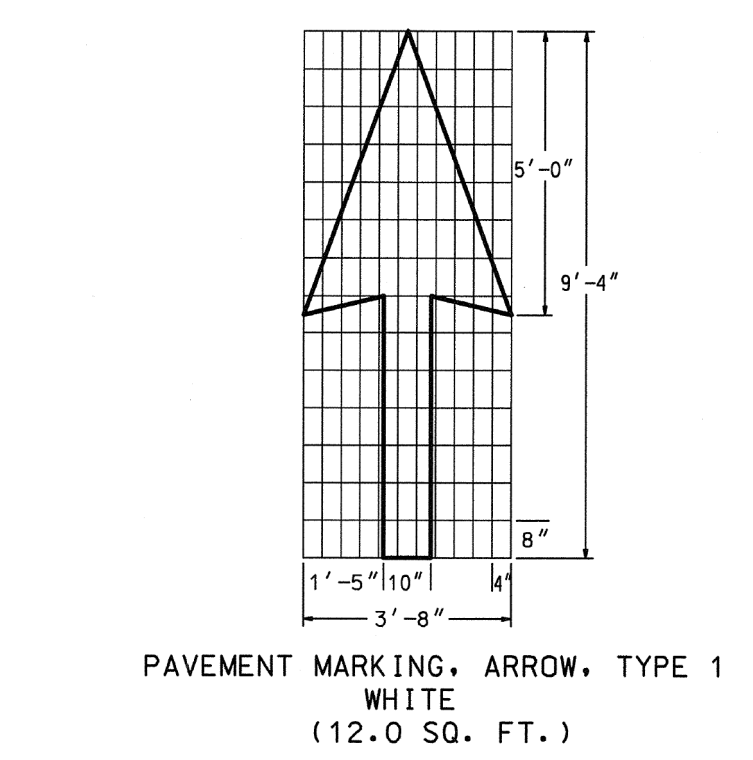
NOT TO SCALE

REVISION DATES

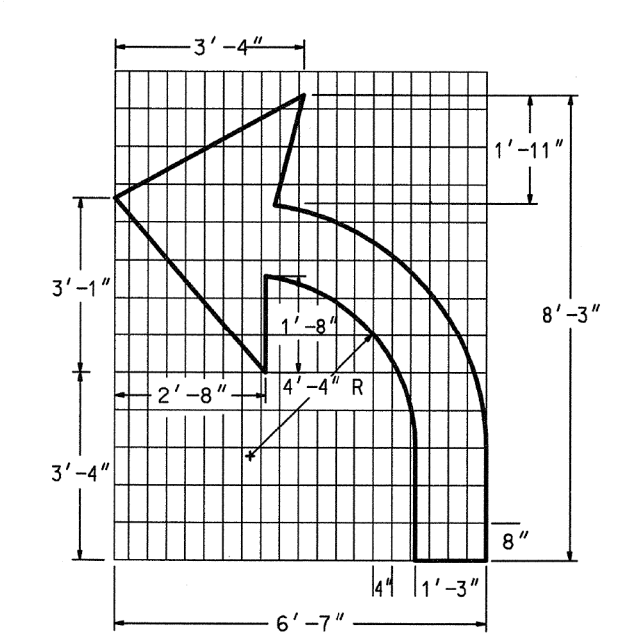

**CONSTRUCTION DETAILS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

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BACKCHECKED:	DATE:	40-0010
CORRECTED:	DATE:	
VERIFIED:	DATE:	

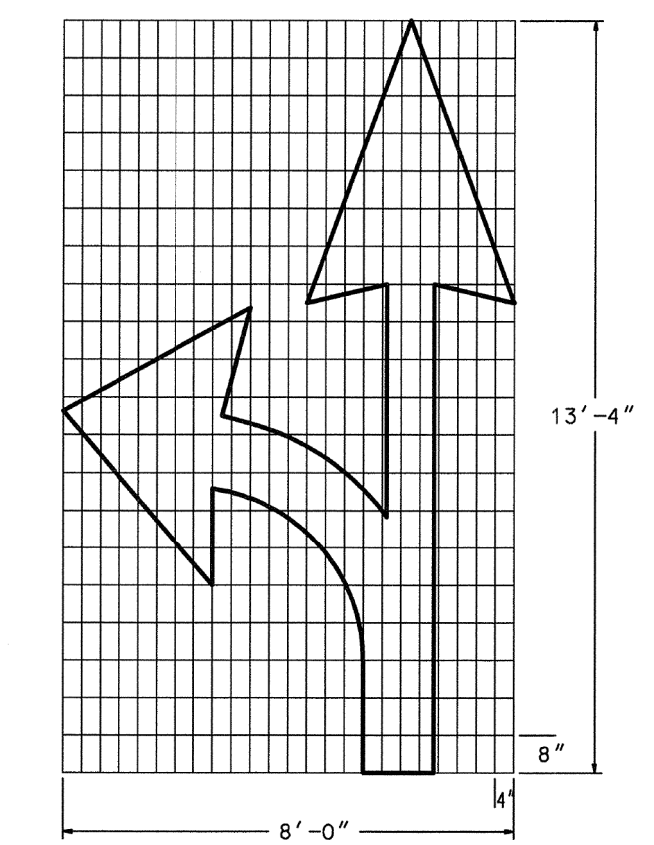
STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



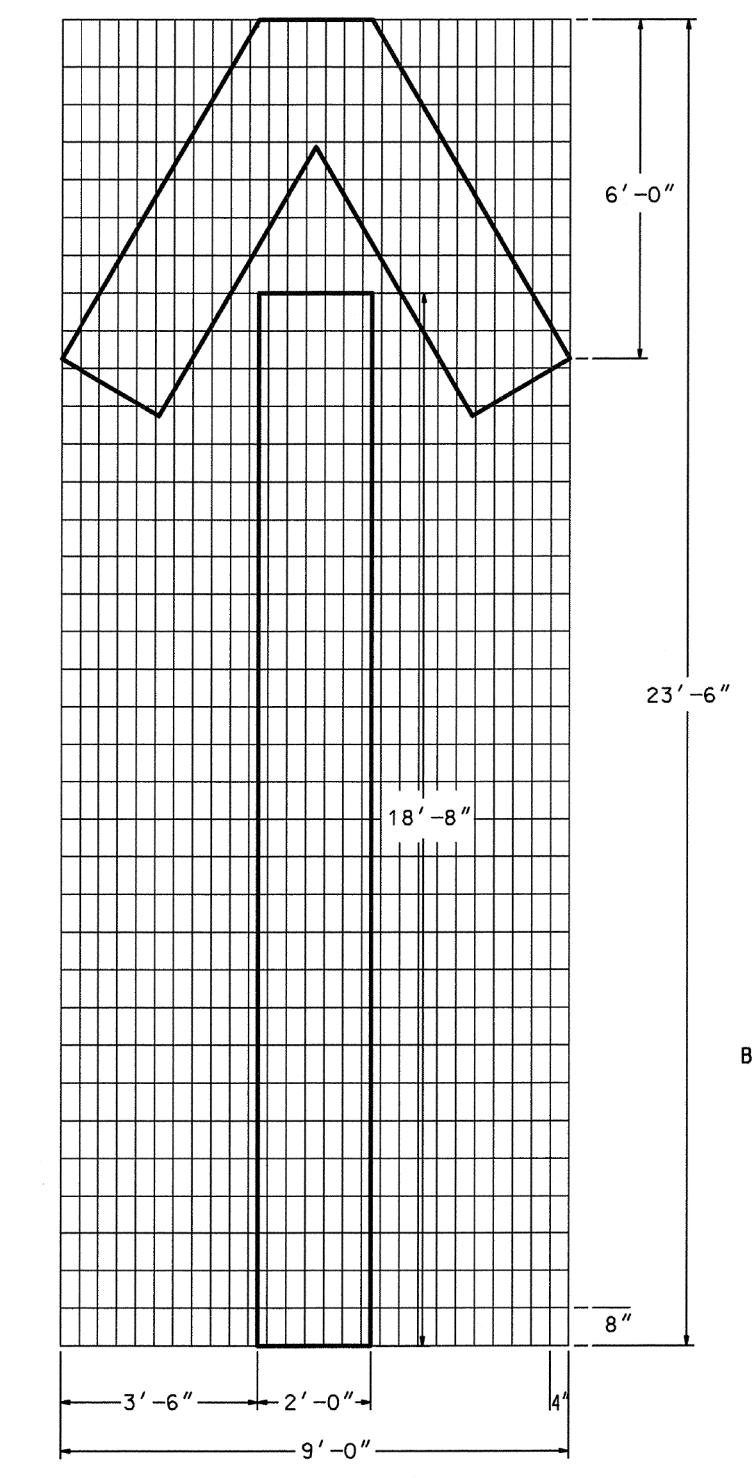
PAVEMENT MARKING, ARROW, TYPE 1  
WHITE  
(12.0 SQ. FT.)



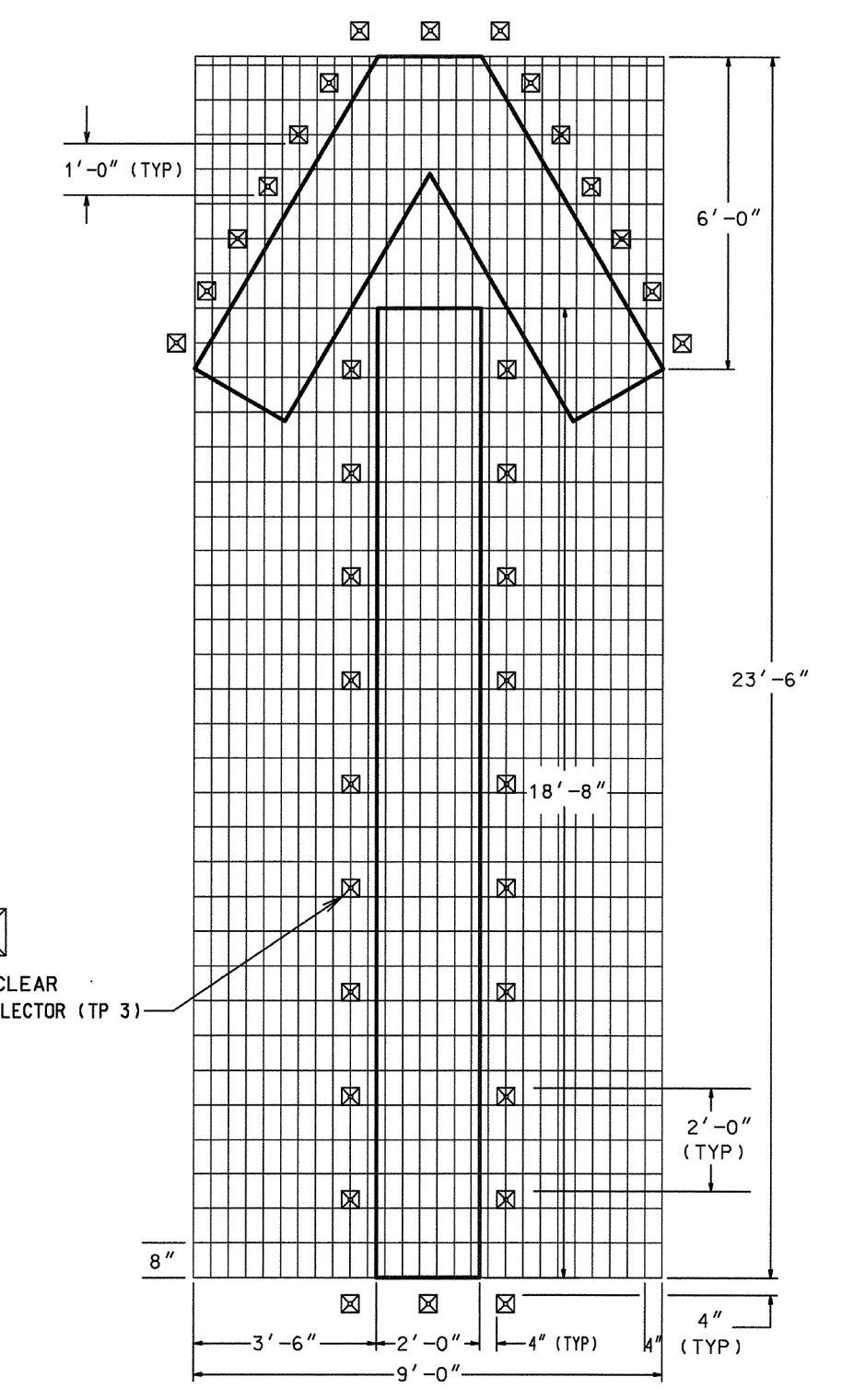
PAVEMENT MARKING, ARROW, TYPE 2  
WHITE  
(16.0 SQ. FT.)



PAVEMENT MARKING, ARROW, TYPE 3  
WHITE  
(28.5 SQ. FT.)

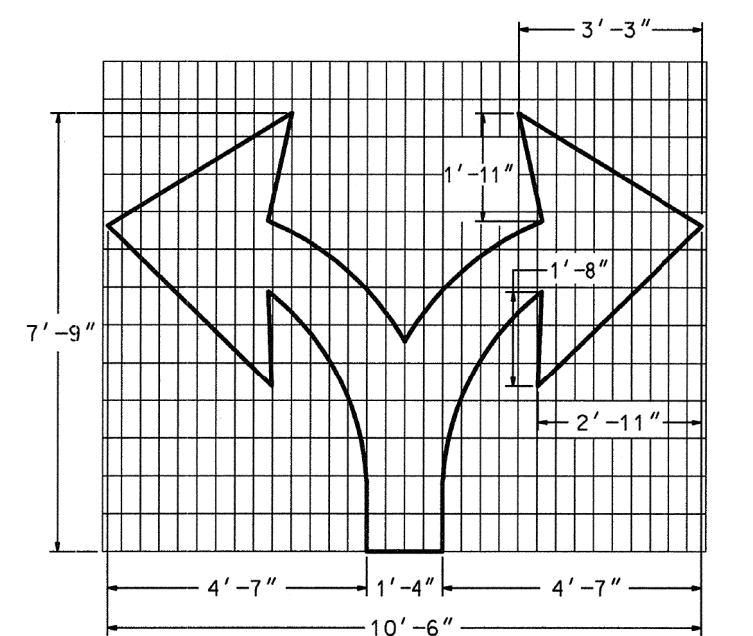


PAVEMENT MARKING, ARROW, TYPE 4  
WHITE  
(64.5 SQ. FT.)

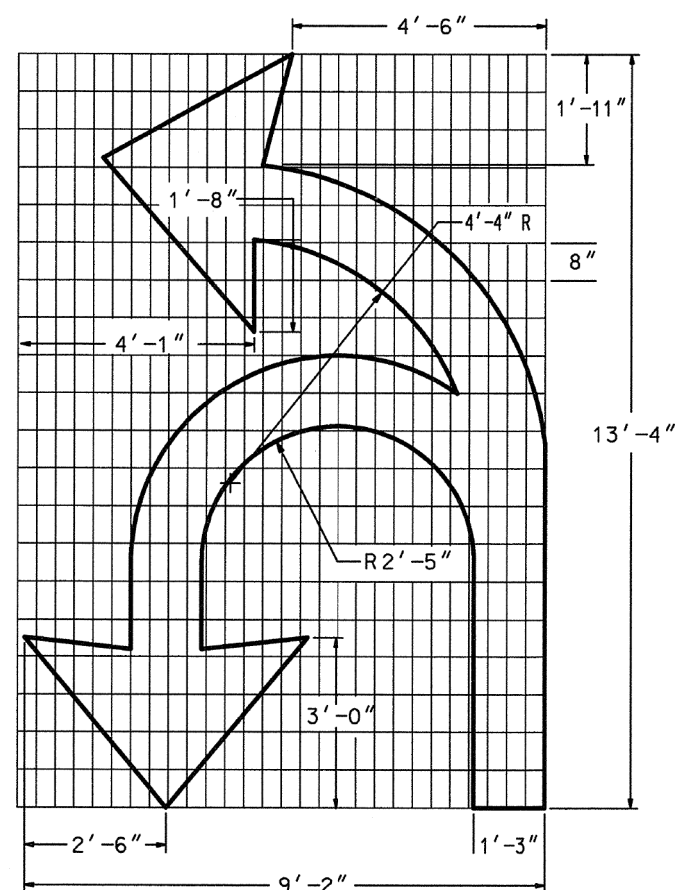


PAVEMENT MARKING, ARROW, WRONG WAY  
WHITE  
(64.5 SQ. FT.)

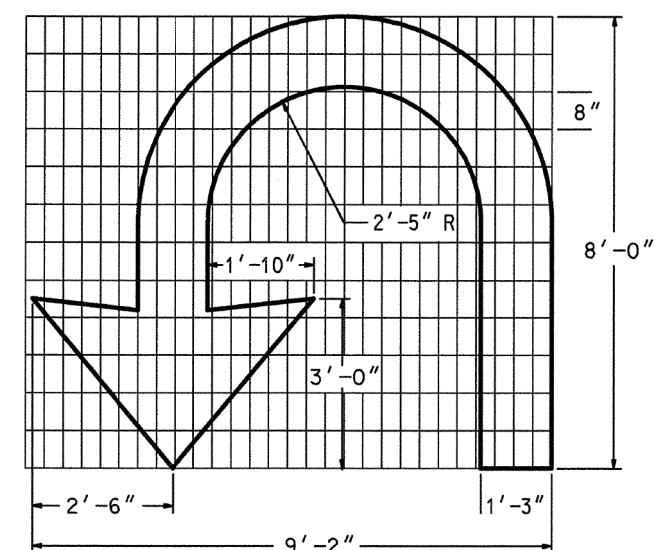
ARROW- WHITE REFLECTORIZED PAINT THERMOPLASTIC, AS SPECIFIED BY GEORGIA STANDARD SPECIFICATIONS, SECTION 655.



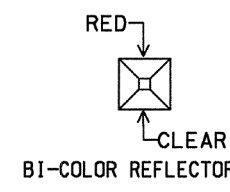
PAVEMENT MARKING, ARROW, TYPE 5  
WHITE  
(25.5 SQ. FT.)



PAVEMENT MARKING, ARROW, TYPE 6  
WHITE  
(42.0 SQ. FT.)



PAVEMENT MARKING, ARROW, TYPE 7  
WHITE  
(26.0 SQ. FT.)



DATE	REVISIONS	GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE OF TRAFFIC SAFETY & DESIGN
4-11-00	CHANGED LOCATION OF RPMS ON WRONG WAY ARROW	

DETAILS OF  
PAVEMENT MARKINGS-ARROWS  
NO SCALE  
JANUARY 2000

T-12B



NOT TO SCALE

REVISION DATES

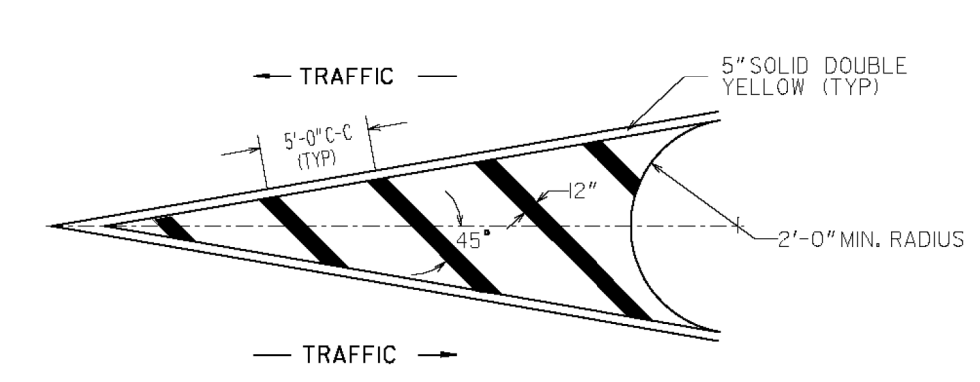

CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

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BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	40-0011

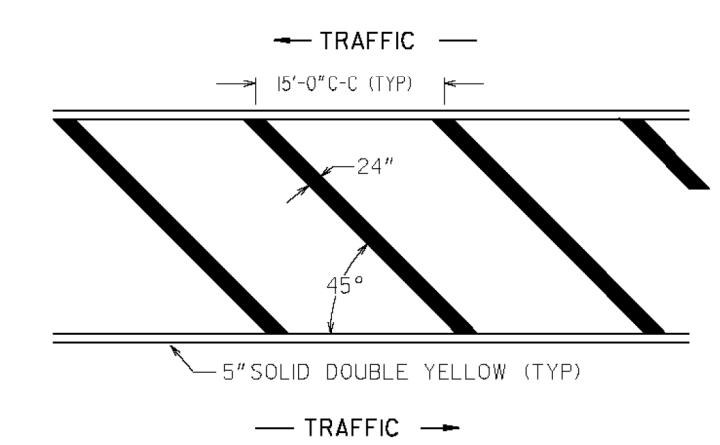


COUNTY	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
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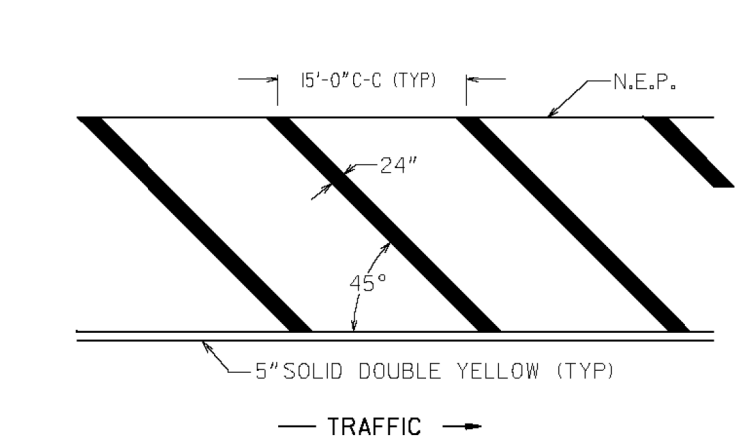
DETAIL "A" (YELLOW)



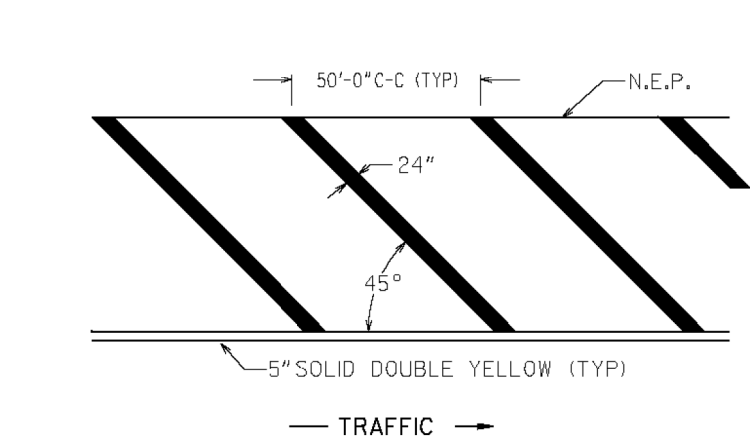
DETAIL "B" (YELLOW)



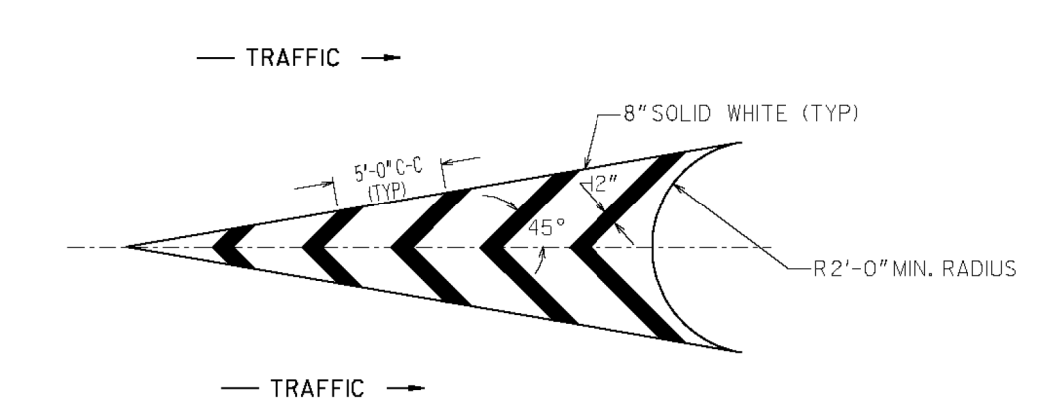
DETAIL "C" (YELLOW)



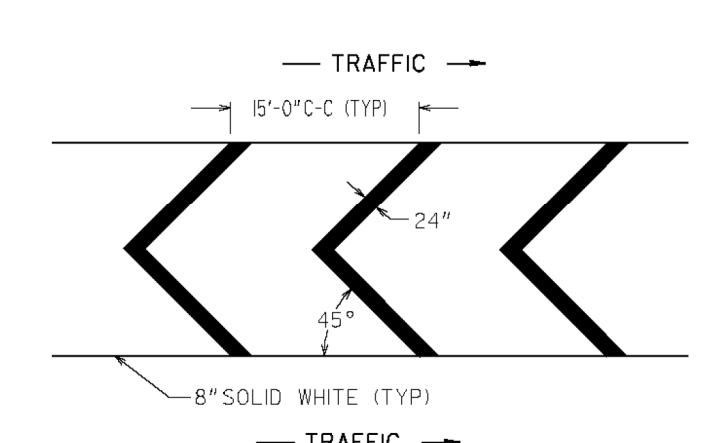
DETAIL "D" (YELLOW)



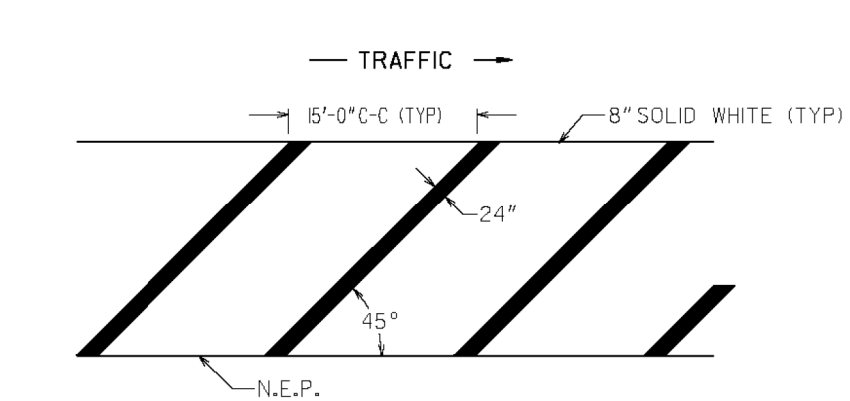
DETAIL "A" (WHITE)



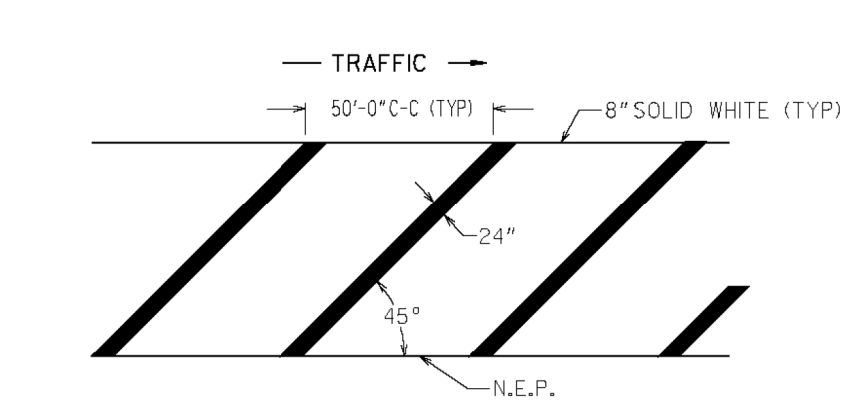
DETAIL "B" (WHITE)



DETAIL "C" (WHITE)



DETAIL "D" (WHITE)



GENERAL NOTES:

1. FOR YELLOW STRIPING, THE SQUARE YARDS SHOWN ON PLAN, SUMMARY AND DETAILED ESTIMATE SHEETS INCLUDE THE AREA WITHIN THE BORDERS AND THE 5" SOLID DOUBLE YELLOW BORDER.
2. FOR WHITE STRIPING, THE SQUARE YARDS SHOWN ON PLAN, SUMMARY AND DETAILED ESTIMATE SHEETS INCLUDES THE AREA WITHIN THE BORDERS AS WELL AS THE 8" SOLID WHITE BORDER.

**GEORGIA**  
DEPARTMENT  
OF  
TRANSPORTATION

- NO SCALE -

DATE	REVISIONS
6/25/04	Modified general note 1
1/18/05	CHANGED BORDER
11/21/08	Modified general note 1

STATE OF GEORGIA  
DEPARTMENT OF TRANSPORTATION  
OFFICE: TRAFFIC OPERATIONS  
SIGNING AND MARKING PLANS

DETAIL OF PAVEMENT MARKING  
HATCHING

NUMBER T-14  
JANUARY 2000

**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING

NOT TO SCALE

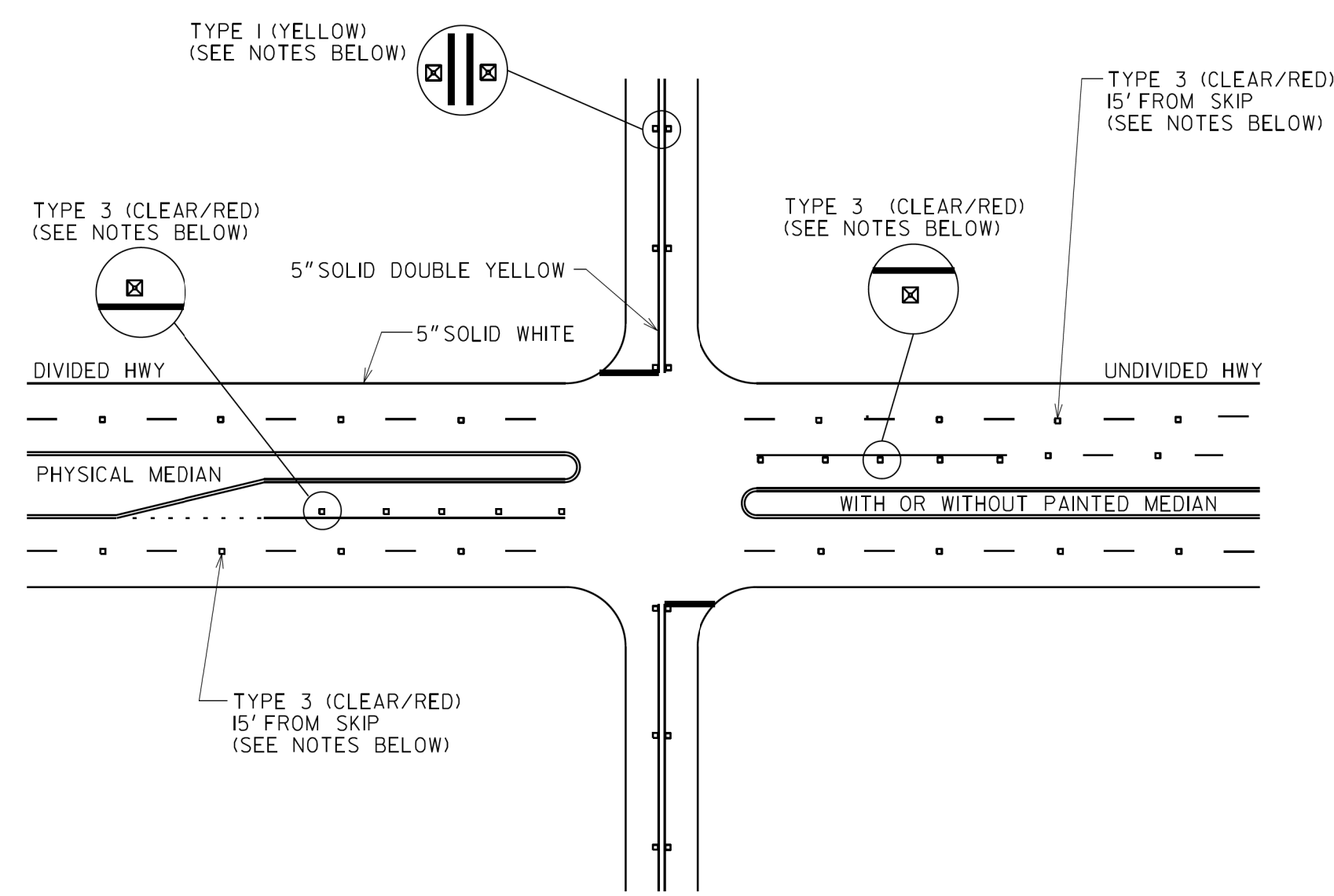
REVISION DATES	

**CONSTRUCTION DETAILS**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

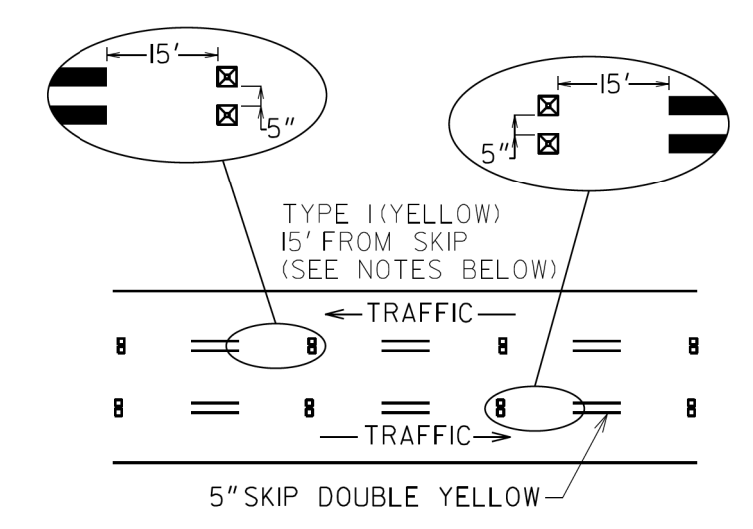
CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	40-0012
CORRECTED:	DATE:	
VERIFIED:	DATE:	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

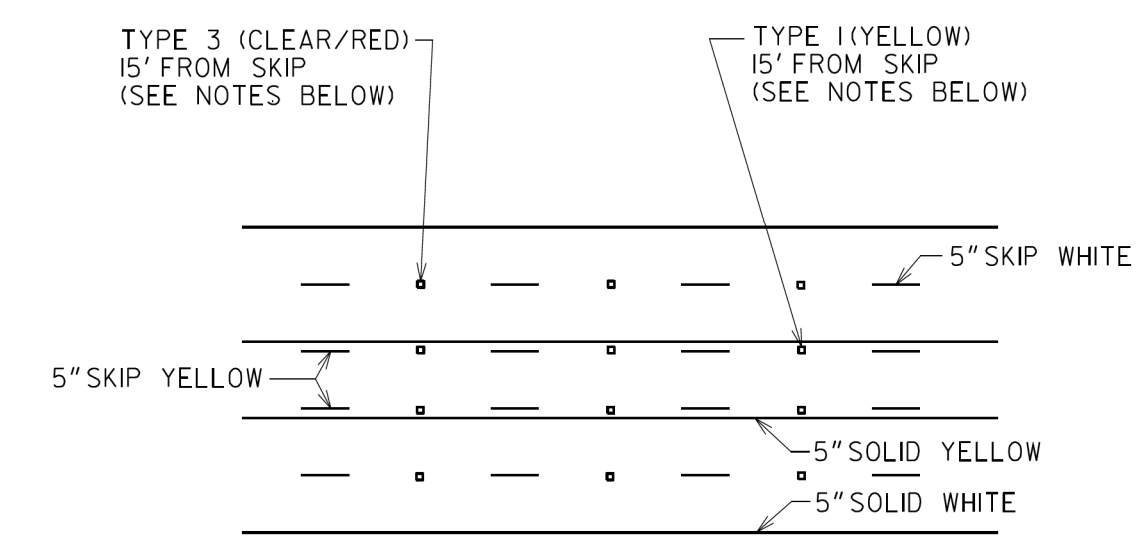
**DIVIDED / UNDIVIDED HIGHWAY**



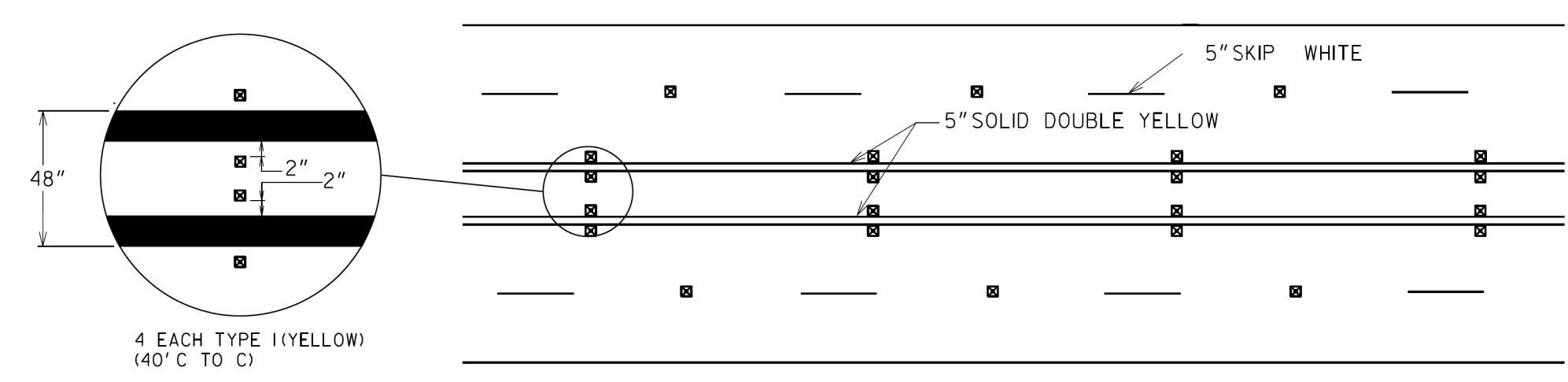
**REVERSIBLE LANE**



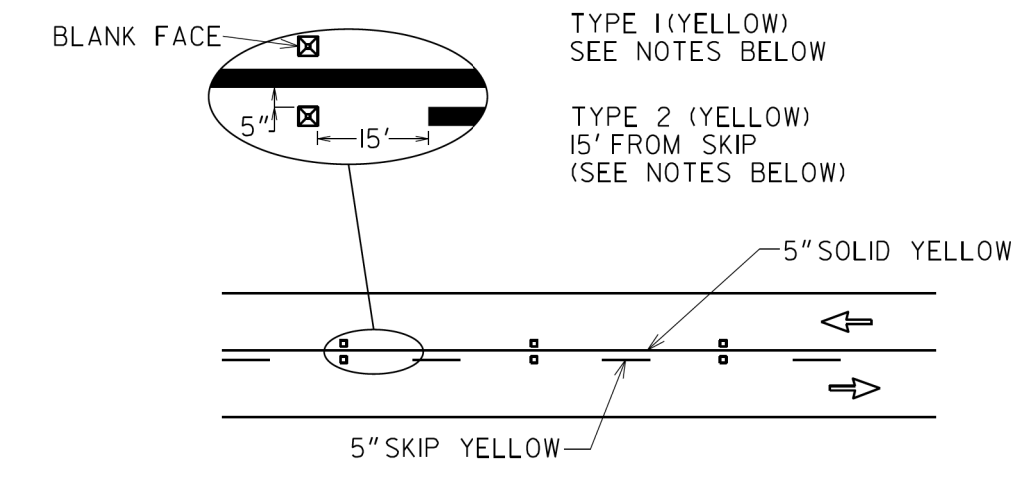
**TWO WAY LEFT TURN LANE**



**4'-0" STRIPED FLUSH MEDIAN**

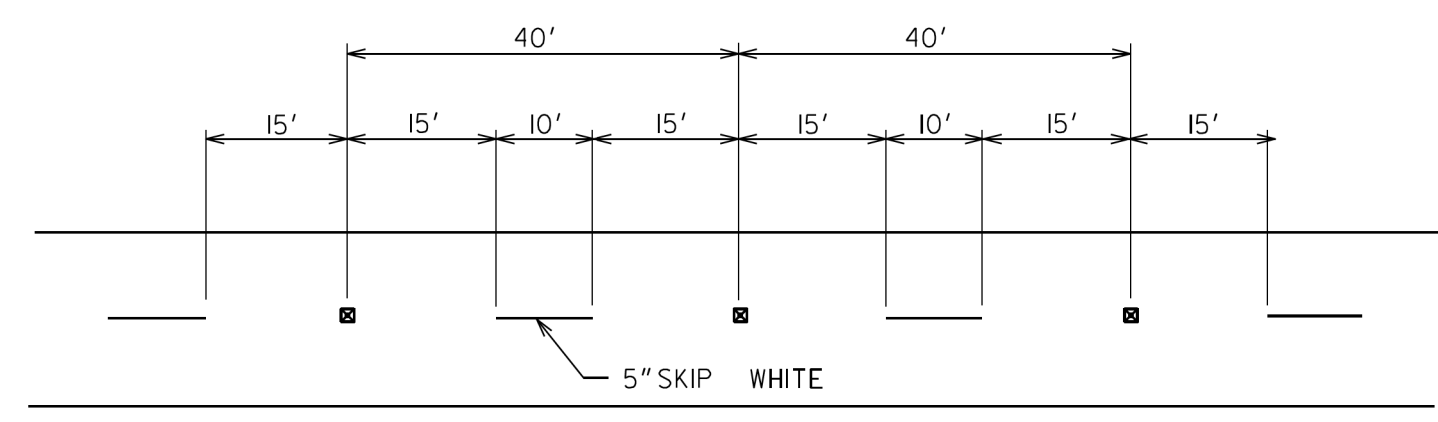


**NO PASSING ZONE**



- GENERAL NOTES:
1. RAISED PAVEMENT MARKERS SHALL BE SPACED EVERY 40 FT UNLESS OTHERWISE SPECIFIED.
  2. ON SOLID WHITE TURN BAY LINES, SPACING SHALL BE 20 FT.
  3. RAISED PAVEMENT MARKERS SHALL BE OFFSET 5 INCHES FROM SOLID LANE LINES.
  4. CLEAR FACE OF TYPE 3 RAISED PAVEMENT MARKERS SHALL BE ORIENTED TOWARD ONCOMING TRAFFIC.

**TYPICAL RPM/STRIPE SPACING**



DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
CONSTRUCTION DETAILS RAISED PAVEMENT MARKER LOCATION NON-LIMITED ACCESS ROADWAY	
NO SCALE	REV. AND REDRAWN, JUNE 2015
COR BY	DESIGNED _____ DRAWN _____ TRACED _____ CHECKED _____
	NUMBER T-15A

**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING

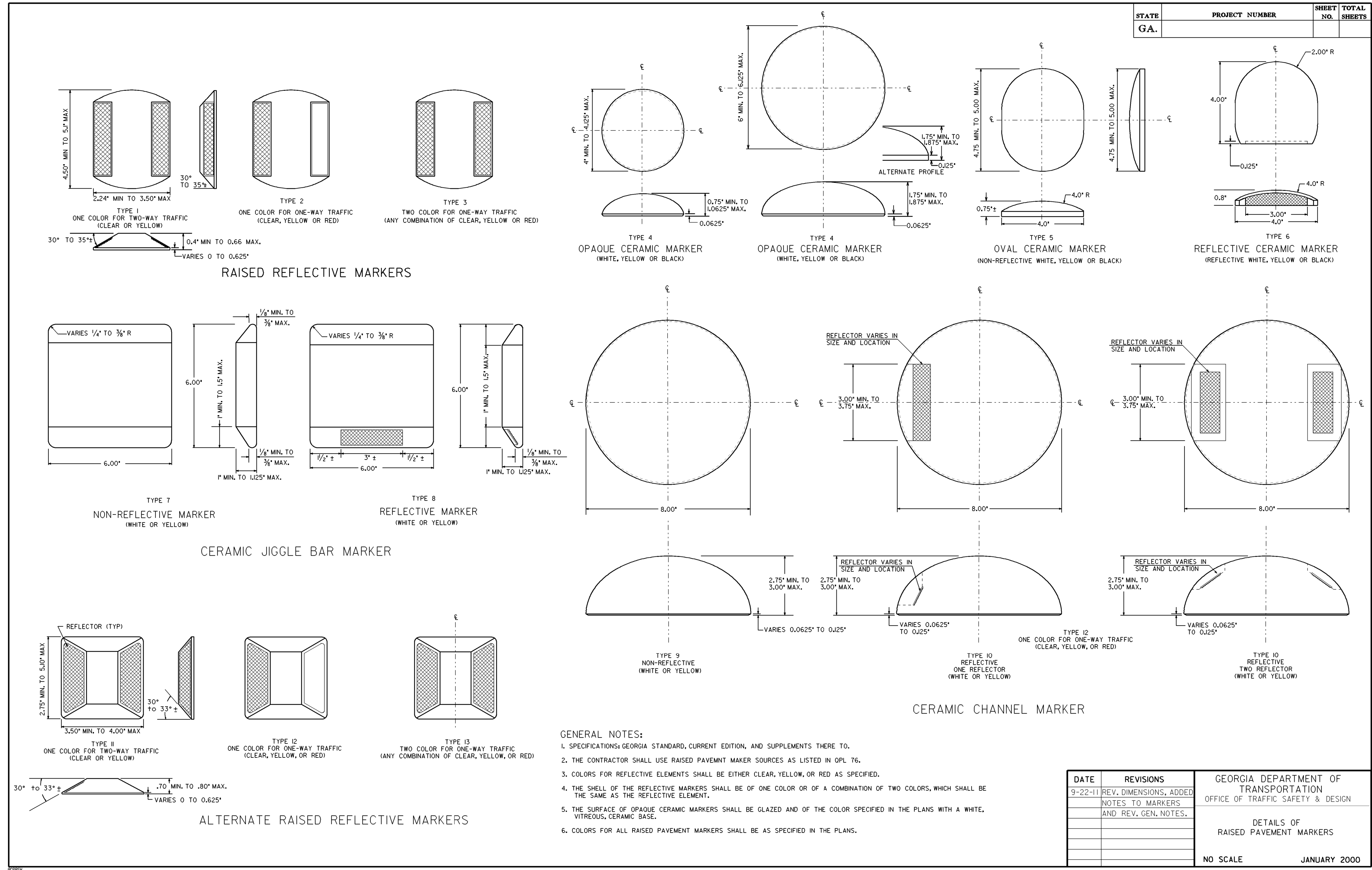
NOT TO SCALE

REVISION DATES	
8/16/2019	

**CONSTRUCTION DETAILS**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

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BACKCHECKED:	DATE:	40-0013
CORRECTED:	DATE:	
VERIFIED:	DATE:	

9/22/2011 10:58:57 AM \\GDDOT-DSN1\GDDOT\GDDOT\GDDOT\K1p8000.qcf gowens V:\GARY\Raised Pavement Markings\T15c.prf 60-RD6



STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

- GENERAL NOTES:**
- SPECIFICATIONS: GEORGIA STANDARD, CURRENT EDITION, AND SUPPLEMENTS THERE TO.
  - THE CONTRACTOR SHALL USE RAISED PAVEMENT MAKER SOURCES AS LISTED IN OPL 76.
  - COLORS FOR REFLECTIVE ELEMENTS SHALL BE EITHER CLEAR, YELLOW, OR RED AS SPECIFIED.
  - THE SHELL OF THE REFLECTIVE MARKERS SHALL BE OF ONE COLOR OR OF A COMBINATION OF TWO COLORS, WHICH SHALL BE THE SAME AS THE REFLECTIVE ELEMENT.
  - THE SURFACE OF OPAQUE CERAMIC MARKERS SHALL BE GLAZED AND OF THE COLOR SPECIFIED IN THE PLANS WITH A WHITE, VITREOUS, CERAMIC BASE.
  - COLORS FOR ALL RAISED PAVEMENT MARKERS SHALL BE AS SPECIFIED IN THE PLANS.

DATE	REVISIONS
9-22-11	REV. DIMENSIONS, ADDED NOTES TO MARKERS AND REV. GEN. NOTES.

GEORGIA DEPARTMENT OF TRANSPORTATION  
OFFICE OF TRAFFIC SAFETY & DESIGN

DETAILS OF RAISED PAVEMENT MARKERS

NO SCALE JANUARY 2000

PC807B 9/22/2011 10:58:57 AM \\GDDOT-DSN1\GDDOT\GDDOT\GDDOT\K1p8000.qcf gowens V:\GARY\Raised Pavement Markings\T15c.prf

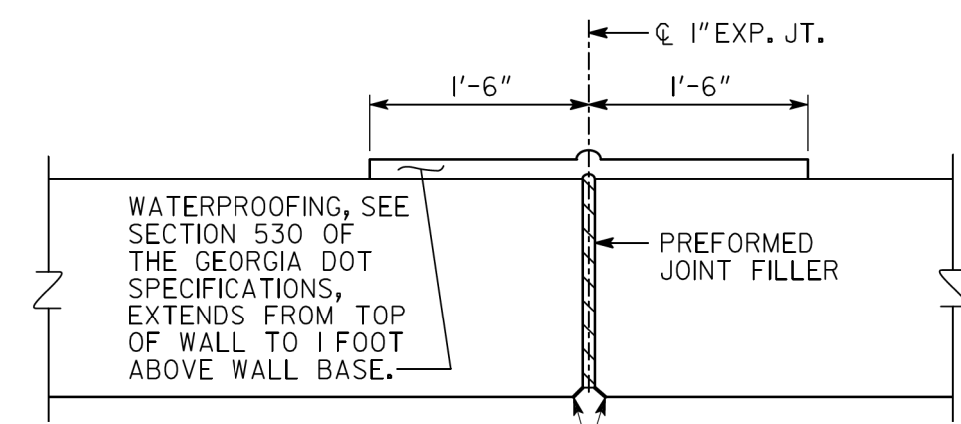
T-15C

REVISION DATES	
8/16/2019	

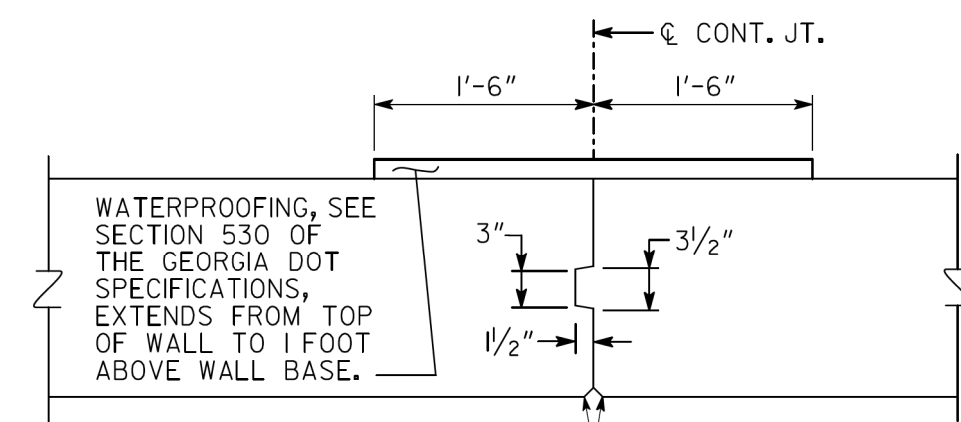
**CONSTRUCTION DETAILS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

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BACKCHECKED:	DATE:	
CORRECTED:	DATE:	40-0014
VERIFIED:	DATE:	

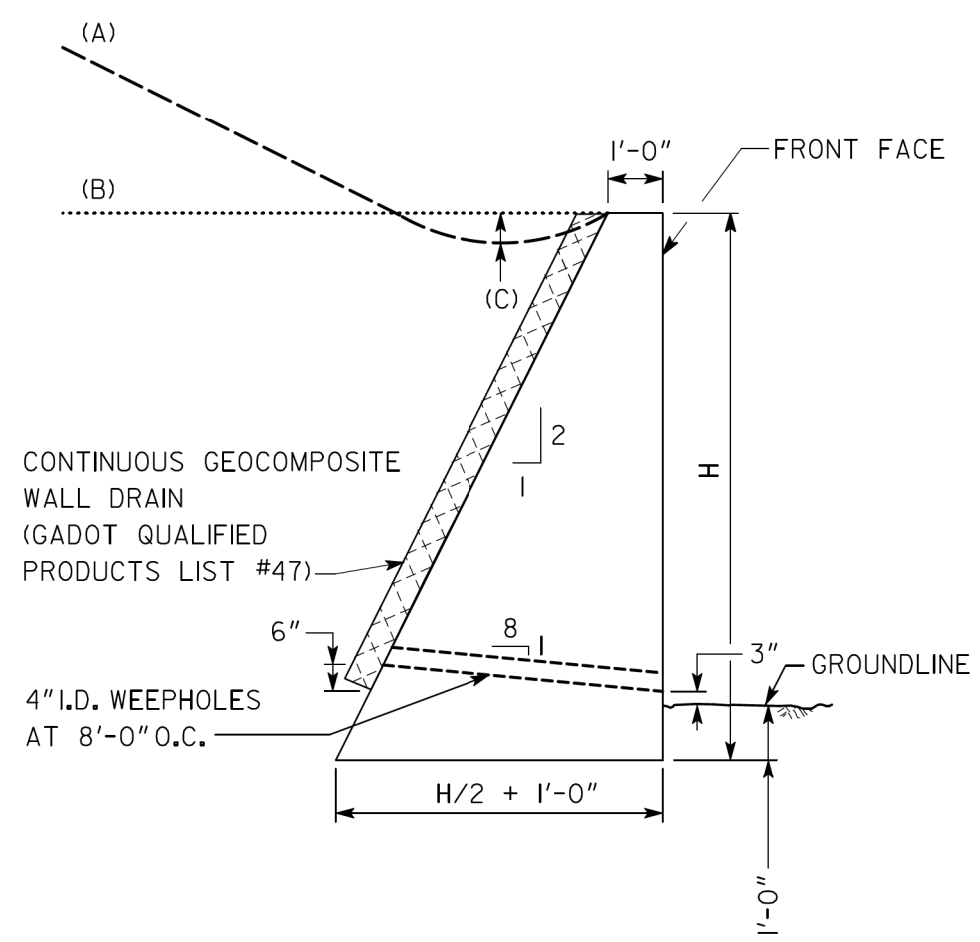
STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



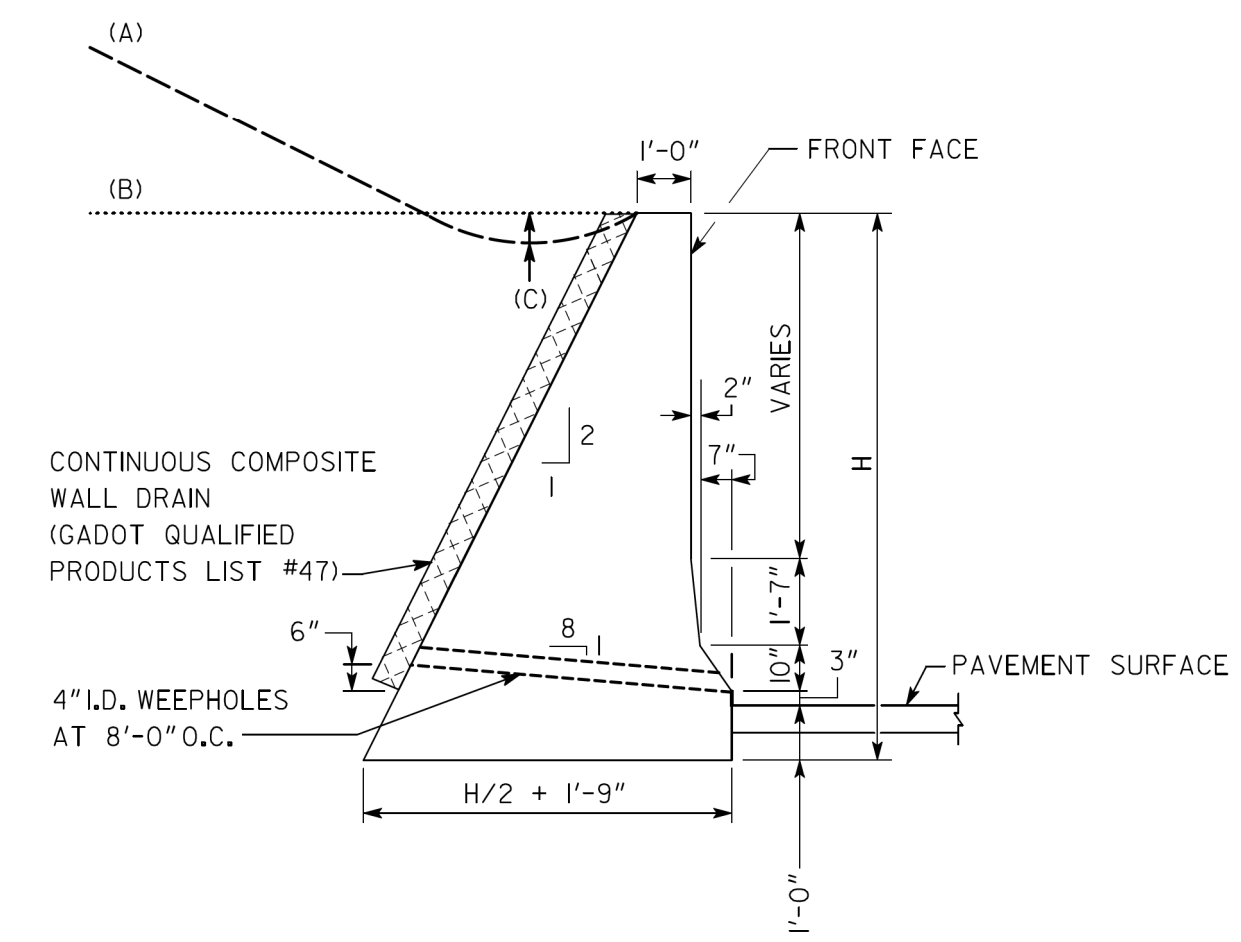
DETAIL OF EXPANSION JOINT  
SEE GENERAL NOTE #3



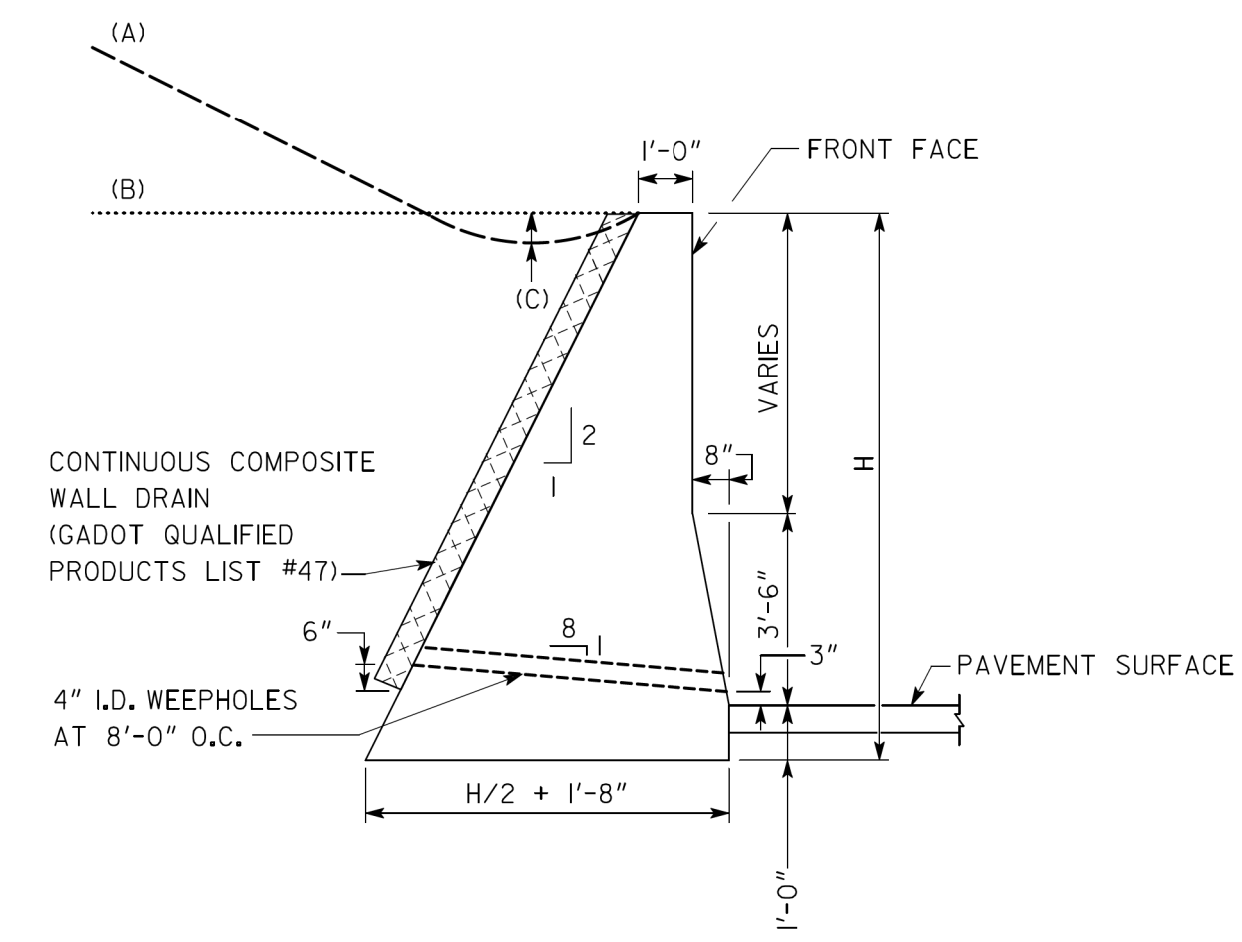
DETAIL OF CONTRACTION JOINT  
SEE GENERAL NOTE #3



TYPICAL SECTION A



TYPICAL SECTION B  
(NEW JERSEY BARRIER FACE)



TYPICAL SECTION C  
(SINGLE SLOPE BARRIER FACE)

BACKSLOPE	MAXIMUM "H" *		
	TYP. SECTION A	TYP. SECTION B **	TYP. SECTION C **
FLAT	8'-6"	10'-0"	10'-0"
SLOPE TO 4H	6'-3"	7'-0"	7'-0"
SLOPE TO 2H	4'-6"	4'-9"	4'-9"

\* GREATER "H" PERMITTED IF APPROVED BY BRIDGE DESIGN.  
\*\* TYPICAL SECTION B SHALL HAVE A MINIMUM H OF 3'-8"  
TYPICAL SECTION C SHALL HAVE A MINIMUM H OF 4'-6"

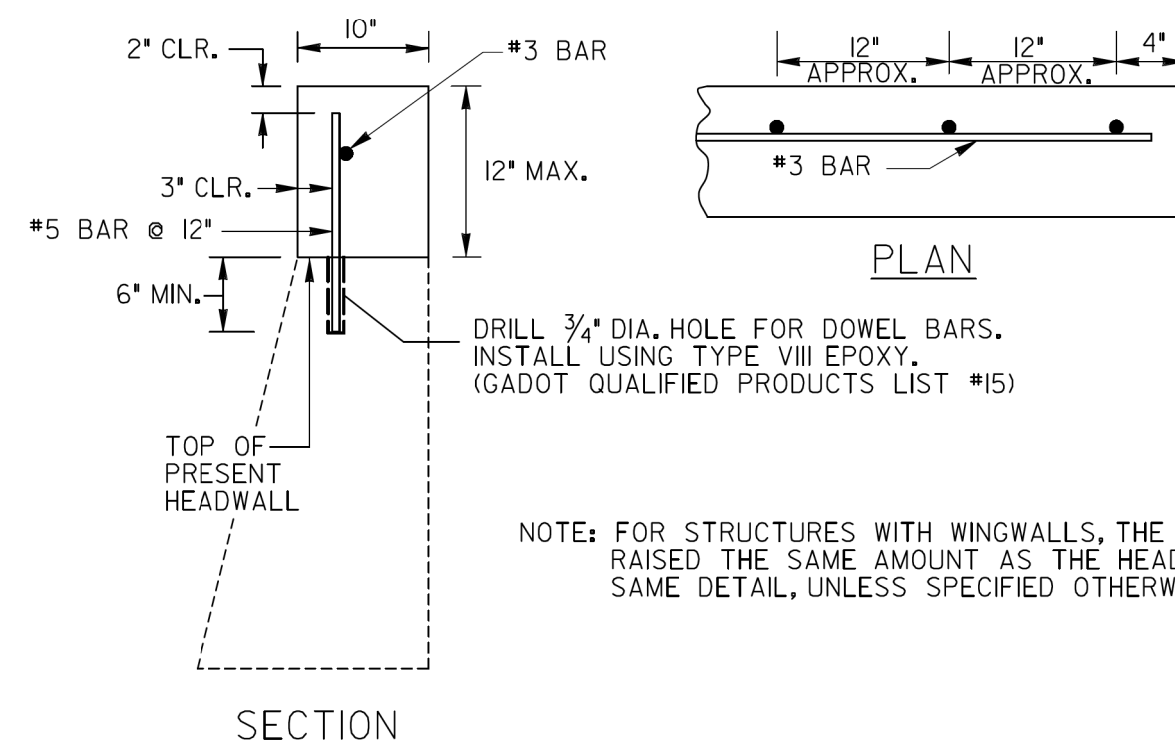
GENERAL NOTES:

- GRAVITY WALLS SHALL NOT BE USED WHEN HORIZONTAL DISTANCE FROM EDGE OF TRAVEL WAY TO FRONT FACE OF WALL IS LESS THAN (H + 1'0").
- GRAVITY WALLS DESIGNED FOR THE FOLLOWING SOIL PROPERTIES:  

	FOUNDATION	BACKFILL
COHESION =	0 PSF	0 PSF
φ =	28°	28°
UNIT WEIGHT =	120 PCF	120 PCF
- EXPANSION JOINTS SHALL BE LOCATED AT A MAXIMUM SPACING OF 90'-0" AND EXTEND THROUGH THE WALL. CONTRACTION JOINTS SHALL BE LOCATED AT A MAXIMUM SPACING OF 30'-0".
- GRAVITY WALLS WITH A VERTICAL FRONT FACE SHALL BE PAID FOR AS "CLASS B CONCRETE OR MORTAR RUBBLE MASONRY, RETAINING WALL". GRAVITY WALLS WITH A BARRIER FRONT FACE SHALL BE PAID FOR AS "CLASS A CONCRETE, RETAINING WALL". WATERPROOFING, JOINT FILLER, WALL DRAIN, AND OTHER INCIDENTAL ITEMS SHALL BE INCLUDED IN OVERALL BID SUBMITTED.

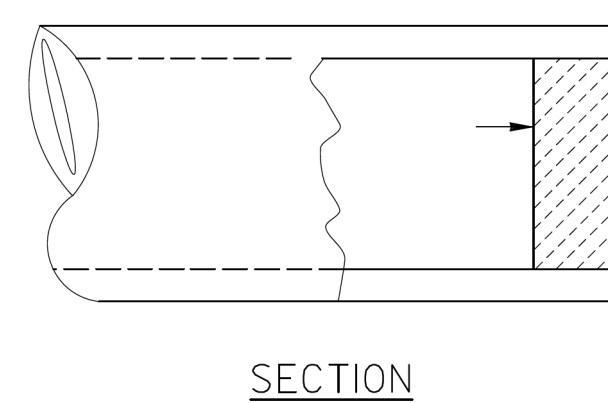
- A CONCRETE DITCH DETAIL FOR THE TOP OF THE WALL SHOULD BE INCLUDED IN THE ROADWAY PLANS WHEN WATER IS FLOWING TOWARDS THE BACK OF THE WALL. SEE CONSTRUCTION DETAIL D-49.
- FINISH EXPOSED SURFACES OF THE WALL WITH A TYPE III FINISH.
- APPLY GRAFFITI PROOF COATING AS PER SECTIONS 500 AND 838 OF THE GEORGIA DOT SPECIFICATIONS.
- ALL NECESSARY FENCE AND HANDRAIL SHOULD BE INCLUDED IN THE ROADWAY PLANS WHEN APPROPRIATE.
- GRAVITY WALL TYPICAL SECTIONS A, B, AND C HAVE BEEN DESIGNED PER THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 7TH EDITION, 2014.

DETAIL FOR RAISING HEADWALL



NOTE: FOR STRUCTURES WITH WINGWALLS, THE WINGS SHALL BE RAISED THE SAME AMOUNT AS THE HEADWALL, USING THIS SAME DETAIL, UNLESS SPECIFIED OTHERWISE.

TYPICAL PIPE PLUG



NOTE: PLAN PAY QUANTITIES ARE TO REFLECT PIPE PLUGS AS CU. YDS. OF CL. B CONCRETE. ON CONSTRUCTION PLUGS MAY BE BUILT WITH BRICK MASONRY, MORTAR RUBBLE MASONRY, CL. A CONC. OR CL. B CONC. WITH NO ADJUSTMENT IN PAYMENT MADE FOR ALTERNATES.

D	T (MIN)	PIPE PLUG (CU. YDS.)
12"	8"	0.0194
15"	8"	0.0303
18"	8"	0.0436
24"	8"	0.0776
30"	8"	0.1212
36"	8"	0.1745
42"	8"	0.2376
48"	8"	0.3103
54"	12"	0.5890
60"	12"	0.7272
66"	12"	0.8799
72"	12"	1.0472

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

STANDARD  
GRAVITY WALL TYPICAL SECTIONS,  
RAISING HEADWALL, AND  
TYPICAL PIPE PLUG

NO SCALE; REV. & REDR. SEPT. 2016

BY	REV. & C.E.M. (SUBMITTED)	NUMBER
CHK. D.D.E.	REDR. STATE ROAD & AIRPORT DESIGN ENGR.	9031L
	(APPROVED) STATE HIGHWAY ENGINEER	SHEET 1 OF 2

REVISION DATES

NO.	DATE	DESCRIPTION

GEORGIA STANDARDS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	41-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

INFRASTRUCTURE  
CONSULTING & ENGINEERING

NOT TO SCALE

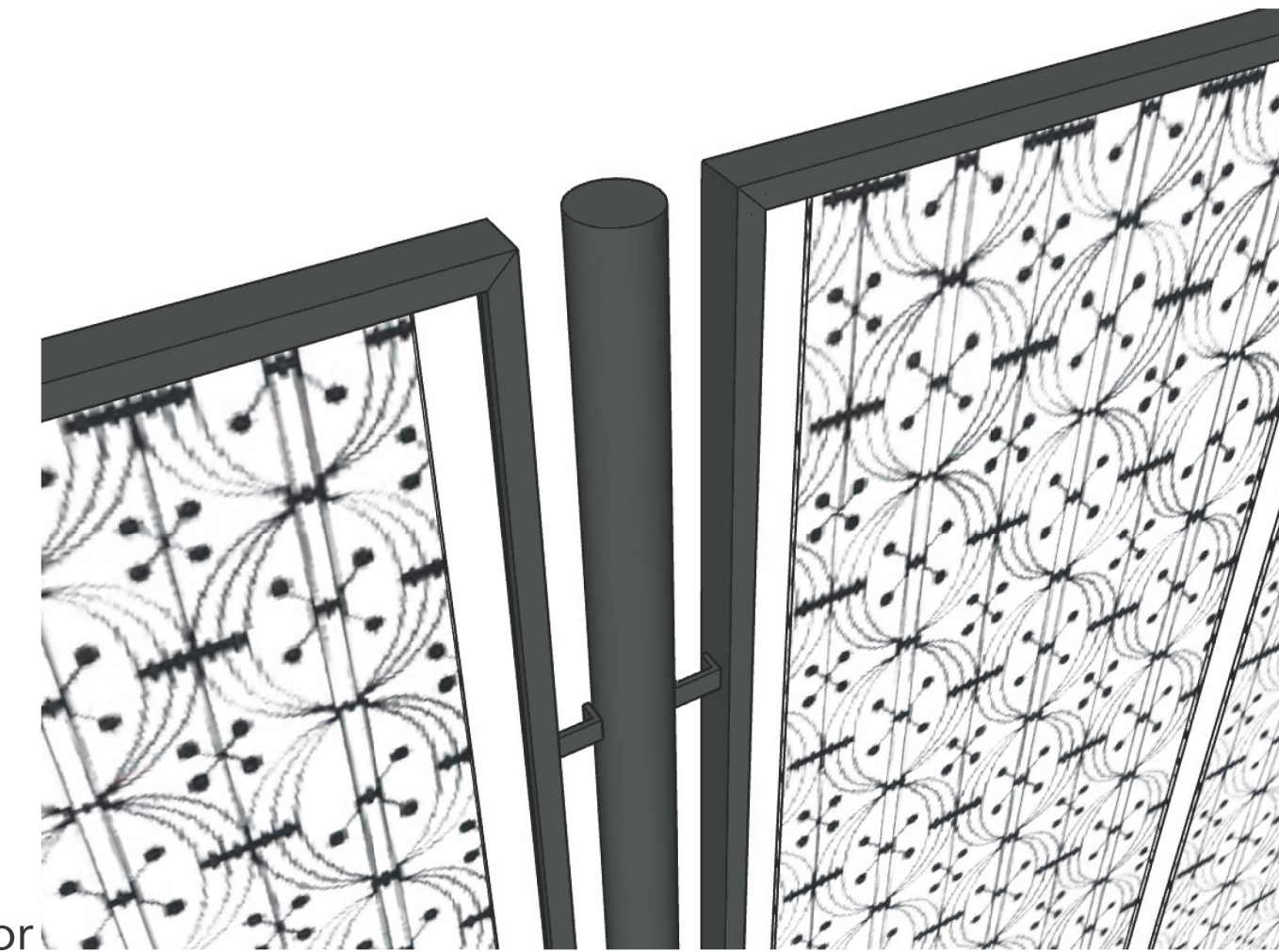
# Guard Railing Systems

Material: Hot rolled Steel  
Finish: Powdercoat finish  
Infil Panel Design: TBD  
Connectors hardware: Stainless Steel 304  
Base insert: Stainless steel sleeve, 2" diameter, backfilled with non-shrink grout.  
Inset depth to be 12" minimum.

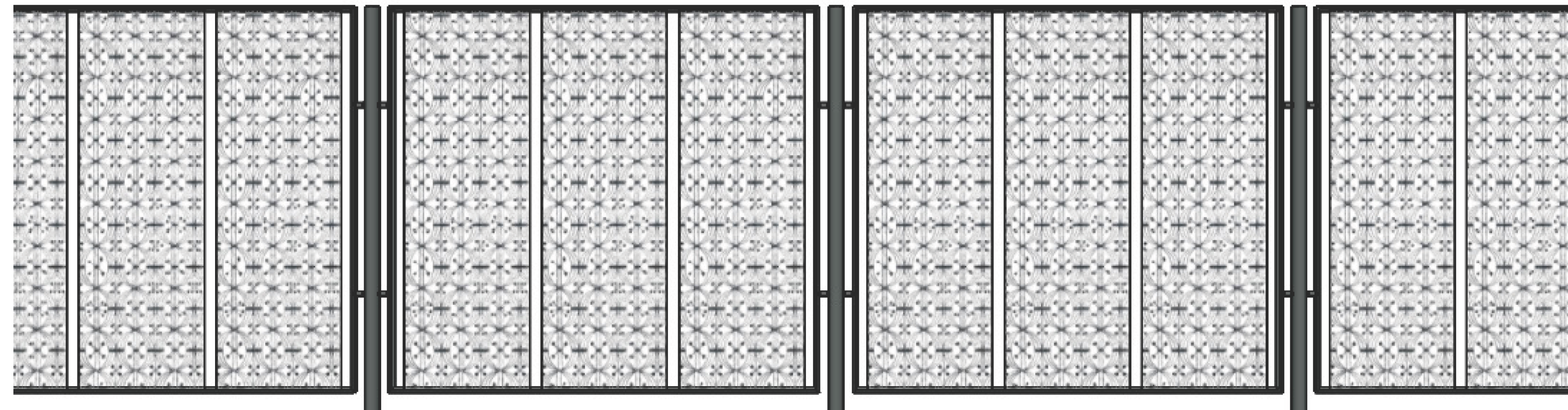
Notes:

1. The railings will be a square pipe rail frame, at 4 foot intervals, that accepts digitally fabricated metal panels that can reflect the unique situations and locations that they are installed in.
2. All steel to be shop welded and finished.
3. Finish assembly to be completed in field.

Color: RAL 9007



Detail at Connector



STREETSCAPE DESIGN STANDARDS | CITY OF BROOKHAVEN | AUGUST 28, 2018

ST-03

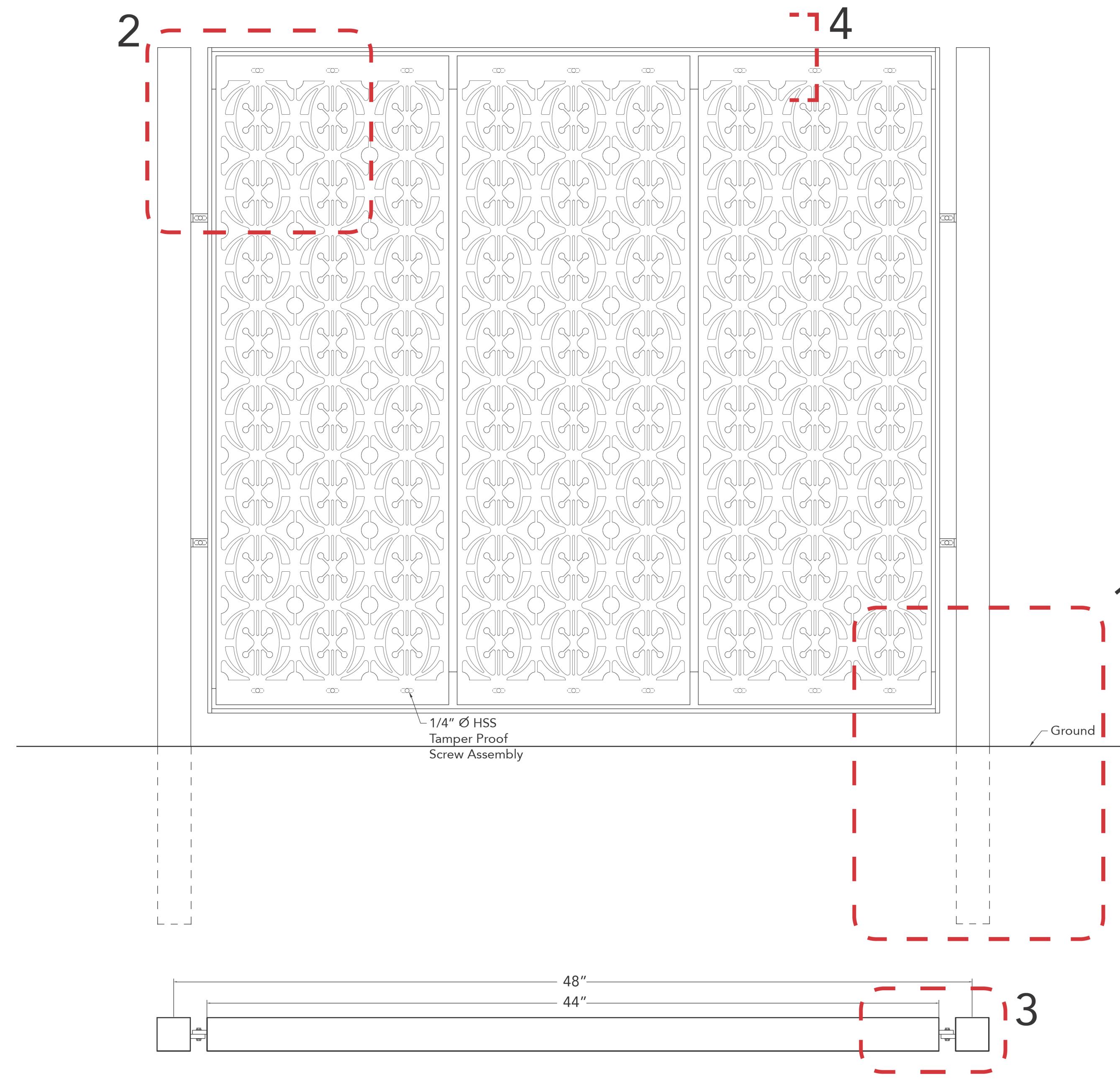


NOT TO SCALE

REVISION DATES	
8/16/2019	

GEORGIA STANDARDS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:	DATE:	DRAWING No.
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VERIFIED:	DATE:	



Typical Bay Elevation



Typical Bay Plan



STREETSCAPE DESIGN STANDARDS | CITY OF BROOKHAVEN | AUGUST 28, 2018

ST-03

**JE** INFRASTRUCTURE  
CONSULTING & ENGINEERING

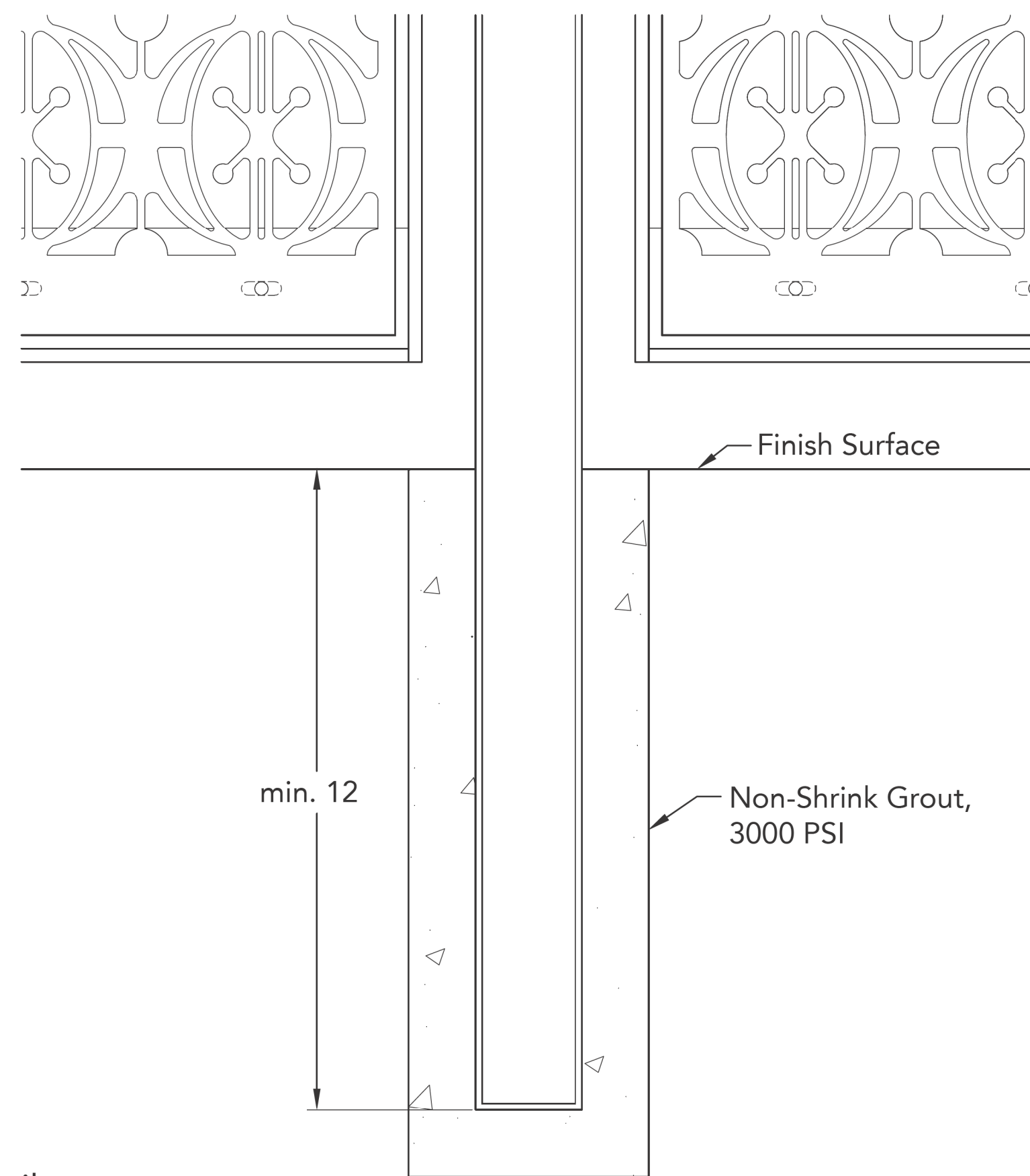
NOT TO SCALE

REVISION DATES	
8/16/2019	

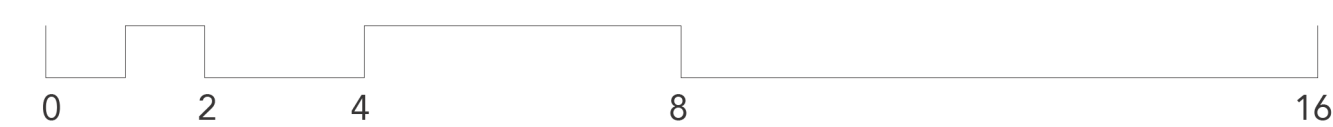
**GEORGIA STANDARDS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

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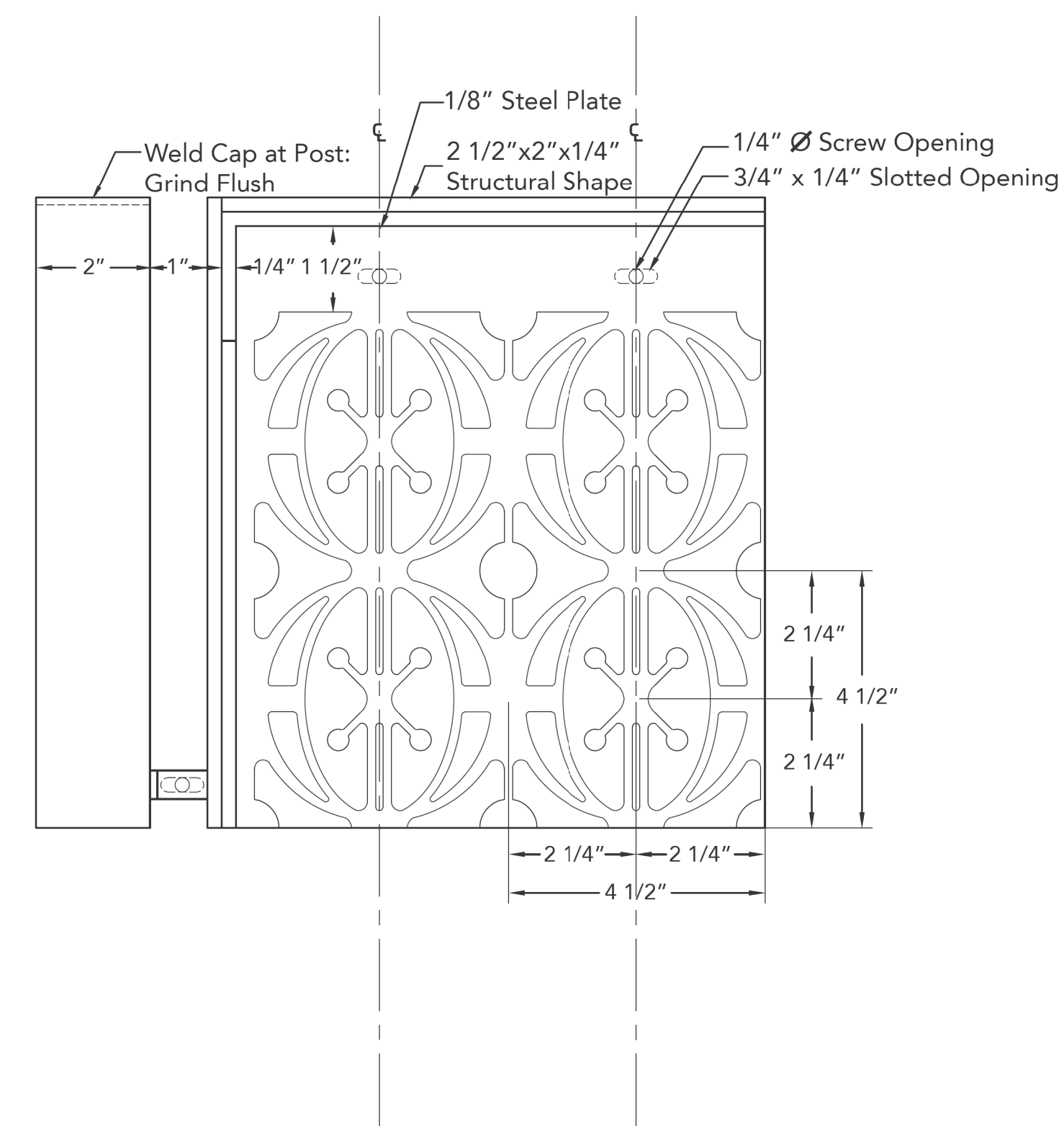
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Post Anchor Detail



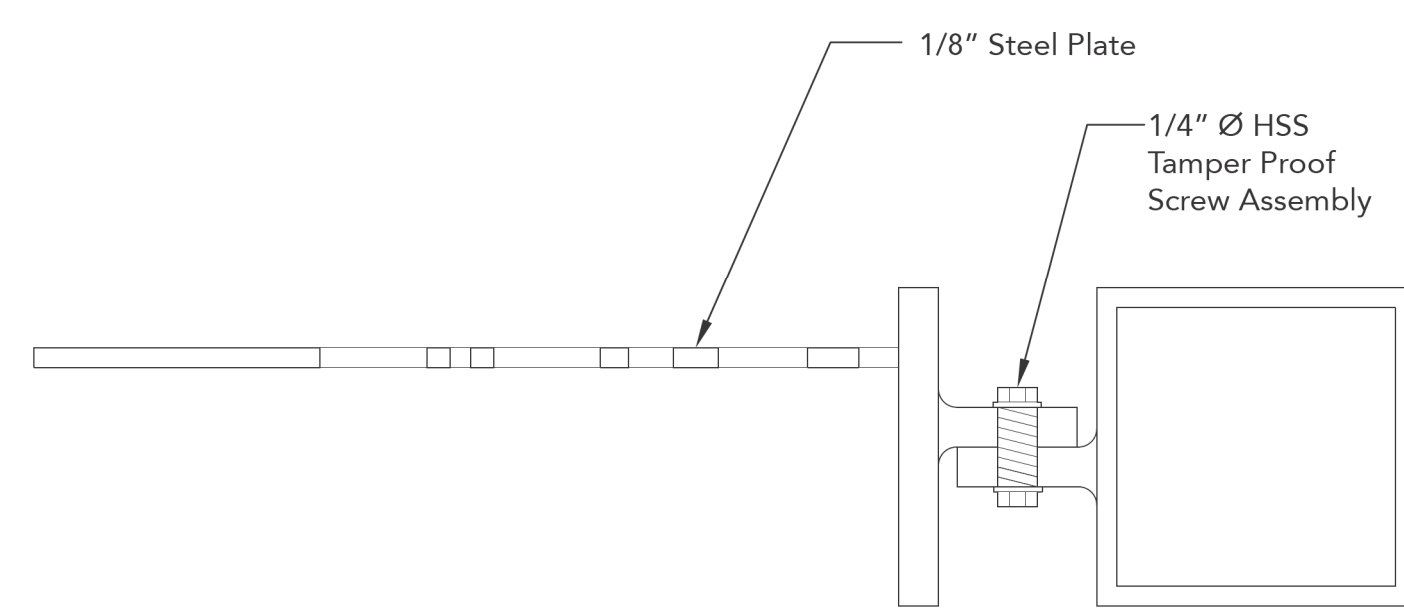
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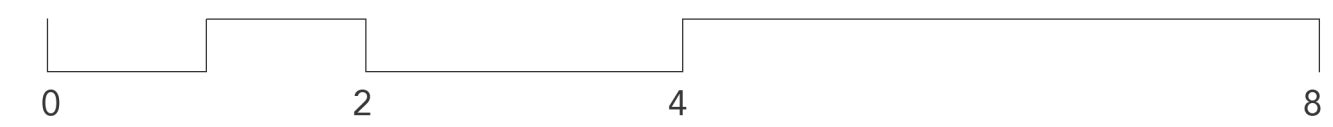
Elevation Detail at Panel



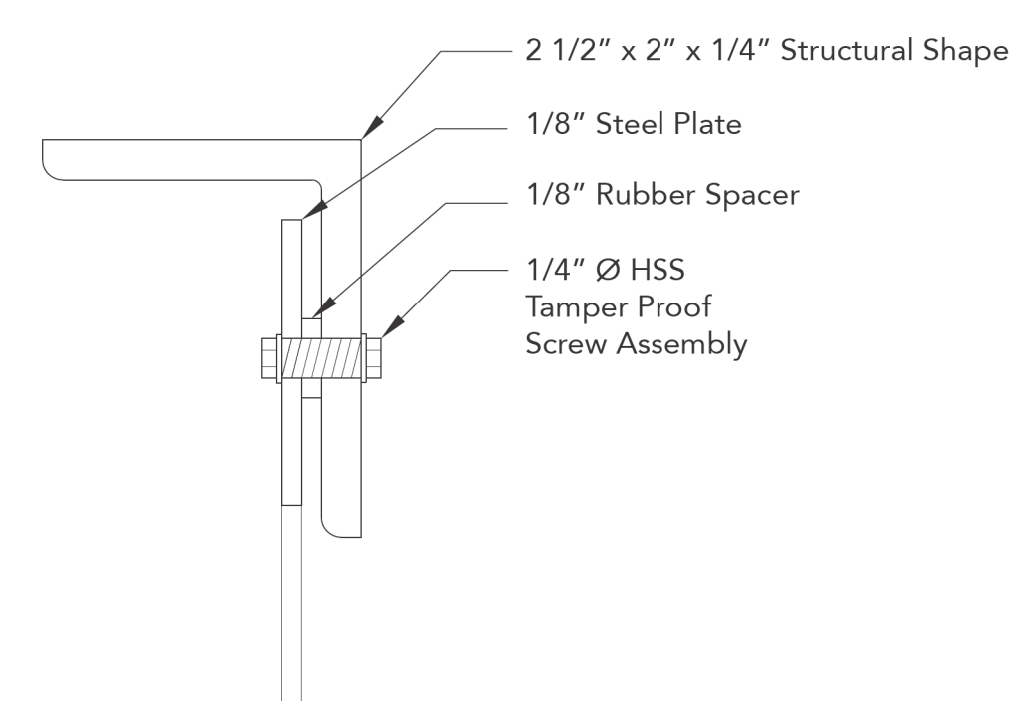
3



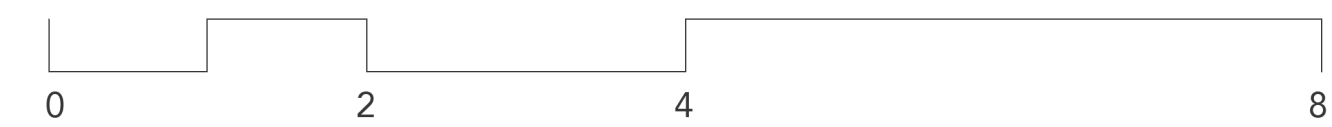
Plan Detail at Frame/Post Connection



4



Section Detail at Frame/Panel Connection



STREETSCAPE DESIGN STANDARDS | CITY OF BROOKHAVEN | AUGUST 28, 2018

ST-03

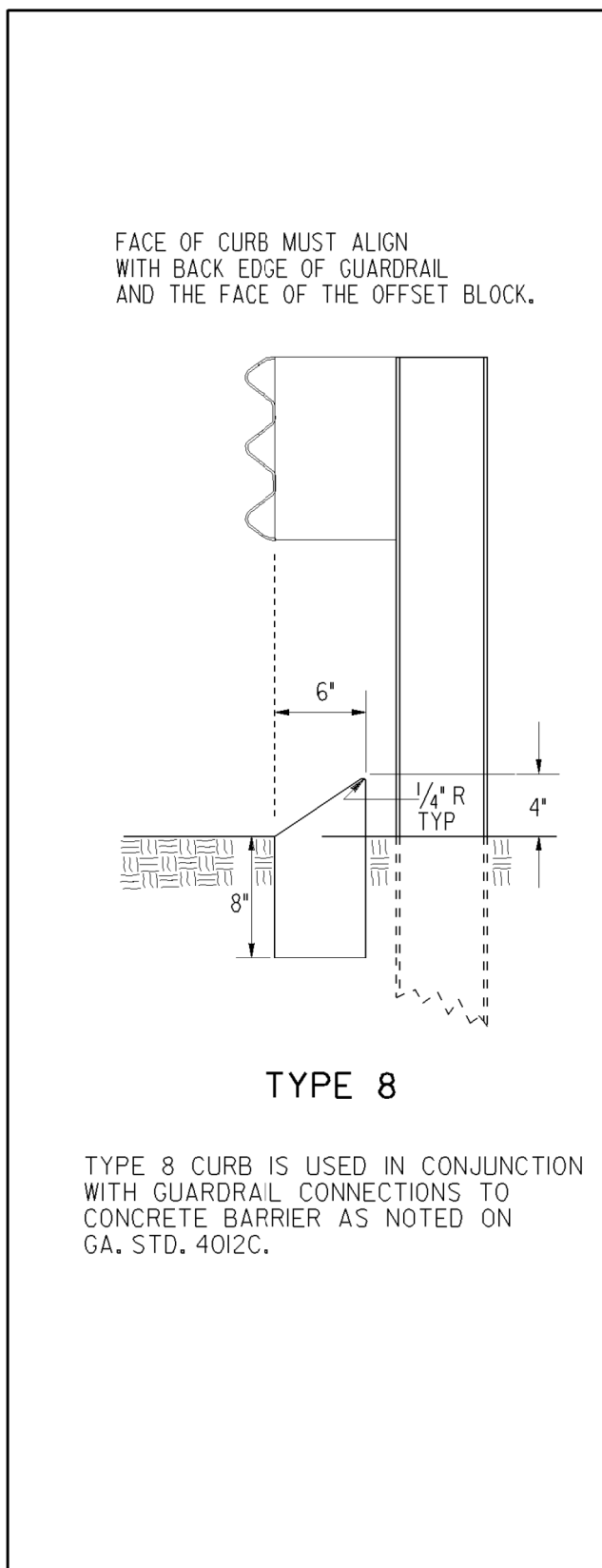
REVISION DATES	
8/16/2019	

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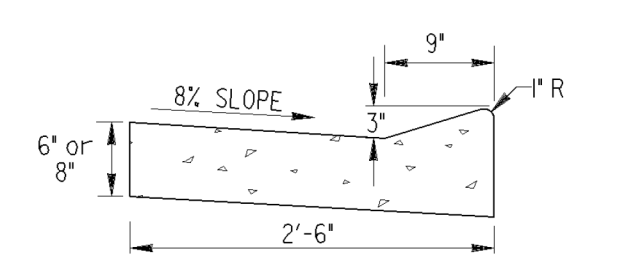
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STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

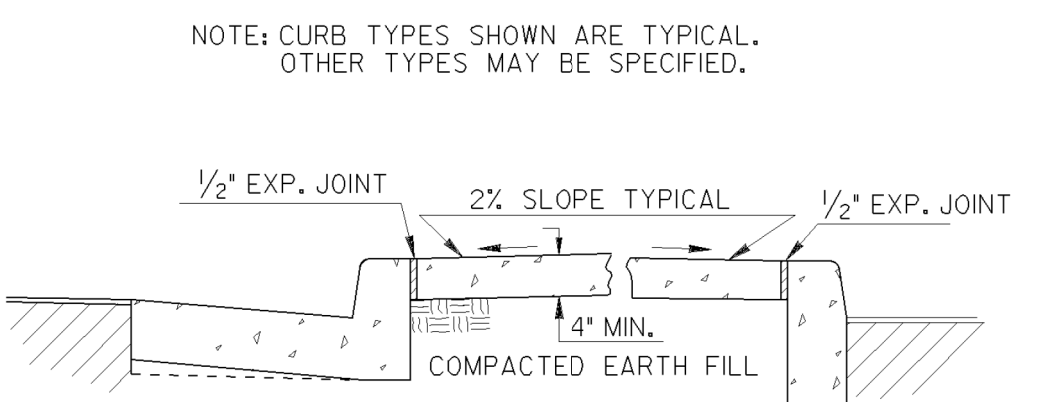


**RAISED EDGE WITH CONCRETE GUTTER**

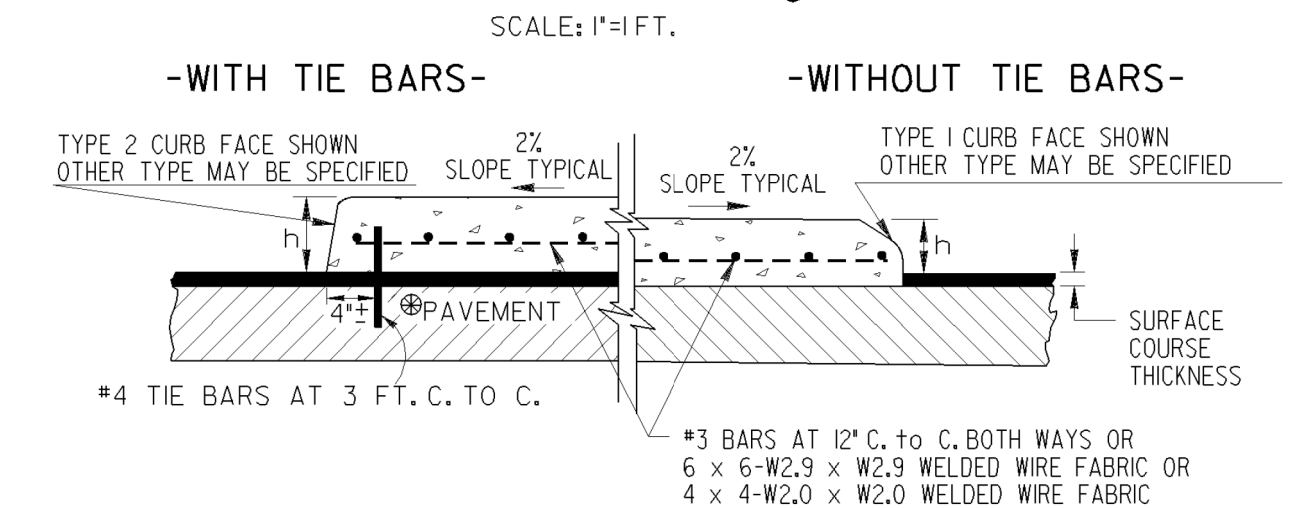


RAISED EDGE TO BE CONSTRUCTED WITH SAME CONCRETE MIX AS THE GUTTER AND SHALL BE FORMED MONOLITHIC WITH GUTTER. JOINTS IN RAISED EDGE SHALL MATCH THOSE IN THE GUTTER.

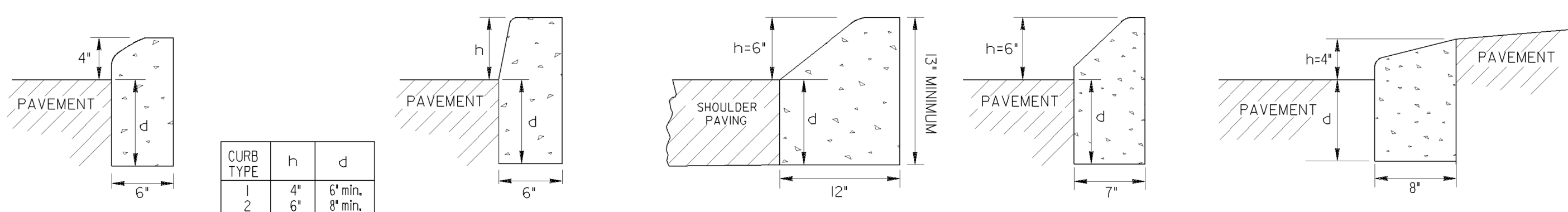
**CONCRETE MEDIAN (Between Curbs)**



**CONCRETE MEDIANS (Integral)**

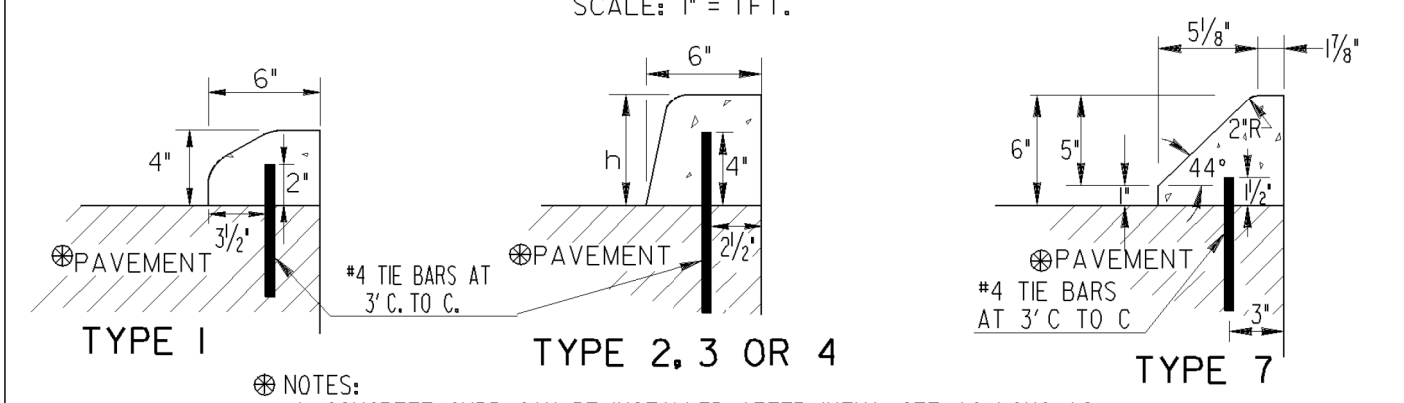


**CONCRETE HEADER CURBS**



THE DIMENSION d MAY BE INCREASED AT CONTRACTOR'S OPTION SO BOTTOM OF HEADER CURB WILL ALIGN WITH BOTTOM OF PAVEMENT TYPICAL SECTION.

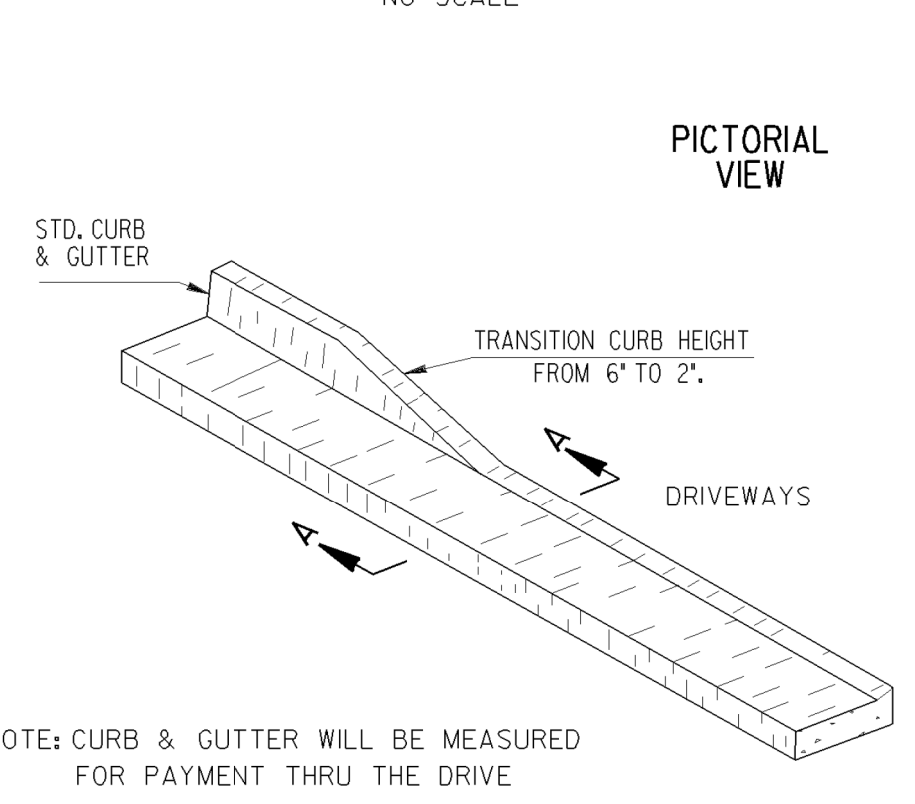
**CONCRETE DOWELED INTEGRAL CURBS**



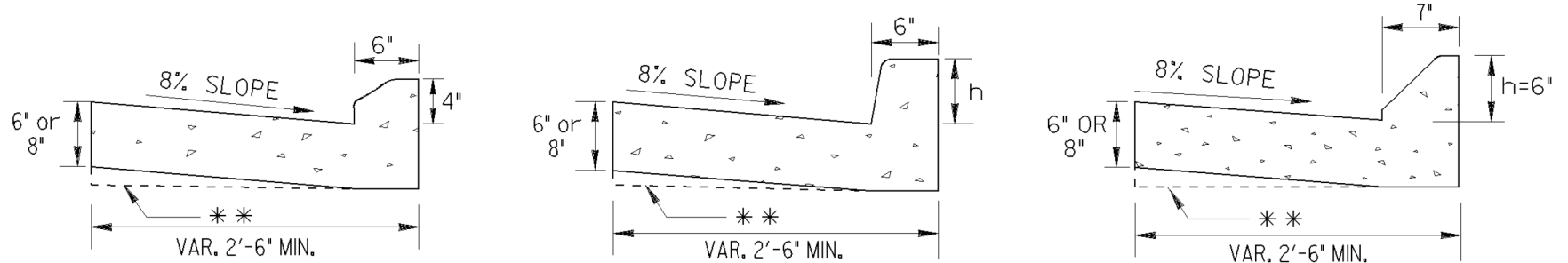
- NOTES:
- CONCRETE CURB CAN BE INSTALLED AFTER INITIAL SET AS LONG AS TIE BARS ARE DRILLED INTO UNDERLYING CONCRETE PAVEMENT.
  - CONCRETE CURB CAN BE INSTALLED BEFORE INITIAL SET WITH DOWELS THAT ARE DRIVEN INTO UNDERLYING CONCRETE PAVEMENT.
  - JOINTS IN CURB AND CONCRETE MEDIAN WILL MATCH THOSE IN THE CONCRETE PAVEMENT.
  - ALL TYPES OF CONCRETE CURB CAN BE PLACED ON ASPHALT PAVEMENTS WHERE TIE BARS MAY BE EITHER DRIVEN OR DRILLED INTO THE UNDERLYING PAVEMENT. CONTRACTION JOINTS SHALL BE CONSTRUCTED IN CURB OR CONCRETE MEDIAN AT 20 FT. SPACING.

MINIMUM TIE BAR LENGTHS (FOR CONC. DOWELED CURBS OR CONC. MEDIAN)		
CURB TYPE	P.C. CONC. PAV.	ASPHALT PAV.
1	6'	8'
2, 3 or 4	8'	12'
7	6'	8'

**DETAILS OF RECESSED CURB FOR DRIVEWAYS**

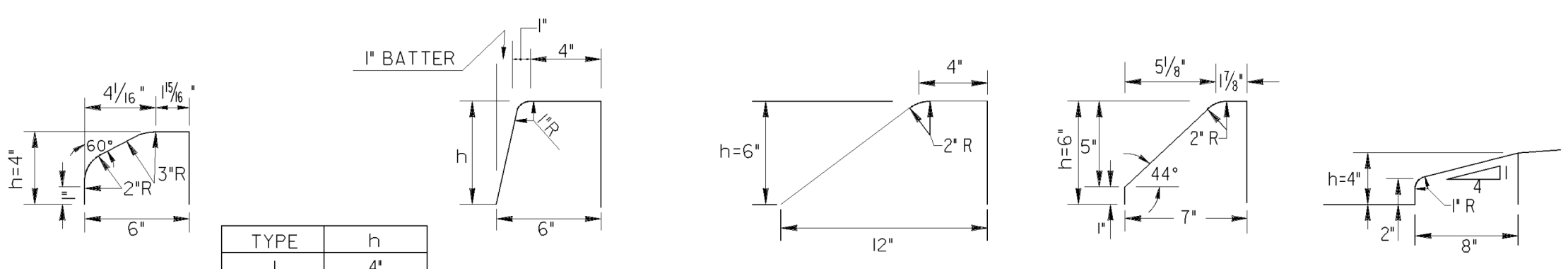


**CONCRETE CURB & GUTTER**

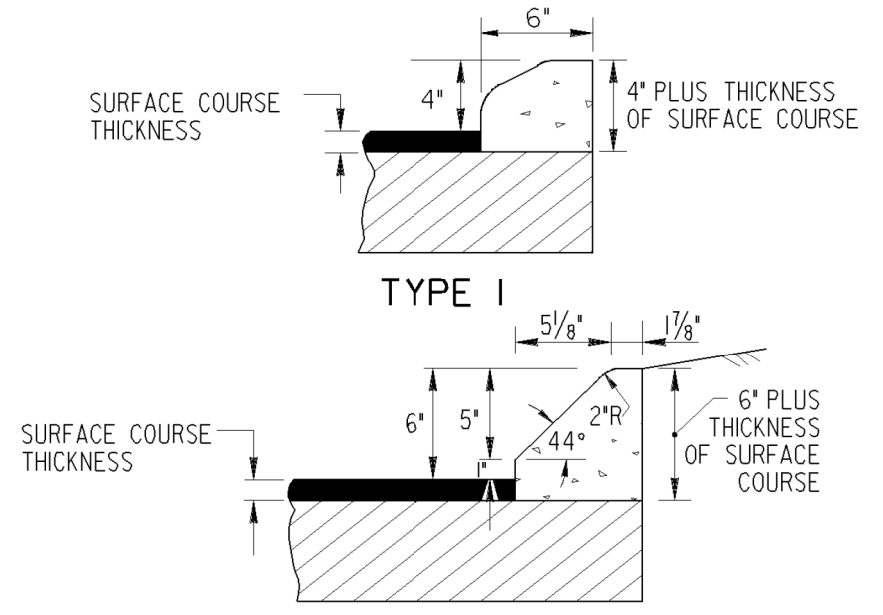


\*\* AT CONTRACTOR'S OPTION THE GUTTER THICKNESS MAY BE INCREASED AT EDGE OF PAVEMENT TO MAKE BOTTOM OF GUTTER PARALLEL WITH PAVING OF BASE COURSE, BUT THE GUTTER THICKNESS MUST NOT BE LESS THAN THE SPECIFIED 6" OR 8" AT ANY POINT.

**CURB FACE DESIGN**



**CONCRETE INTEGRAL CURB**



DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

STANDARD  
CONCRETE CURB & GUTTER  
CONCRETE CURBS, CONCRETE MEDIANS

SCALE: AS SHOWN REVISED AND REDRAWN OCT. 2011

TC	REV. TYPE 9 CURB DETAIL & REV. OVERALL LAYOUT	11-5-11	DATE
GL	REV. MEDIAN NOTE AND ADDED TYPE 9 CURB DETAIL	1-27-11	DATE
BY	ADDED TYPE 9 DETAIL	3-03	REVISION

DES. (SUBMITTED) *[Signature]*

DRW. STATE DESIGN POLICY ENGINEER

TRA. (APPROVED) *[Signature]*

CHK. CHIEF ENGINEER

NUMBER 9032B

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**INFRASTRUCTURE**  
CONSULTING & ENGINEERING

NOT TO SCALE

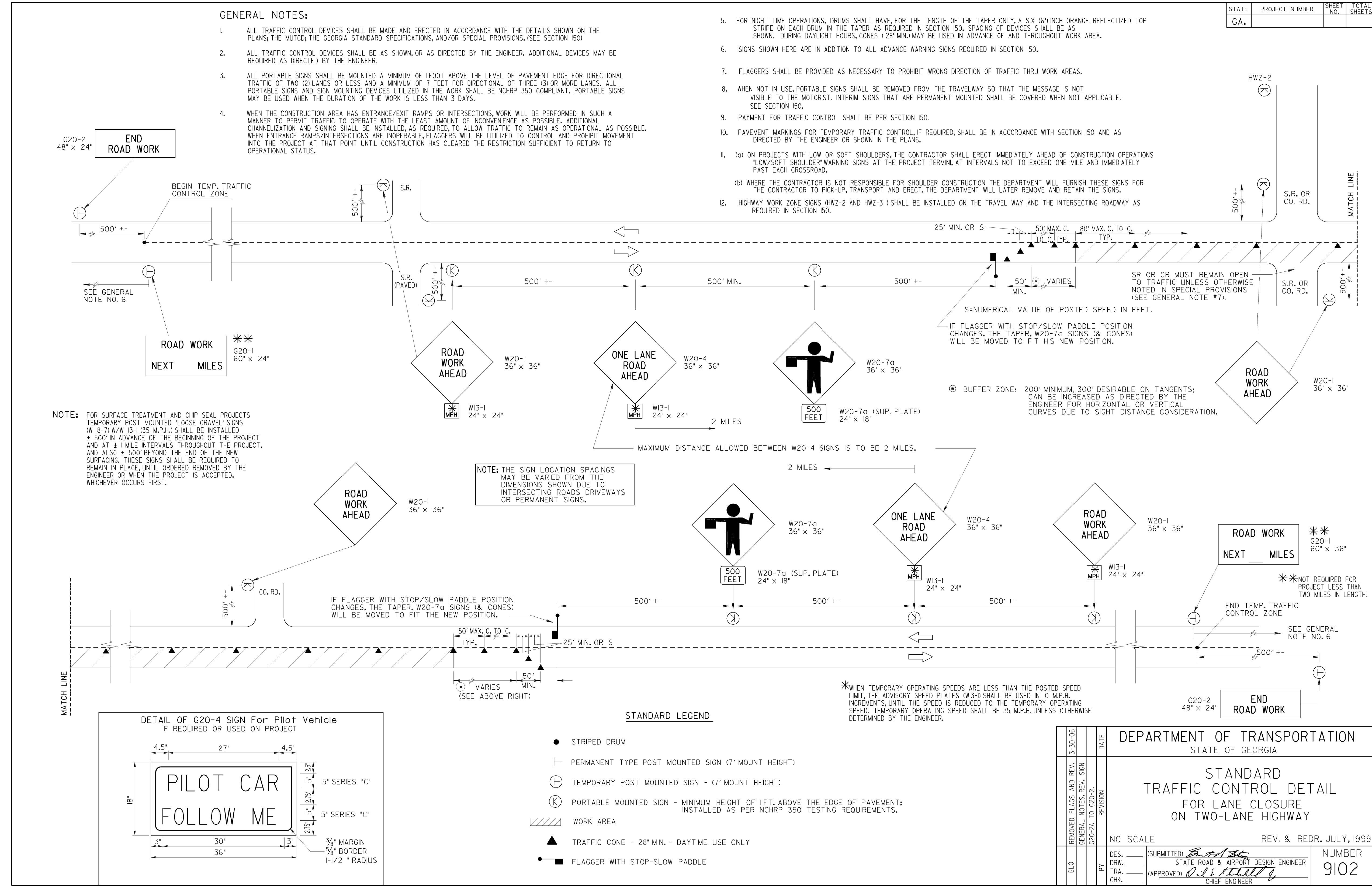
**REVISION DATES**

**GEORGIA STANDARDS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

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BACKCHECKED:	DATE:	41-0003
CORRECTED:	DATE:	
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REVISION DATES

**GEORGIA STANDARDS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	41-0004
CORRECTED:	DATE:	
VERIFIED:	DATE:	

ESPCP GENERAL NOTES

1. AN N. O. I. (NOTICE OF INTENT) IS NOT REQUIRED FOR THIS PROJECT. THE TOTAL DISTURBED AREA IS 0.45 ACRES. THE TOTAL PROJECT AREA IS 0.57 ACRES.
2. PRIOR TO ANY OTHER CONSTRUCTION, A STABILIZED CONSTRUCTION ENTRANCE SHALL BE CONSTRUCTED AT EACH ENTRY TO OR EXIT FROM THE SITE.
3. THE CONSTRUCTION EXITS SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ON TO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH STONE, AS CONDITIONS DEMANDS, AND REPAIR AND/OR CLEAN-OUT OF ANY STRUCTURES USED TO TRAP SEDIMENT. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLE ONTO PUBLIC ROADWAY OR INTO STORM DRAIN MUST BE REMOVED.
4. THERE NO ARE STATE WATERS LOCATED WITHIN 200 FEET OF THE PROJECT AREA.
5. PRIOR TO COMMENCING LAND DISTURBANCE ACTIVITY, THE LIMITS OF LAND DISTURBANCE SHALL BE CLEARLY AND ACCURATELY DEMARCATED WITH STAKES, RIBBONS, OR OTHER APPROPRIATE MEANS. THE LOCATION AND EXTENT OF ALL AUTHORIZED LAND DISTURBANCE SHALL OCCUR WITHIN THE APPROVED LIMITS INDICATED ON THE APPROVED PLANS.
6. IMMEDIATELY AFTER THE ESTABLISHMENT OF CONSTRUCTION ENTRANCES/EXITS, ALL PERIMETER EROSION CONTROL DEVICES AND STORM WATER MANAGEMENT DEVICES SHALL BE INSTALLED PRIOR TO ANY OTHER CONSTRUCTION.
7. OWNER AGREES TO PROVIDE AND MAINTAIN OFF-STREET PARKING ON THE SUBJECT PROPERTY DURING THE ENTIRE CONSTRUCTION PERIOD.
8. THE CONTRACTOR SHALL FURNISH AND MAINTAIN ALL NECESSARY BARRICADES WHILE ROADWAY FRONTAGE IMPROVEMENTS ARE BEING MADE.
9. THE CONSTRUCTION OF THE SITE WILL INITIATE WITH THE INSTALLATION OF EROSION CONTROL MEASURES SUFFICIENT TO CONTROL SEDIMENT DEPOSITS AND EROSION. ALL SEDIMENT CONTROL WILL BE MAINTAINED UNTIL ALL UP STREAM GOUND WITHIN THE CONSTRUCTION AREA HAS BEEN COMPLETELY STABILIZED WITH PERMANENT VEGETATION AND ALL ROADS/DRIVEWAYS HAVE BEEN PAVED.
10. FAILURE TO INSTALL, OPERATE OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB SITE UNTIL SUCH MEASURES ARE CORRECTED CONSISTENT WITH THE CITY OF BROOKHAVEN EROSION CONTROL ORDINANCE.
11. A COPY OF THE APPROVED LAND DISTURBANCE PLAN AND PERMIT SHALL BE PRESENT ON THE SITE WHENEVER LAND DISTURBANCE ACTIVITY IS IN PROGRESS.
12. ALL SEWER EASEMENTS DISTURBED MUST BE DRESSED AND GRASSED TO CONTROL EROSION.

13. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE AS NECESSARY.
14. ANY DISTRUBED AREA LEFT EXPOSED SHALL BE TEMPORARILY STABILIZED WITH MULCH OR TEMPORARY SEEDING AS SOON AS POSSIBLE AFTER ROUGH GRADING IS COMPLETED BUT WITHIN 14 DAYS AFTER DISTURBANCE; PERMANENT VEGETATION SHALL BE PLANTED IF THE AREA IS TO BE LEFT UNDISTURBED FOR GREATER THAN 6 MONTHS.
15. THE WASHING OF READY-MIX CONCRETE DRUMS AND DUMP TRUCK BODIES USED IN THE DELIVERY OF PORTLAND CEMENT CONCRETE IS PROHIBITED ON THIS SITE.

IN ACCORDANCE WITH STANDARD SPECIFICATION 107: LEGAL REGULATIONS AND RESPONSIBILITY TO THE PUBLIC, ONLY THE DISCHARGE CHUTE UTILIZED IN THE DELIVERY OF PORTLAND CEMENT CONCRETE MAY BE RINSED FREE OF FRESH CONCRETE REMAINS. THE CONTRACTOR SHALL EXCAVATE A PIT OUTSIDE OF STATE WATER BUFFERS, AT LEAST 25 FEET FROM ANY STORM DRAIN AND OUTSIDE OF THE TRAVELLED WAY, INCLUDING SHOULDERS, FOR A WASH-DOWN PIT. THE PIT SHALL BE LARGE ENOUGH TO STORE ALL WASH-DOWN WATER WITHOUT OVERTOPPING. IMMEDIATELY AFTER THE WASH-DOWN OPERATIONS ARE COMPLETED AND AFTER THE WASH-DOWN WATER HAS SOAKED INTO THE GROUND, THE PIT SHALL BE FILLED IN, AND THE GROUND ABOVE IT SHALL BE GRADED TO MATCH THE ELEVATION OF THE SURROUNDING AREAS. ALTERNATE WASH-DOWN PLANS MUST BE APPROVED BY THE PROJECT ENGINEER.

WASH-DOWN PLANS DESCRIBE PROCEDURES THAT PREVENT WASH-DOWN WATER FROM ENTERING STREAMS AND RIVERS. NEVER DISPOSE OF WASH-DOWN WATER DOWN A STORM DRAIN. ESTABLISH A WASH-DOWN PIT THAT INCLUDES THE FOLLOWING: (1) A LOCATION AWAY FROM ANY STORM DRAIN, STREAM OR RIVER, (2) ACCESS TO THE VEHICLE BEING USED FOR WASH-DOWN, (3) SUFFICIENT VOLUME FOR WASH-DOWN WATER, AND (4) PERMISSION TO USE THE AREA FOR WASH-DOWN.

ON SITES WHERE PERMISSION OR ACCESS TO EXCAVATE A WASH-DOWN PIT IS UNAVAILABLE, THE CONTRACTOR MAY HAVE TO WASH-DOWN INTO A SEALABLE 55-GALLON DRUM OR OTHER SUITABLE CONTAINER AND THEN TRANSPORT THE CONTAINER TO A PROPER DISPOSAL SITE. FOR ADDITIONAL INFORMATION, REFER TO THE GEORGIA SMALL BUSINESS ENVIRONMENTAL ASSISTANCE PROGRAM'S "A GUIDE FOR READY MIX CHUTE/HOPPER WASH-DOWN".



REVISION DATES	
8/16/2019	

**ESPCP GENERAL NOTES  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	51-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
	ORANGE BARRIER FENCE		ORANGE BARRIER FENCE DELINEATES ENVIRONMENTALLY SENSITIVE AREAS WHERE THE CONTRACTOR SHALL NOT CLEAR, GRUB, OR PLACE CONSTRUCTION MATERIALS OR EQUIPMENT WITHIN THIS AREA.
		LINE CODE 	
ESA	ENVIRONMENTALLY SENSITIVE AREA		AN ENVIRONMENTALLY SENSITIVE AREA (ESA) CONTAINS RESOURCES THAT ARE ENVIRONMENTALLY, CULTURALLY, OR HISTORICALLY SENSITIVE. ESAs INCLUDE, BUT ARE NOT LIMITED TO: STATE WATER BUFFERS, HISTORIC SITES, ARCHAEOLOGICAL SITES, AND PROTECTED ANIMAL AND PLANT SPECIES HABITATS.  IF WORK IS AUTHORIZED IN THIS AREA, THE WORK MUST BE PERFORMED IN ACCORDANCE WITH SECTION 107 AND ANY OTHER APPLICABLE SPECIAL PROVISIONS AND APPLICABLE PLAN NOTES.
		LINE CODE 	
Bf	BUFFER ZONE		A STRIP OF UNDISTURBED ORIGINAL VEGETATION, ENHANCED OR RESTORED EXISTING VEGETATION, OR THE RE-ESTABLISHMENT OF VEGETATION SURROUNDING AN AREA OF DISTURBANCE OR BORDERING STREAMS, PONDS, WETLANDS, LAKES, AND COASTAL WATERS.  WHEN NECESSARY, BUFFER ZONES ARE TO BE PROTECTED BY ORANGE BARRIER FENCE.
		SYMBOL 	
Ds1	MULCH SECTION 163		THIS IS AN APPLICATION OF STRAW MULCH USED TO REDUCE SOIL EROSION AND STABILIZE THE SOIL. IT IS USED TO CONTROL EROSION IN AREAS WHERE PERMANENT VEGETATION IS OUT OF SEASON OR TO TEMPORARILY STABILIZE AREAS PRIOR TO FINAL GRADING.  MULCHING REQUIREMENTS ARE ADDRESSED BY STANDARD SPECIFICATIONS AND/OR THE PROJECT ENGINEER.
		SYMBOL 	THE BMP SYMBOL FOR APPLICABLE AREAS AND/OR A NOTE SHALL BE INCLUDED ON APPLICABLE SHEETS IN SECTION 54.
Ds2	TEMPORARY GRASSING SECTION 163,700		THE SOWING OF A QUICK GROWING SPECIES OF GRASS SUITABLE TO THE AREA AND SEASON. IT IS TYPICALLY USED TO CONTROL EROSION IN AREAS LONGER THAN MULCHING IS EXPECTED TO LAST.  TEMPORARY GRASSING SHOULD BE USED ON ALL PROJECTS ACCORDING TO THE STANDARD SPECIFICATIONS.
		SYMBOL 	THE BMP SYMBOL FOR APPLICABLE AREAS AND/OR A NOTE SHALL BE INCLUDED ON APPLICABLE SHEETS IN SECTION 54.

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Ds3	PERMANENT GRASSING SECTION 700		THE SOWING OF PERMANENT VEGETATION, SUCH AS GRASS, SUITABLE TO THE AREA AND SEASON.  PERMANENT VEGETATION SHALL BE USED ON ALL PROJECTS ACCORDING TO THE STANDARD SPECIFICATION.  THE BMP SYMBOL FOR APPLICABLE AREAS AND/OR A NOTE SHALL BE INCLUDED ON APPLICABLE SHEETS IN SECTION 54.
		SYMBOL 	
Ds4	SODDING CONSTRUCTION DETAIL D-54 SECTION 700, 890		THE INSTALLATION OF A SPECIES OF GRASS SODDING SUITABLE TO THE AREA AND SEASON TO PROVIDE IMMEDIATE PERMANENT VEGETATION.  SODDING MAY BE SHOWN FOR HIGHLY SENSITIVE AREAS, TO IMPROVE AESTHETICS, OR FOR SPECIAL PLANTING REQUIREMENTS ON THE BASIS OF ENVIRONMENTAL COMMITMENTS OR LANDSCAPING REQUIREMENTS.
		PATTERN 	THE BMP PATTERN FOR APPLICABLE AREAS AND/OR A NOTE SHALL BE INCLUDED ON APPLICABLE SHEETS IN SECTION 54.
Fi-Co	FLOCCULANTS COAGULANTS SECTION 163,700, 895		FLOCCULANTS AND COAGULANTS ARE USED TO SETTLE SUSPENDED SEDIMENT, HEAVY METALS, AND HYDROCARBONS (TSS) IN SLOW MOVING RUNOFF FROM CONSTRUCTION SITES FOR WATER CLARIFICATION.  ANIONIC POLYACRYLAMIDES (PAM) MAY BE USED IN CONJUNCTION WITH BMPs WITHIN CHANNELS UPSTREAM OF A POST-CONSTRUCTION POND, TEMPORARY SEDIMENT BASIN, OR TEMPORARY SEDIMENT TRAP. FLOCCULANTS SHALL NOT BE USED DOWNSTREAM OF AFOREMENTIONED BMPs!
		SYMBOL 	FLOCCULANTS/COAGULANTS ARE TO BE SHOWN ON PLANS WITH APPLICABLE BMP IF NEEDED. PAYMENT FOR PAM AS A FLOCCULANT WILL BE INCLUDED IN THE PRICE FOR THE INSTALLATION AND/OR MAINTENANCE OF THE BMP IT IS USED IN CONJUNCTION WITH. NO SEPARATE PAYMENT WILL BE MADE.
Sb	STREAMBANK STABILIZATION SECTION 702		STREAMBANK STABILIZATION IS THE USE OF READILY AVAILABLE NATIVE PLANT MATERIALS TO MAINTAIN AND ENHANCE STREAMBANKS, OR TO PREVENT, OR RESTORE AND REPAIR SMALL STREAMBANK EROSION PROBLEMS.  STREAMBANK STABILIZATION AREAS SHOULD BE SHOWN ON THE PLANS WHEN APPLICABLE TO THE PROJECT. REFER TO THE PROJECT'S STREAM AND STREAM BUFFER MITIGATION PLANS FOR PLANT SPECIES, LOCATIONS, AND OTHER PLANTING DETAILS.
		PATTERN 	

**NOTE:**

- DO NOT USE EROSION CONTROL ITEMS IN A FLOWING STREAM OR IN A TIDAL AREA BELOW HIGH TIDE.
- FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA".
- ALL TEMPORARY & PERMANENT VEGETATIVE PRACTICES INCLUDING PLANT SPECIES, PLANTING DATES, SEEDING, FERTILIZING, LIMING, AND MULCHING FOR THIS PROJECT CAN BE FOUND IN SECTION 700 OF THE CURRENT EDITION OF GDOT'S STANDARDS SPECIFICATIONS (OR SPECIAL PROVISIONS).



NO SCALE

REVISION DATES	
3/2/2017	

**EROSION CONTROL LEGEND**  
UNIFORM CODE SHEET  
SHEET 1 OF 7

CHECKED:	D. EAGLETON	DATE:	01/01/16	DRAWING No.
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VERIFIED:		DATE:		

52-0001

**REVISION DATES**

REVISION DATES	

**EROSION CONTROL LEGEND**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

CHECKED:		DATE:		DRAWING No.
BACKCHECKED:		DATE:		
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VERIFIED:		DATE:		

52-0001

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Ss	SLOPE STABILIZATION CONSTRUCTION DETAIL D-35 SECTION 716		SLOPE STABILIZATION (EROSION CONTROL MATTING) IS A PROTECTIVE COVERING USED TO PREVENT EROSION AND ESTABLISH TEMPORARY OR PERMANENT VEGETATION ON STEEP SLOPES, SHORE LINES, OR CHANNELS.  SLOPE STABILIZATION MAY BE A ROLLED EROSION CONTROL PRODUCT (RECP) OR A HYDRAULIC EROSION CONTROL PRODUCT (HECP).  SLOPE STABILIZATION SHALL BE USED ON ALL CUT OR FILL SLOPES OF 2.5:1 OR STEEPER AND WITHIN 50 FEET OF ALL CROSS DRAINS AND CULVERTS.  NOTE: ONLY COCONUT FIBER BLANKET OR WOOD FIBER BLANKET SHALL BE USED AS SLOPE STABILIZATION WITHIN BUFFERED AREAS.
		PATTERN 	
TAc	TACKIFIERS SECTION 163, 700, 895		TACKIFIERS HYDRATE IN WATER AND READILY BLEND WITH OTHER SLURRY MATERIALS AND ARE USED TO TIE-DOWN FOR SOIL, COMPOST, SEED, STRAW, HAY OR MULCH.  TACKIFIERS REQUIREMENTS, SUCH AS ANIONIC POLYACRYLAMIDES (PAM) ARE ADDRESSED BY STANDARD SPECIFICATIONS AND ARE NOT TYPICALLY SHOWN ON THE PLANS. PAM IS TYPICALLY USED BY THE CONTRACTOR FOR TEMPORARY OR PERMANENT GRASSING.  REFER TO THE LATEST EDITION OF THE "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA" FOR CRITERIA.
		SYMBOL  POLYACRYLAMIDE	
Cd-F	FABRIC CHECK DAM CONSTRUCTION DETAIL D-24D SECTION 171		A CHECK DAM COMPOSED OF SYNTHETIC FIBER FABRIC, WIRE REINFORCED, POST, OVERFLOW WEIR, AND TURF REINFORCEMENT MATTING (TRM) SPLASHPAD PLACED IN DITCHES IN A SPECIAL CONFIGURATION WHICH CONTROLS ENERGY DISSIPATION AND FILTRATION OF STORM WATER. SEE CONSTRUCTION DETAIL D-24D FOR ADDITIONAL INFORMATION AND SPACING REQUIREMENTS.  THIS ITEM IS SUITABLE FOR USE IN ROADSIDE DITCHES THAT ARE PART OF INFRASTRUCTURE CONSTRUCTION PROJECTS AND WITHIN THE CLEAR ZONE.  IF THIS ITEM IS USED IN AN AREA WITH FLOWS GREATER THAN 2.0-CFS OR WITHOUT A SEDIMENT BASIN, A MINIMUM OF ONE ROCK FILTER DAM SHALL BE USED AT THE DOWNSTREAM DISCHARGE POINT.
		SYMBOL 	
Cd-Fs	COMPOST FILTER SOCK CHECK DAM CONSTRUCTION DETAIL D-52 SECTION 163		A COMPOST FILTER SOCK CHECK DAM IS COMPOSED OF A PHOTODEGRADABLE OR BIODEGRADABLE KNITTED MESH MATERIAL CONTAINING A WEED FREE FILLER MATERIAL DERIVED FROM A WELL-DECOMPOSED SOURCE OF ORGANIC MATTER. THEY SHALL BE PROPERLY STAKED FOR DITCH APPLICATIONS.  REFER TO THE LATEST EDITION OF THE "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA" FOR MATERIAL SPECIFICATIONS.  IF THIS ITEM IS USED IN AN AREA WITH FLOWS GREATER THAN 2.0-CFS OR WITHOUT A SEDIMENT BASIN, A MINIMUM OF ONE ROCK FILTER DAM SHALL BE USED AT THE DOWNSTREAM DISCHARGE POINT.
		SYMBOL 	
Cd-Hb	BALED STRAW CHECK DAM CONSTRUCTION DETAIL D-52 SECTION 163		A BALE STRAW CHECK DAM IS COMPOSED OF BALES PREFERABLY BOUND WITH WIRE OR NYLON INSTEAD OF TWINE. BALES SHOULD BE PLACED IN ROWS WITH BALE ENDS TIGHTLY ABUTTING ADJACENT BALES. THE DOWNSTREAM ROW OF BALES SHALL BE PLACED IN A TRENCH TO ALLOW THE TOP OF THE BALE'S LONG, WIDE SIDE TO BE LEVEL WITH THE GROUND AS A NON-ERODIBLE SPLASHPAD. PROPER STAKING IS ALSO REQUIRED FOR DITCH APPLICATIONS.  IF THIS ITEM IS USED IN AN AREA WITH FLOWS GREATER THAN 2.0-CFS OR WITHOUT A SEDIMENT BASIN, A MINIMUM OF ONE ROCK FILTER DAM SHALL BE USED AT THE DOWNSTREAM DISCHARGE POINT.
		SYMBOL 	

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Cd-S	STONE CHECK DAM OR SANDBAG CHECK DAM CONSTRUCTION DETAIL D-56 SECTION 163, 603		STONE CHECK DAMS ARE CONSTRUCTED OF TYPE-3 RIP-RAP WITH GEOTEXTILE UNDERLINER. STONE CHECK DAMS ARE PREFERRED IN ROADWAY DITCHES OUTSIDE THE CLEAR ZONE. CONSIDERATION SHOULD BE GIVEN TO USING OTHER APPROPRIATE CHECK DAMS AND/OR BMPs WITHIN THE CLEAR ZONE.  SANDBAG CHECK DAMS ARE RECOMMENDED IN CONCRETE LINED CHANNELS FOR TEMPORARY VELOCITY CONTROL ONLY. ENSURE DISCHARGE POINT IS PROPERLY STABILIZED AND INCLUDE APPROPRIATE BMPs FOR SEDIMENT STORAGE UPSTREAM AND/OR DOWNSTREAM OF CONCRETE LINED CHANNELS.  IF THIS ITEM IS USED IN AN AREA WITH FLOWS GREATER THAN 2.0-CFS OR WITHOUT A SEDIMENT BASIN, A MINIMUM OF ONE ROCK FILTER DAM SHALL BE USED AT THE DOWNSTREAM DISCHARGE POINT.
		SYMBOL 	
Ch-1	VEGETATED CHANNEL STABILIZATION SECTION 700		A NEW OR EXISTING CHANNEL MAY BE LINED WITH PERMANENT VEGETATION ONLY FOR VELOCITIES UP TO 5.0 fps. THIS MEASURE SHALL BE DESIGNED IN ACCORDANCE WITH THE GDOT CHANNEL LINING DESIGN PROGRAM. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED.  TYPICALLY NOT SHOWN IN PLANS.
		LINE CODE 	
Ch-2R1	CHANNEL STABILIZATION RIP-RAP, TYPE 1 CONSTRUCTION DETAIL D-49 SECTION 603		THIS ITEM CONSISTS OF LINING A CHANNEL WITH TYPE 1 RIP-RAP 24" THICK (UNLESS SPECIFIED OTHERWISE) PLACED ON TOP OF A GEOTEXTILE UNDERLINER. THE RIP-RAP SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED.  "Dp" SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-2R3	CHANNEL STABILIZATION RIP-RAP, TYPE 3 CONSTRUCTION DETAIL D-49 SECTION 603		THIS ITEM CONSISTS OF LINING A CHANNEL WITH TYPE 3 RIP-RAP 24" THICK (UNLESS SPECIFIED OTHERWISE) PLACED ON TOP OF A GEOTEXTILE UNDERLINER. THE RIP-RAP SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED.  "Dp" SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	

NOTE:

- DO NOT USE EROSION CONTROL ITEMS IN A FLOWING STREAM OR IN A TIDAL AREA BELOW HIGH TIDE.
- FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA".



NO SCALE

REVISION DATES

3/2/2017	
11/28/2018	

EROSION CONTROL LEGEND

UNIFORM CODE SHEET

SHEET 2 OF 7

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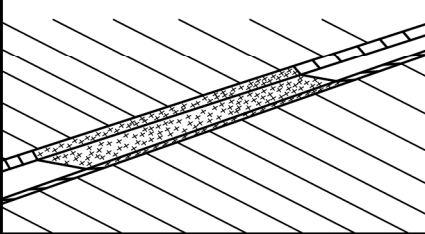
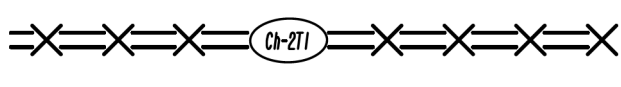
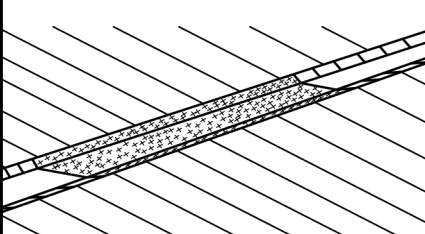

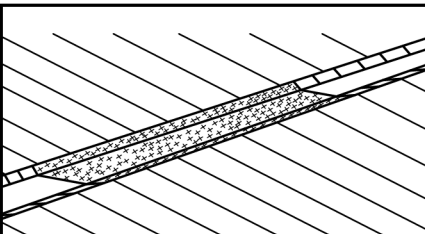

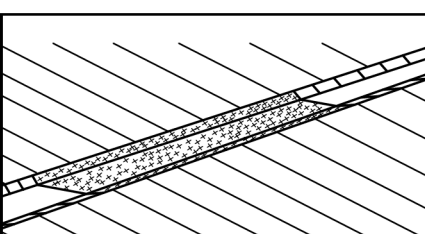
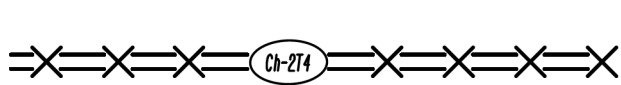
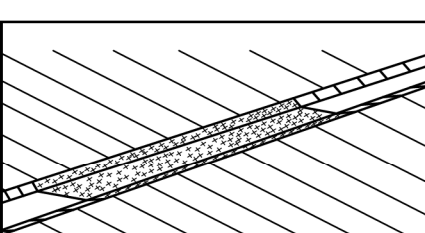
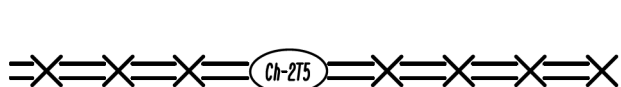
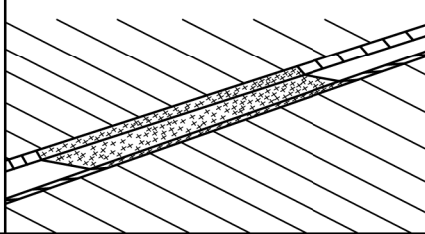
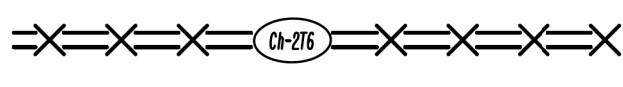
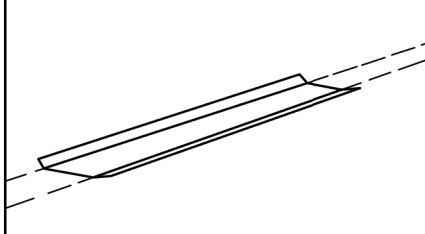
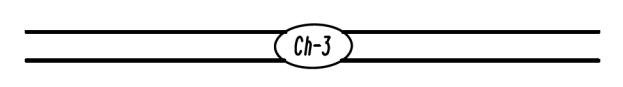
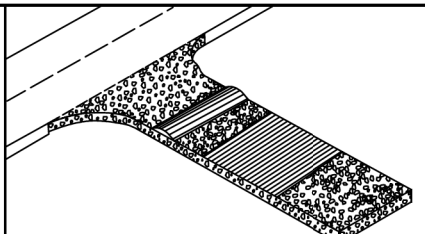

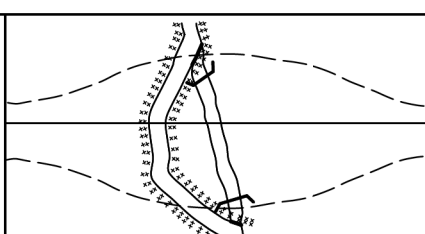
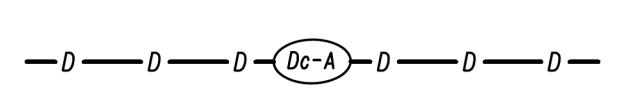
52-0002

REVISION DATES


EROSION CONTROL LEGEND  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN


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52-0002

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Ch-271	TURF REINFORCEMENT MAT (TRM) CONSTRUCTION DETAIL D-35 SECTION 711		THIS THREE DIMENSIONAL EROSION CONTROL MAT IS USED IN CONJUNCTION WITH PERMANENT VEGETATION IN CHANNELS TO STABILIZE THE SOIL BY REINFORCING THE GRASS ROOTS TO PROVIDE LONG-TERM PROTECTION FOR SHEAR STRESSES 0-2 psf. THE TRM SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-272	TURF REINFORCEMENT MAT (TRM) CONSTRUCTION DETAIL D-35 SECTION 711		THIS THREE DIMENSIONAL EROSION CONTROL MAT IS USED IN CONJUNCTION WITH PERMANENT VEGETATION IN CHANNELS TO STABILIZE THE SOIL BY REINFORCING THE GRASS ROOTS TO PROVIDE LONG-TERM PROTECTION FOR SHEAR STRESSES 0-4 psf. THE TRM SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-273	TURF REINFORCEMENT MAT (TRM) CONSTRUCTION DETAIL D-35 SECTION 711		THIS THREE DIMENSIONAL EROSION CONTROL MAT IS USED IN CONJUNCTION WITH PERMANENT VEGETATION IN CHANNELS TO STABILIZE THE SOIL BY REINFORCING THE GRASS ROOTS TO PROVIDE LONG-TERM PROTECTION FOR SHEAR STRESSES 0-6 psf. THE TRM SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-274	TURF REINFORCEMENT MAT (TRM) CONSTRUCTION DETAIL D-35 SECTION 711		THIS THREE DIMENSIONAL EROSION CONTROL MAT IS USED IN CONJUNCTION WITH PERMANENT VEGETATION IN CHANNELS TO STABILIZE THE SOIL BY REINFORCING THE GRASS ROOTS TO PROVIDE LONG-TERM PROTECTION FOR SHEAR STRESSES 0-8 psf. THE TRM SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-275	TURF REINFORCEMENT MAT (TRM) CONSTRUCTION DETAIL D-35 SECTION 711		THIS THREE DIMENSIONAL EROSION CONTROL MAT IS USED IN CONJUNCTION WITH PERMANENT VEGETATION IN CHANNELS TO STABILIZE THE SOIL BY REINFORCING THE GRASS ROOTS TO PROVIDE LONG-TERM PROTECTION FOR SHEAR STRESSES 0-10 psf. THE TRM SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-276	TURF REINFORCEMENT MAT (TRM) CONSTRUCTION DETAIL D-35 SECTION 711		THIS THREE DIMENSIONAL EROSION CONTROL MAT IS USED IN CONJUNCTION WITH PERMANENT VEGETATION IN CHANNELS TO STABILIZE THE SOIL BY REINFORCING THE GRASS ROOTS TO PROVIDE LONG-TERM PROTECTION FOR SHEAR STRESSES 0-12 psf. THE TRM SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.
		LINE CODE 	
Ch-3	CONCRETE CHANNEL STABILIZATION		CHANNELS ARE LINED WITH CONCRETE FOR VELOCITIES >= 10 fps. THIS ITEM CONSISTS OF CONSTRUCTING A 4" THICK CONCRETE CHANNEL. THE CONCRETE SHALL PROTECT THE CHANNEL FLOWING TO A DEPTH "Dp" RECOMMENDED BY THE GDOT CHANNEL LINING PROGRAM.  *Dp* SHALL BE IDENTIFIED IN A TABLE LOCATED ON THE SUMMARY OF QUANTITIES SHEETS AND IN THE EROSION, SEDIMENTATION, AND POLLUTION CONTROL PLAN.  RIP-RAP SHOULD BE USED TO DISSIPATE ENERGY DOWNSTREAM OF CONCRETE LINED CHANNELS.
	CONSTRUCTION DETAIL D-10, D-49 SECTION 441	LINE CODE 	
Co	CONSTRUCTION EXIT		A CONSTRUCTION EXIT IS A STONE STABILIZED PAD THAT REDUCES OR ELIMINATES THE TRANSPORT OF MUD FROM CONSTRUCTION AREAS ONTO PUBLIC ROADS BY EQUIPMENT OR RUNOFF. BEST USED AT ACCESS POINTS, I. e. NEW LOCATION PROJECTS, BORROW PITS, WASTE PITS, ACCESS ROADS, ETC. SHOULD BE MINIMUM 20' WIDE, 50' LONG, 6" THICK, AND REQUIRES A GEOTEXTILE UNDERLINER. ON SITES WHERE THE GRADE TOWARD A PAVED AREA IS GREATER THAN 2%, A FULL WIDTH DIVERSION RIDGE 6" TO 8" HIGH WITH 3:1 SLOPES SHALL BE CONSTRUCTED APPROXIMATELY 15' UPSTREAM OF PAVED AREA. A TIRE WASHING AREA TO REMOVE MUD MAY ALSO BE REQUIRED PRIOR TO ENTRANCE ONTO PUBLIC ROADWAYS.  ALL CONSTRUCTION EXIT REQUIREMENTS ARE INCLUDED IN THE PRICE OF THE CONSTRUCTION EXIT.
	CONSTRUCTION DETAIL D-41 SECTION 163,800	SYMBOL 	
Dc-A	STREAM DIVERSION CHANNEL GEOTEXTILE, POLYETHYLENE FILM		A TEMPORARY CHANNEL CONSTRUCTED TO CONVEY FLOW AROUND A CONSTRUCTION SITE WHILE A PERMANENT DRAINAGE STRUCTURE IS BEING CONSTRUCTED IN A NATURAL STREAM. THIS IS A MEASURE USED TO PROTECT STREAM BEDS FROM EROSION. LINE THE CHANNEL WITH GEOTEXTILE OR POLYETHYLENE FILM. INSTALL TWO ROWS OF sd1-S PARALLEL TO THE CHANNEL TO PREVENT SEDIMENT LADEN RUNOFF FROM ENTERING THE STREAM. THE SIZE OF THE CHANNEL WILL DEPEND ON THE DISCHARGE, CHANNEL GEOMETRY, CHANNEL SLOPE AND ROUGHNESS. IT IS ACCEPTABLE FOR VELOCITIES BETWEEN 0 - 2.5 fps.  THE DRAINAGE AREA SHALL BE NOT GREATER THAN 1 SQUARE MILE.  CONSTRUCTION OF THE DIVERSION CHANNEL IS INCLUDED IN THE COST OF THE STRUCTURE.
	SECTION 163	LINE CODE 	

**NOTE:**

- DO NOT USE EROSION CONTROL ITEMS IN A FLOWING STREAM OR IN A TIDAL AREA BELOW HIGH TIDE.
- FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA".
- STONE SIZE FOR CONSTRUCTION EXITS MUST BE BETWEEN 1.5" TO 3.5".

	NO SCALE	<table border="1" style="width:100%; border-collapse: collapse;"> <tr><th colspan="2">REVISION DATES</th></tr> <tr><td>3/2/2017</td><td></td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>	REVISION DATES		3/2/2017										<p><b>EROSION CONTROL LEGEND</b> UNIFORM CODE SHEET SHEET 3 OF 7</p> <table border="1" style="width:100%; border-collapse: collapse;"> <tr> <td>CHECKED: D. EAGLETON</td> <td>DATE: 01/01/16</td> <td>DRAWING No.</td> </tr> <tr> <td>BACKCHECKED:</td> <td>DATE:</td> <td rowspan="3" style="text-align: center; vertical-align: middle; font-size: 1.2em;">52-0003</td> </tr> <tr> <td>CORRECTED:</td> <td>DATE:</td> </tr> <tr> <td>VERIFIED:</td> <td>DATE:</td> </tr> </table>	CHECKED: D. EAGLETON	DATE: 01/01/16	DRAWING No.	BACKCHECKED:	DATE:	52-0003	CORRECTED:	DATE:	VERIFIED:	DATE:
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CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION	CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION	
Dc-B	STREAM DIVERSION CHANNEL GEOTEXTILE ONLY SECTION 163		A TEMPORARY CHANNEL CONSTRUCTED TO CONVEY FLOW AROUND A CONSTRUCTION SITE WHILE A PERMANENT DRAINAGE STRUCTURE IS BEING CONSTRUCTED IN A NATURAL STREAM. THIS IS A MEASURE USED TO PROTECT STREAM BEDS FROM EROSION. LINE THE CHANNEL WITH GEOTEXTILE ONLY. INSTALL TWO ROWS OF Sd1-S PARALLEL TO THE CHANNEL TO PREVENT SEDIMENT LADEN RUNOFF FROM ENTERING THE STREAM. THE SIZE OF THE CHANNEL WILL DEPEND ON THE DISCHARGE, CHANNEL GEOMETRY, CHANNEL SLOPE AND ROUGHNESS. IT IS ACCEPTABLE FOR VELOCITIES BETWEEN 2.5 - 9.0 fps.	Dn2-A	PERMANENT DOWNDRAIN STRUCTURE CONCRETE CONSTRUCTION DETAIL D-9 SECTION 441		A CONCRETE FLUME TYPE "A" IS USED TO DIRECT SURFACE RUNOFF DOWN A ROADWAY SLOPE INTO ANOTHER FORM OF CONTROL. IT IS USED IN ALL DEPRESSED AREAS WHERE WATER WILL FLOW DOWN THE SLOPE. IT IS DESIGNED FOR A 25-YEAR STORM AND MUST HAVE SOME FORM OF OUTLET PROTECTION. ADDITIONAL LABELING IS NOT REQUIRED IF SHOWN AS A PERMANENT DRAINAGE STRUCTURE ON THE CONSTRUCTION PLANS. INLETS SHALL BE SPACED ACCORDING TO GDOT GUIDELINES (REGARDING GUTTER SPREAD AND OTHER CRITERIA).	
	LINE CODE		THE DRAINAGE AREA SHALL BE NOT GREATER THAN 1 SQUARE MILE. CONSTRUCTION OF THE DIVERSION CHANNEL IS INCLUDED IN THE COST OF THE STRUCTURE.		LINE CODE			
Dc-C	STREAM DIVERSION CHANNEL RIP-RAP & GEOTEXTILE SECTION 163		A TEMPORARY CHANNEL CONSTRUCTED TO CONVEY FLOW AROUND A CONSTRUCTION SITE WHILE A PERMANENT DRAINAGE STRUCTURE IS BEING CONSTRUCTED IN A NATURAL STREAM. THIS IS A MEASURE USED TO PROTECT STREAM BEDS FROM EROSION. LINE THE CHANNEL WITH RIP-RAP AND GEOTEXTILE. INSTALL TWO ROWS OF Sd1-S PARALLEL TO THE CHANNEL TO PREVENT SEDIMENT LADEN RUNOFF FROM ENTERING THE STREAM. THE SIZE OF THE CHANNEL WILL DEPEND ON THE DISCHARGE, CHANNEL GEOMETRY, CHANNEL SLOPE AND ROUGHNESS. IT IS ACCEPTABLE FOR VELOCITIES BETWEEN 9.0 - 13.0 fps.	Dn2-B	PERMANENT DOWNDRAIN STRUCTURE CONCRETE CONSTRUCTION DETAIL D-9 SECTION 441		A CONCRETE FLUME TYPE "B" IS USED TO DIRECT SURFACE DITCH RUNOFF DOWN A BACK SLOPE INTO ANOTHER FORM OF CONTROL. IT IS USED IN DEPRESSED AREAS WHERE CONCENTRATED OFFSITE WATER REACHES THE CUT SLOPE. IT IS DESIGNED TO SAFELY CONVEY WATER DOWN THE CUT SLOPE. IT IS DESIGNED FOR A 25-YEAR STORM AND MUST HAVE SOME FORM OF OUTLET PROTECTION. ADDITIONAL LABELING IS NOT REQUIRED IF SHOWN AS A PERMANENT DRAINAGE STRUCTURE ON THE CONSTRUCTION PLANS. INLETS SHALL BE SPACED ACCORDING TO GDOT GUIDELINES (REGARDING GUTTER SPREAD AND OR OTHER CRITERIA).	
	LINE CODE		THE DRAINAGE AREA SHALL BE NOT GREATER THAN 1 SQUARE MILE. CONSTRUCTION OF THE DIVERSION CHANNEL IS INCLUDED IN THE COST OF THE STRUCTURE.		LINE CODE			
D1-1	DIVERSION BERM CONSTRUCTION DETAIL D-47 SECTION 205		A NON-DESIGNED TEMPORARY EARTHEN BERM WITH A COMPACTED SUPPORTING RIDGE ON THE LOWER SIDE TO BE USED AT THE EDGE OF EMBANKMENT DURING THE GRADING OPERATION. THE BERMS ARE ALSO CONSTRUCTED ABOVE, ACROSS OR BELOW A SLOPE TO REDUCE THE LENGTH OF A SLOPE. THEY ARE USED TO INTERCEPT RUNOFF, PREVENTING SLOPE EROSION AND TO DIRECT THE RUNOFF TO A STABLE OUTLET, DOWN DRAINS "Dn1" OR CATCHMENT AREAS AND ON ALL GRADING PROJECTS.	Dn2-1	PERMANENT DOWNDRAIN STRUCTURE GA. STD 9013 TP1, 9017 J TP1, DETAIL D-26 TP1 SECTION 576, 577		CONCRETE DRAIN INLET WITH METAL PIPE IS USED TO DRAIN CURBS, ON A GRADE, DOWN TO A LOWER ELEVATION. THIS IS A PERMANENT STRUCTURE, REQUIRING OUTLET PROTECTION, TEMPORARY AND PERMANENT. INLETS SHALL BE SPACED ACCORDING TO GDOT GUIDELINES (REGARDING GUTTER SPREAD AND OR OTHER CRITERIA).	
	LINE CODE				LINE CODE			
D1-2	DIVERSION CHANNEL SECTION 205		A DESIGNED TEMPORARY OR PERMANENT CHANNEL WITH A COMPACTED SUPPORTING RIDGE ON THE LOWER SIDE TO DIVERT OFFSITE RUNOFF AWAY FROM DISTURBED AREAS WITHIN THE PROJECT AREA. CHANNEL FOR OFFSITE RUNOFF SHALL BE STABILIZED WITH APPROPRIATE CHANNEL STABILIZATION.	Dn2-2	PERMANENT DOWNDRAIN STRUCTURE GA. STD 9013 TP2, 9017 J TP2, DETAIL D-26 TP2 SECTION 576, 577		CONCRETE DRAIN INLET AND METAL PIPE IS USED TO DRAIN CURB, IN A SAG, DOWN TO A LOWER ELEVATION. THIS IS A PERMANENT STRUCTURE, REQUIRING OUTLET PROTECTION, TEMPORARY AND PERMANENT. INLETS SHALL BE SPACED ACCORDING TO GDOT GUIDELINES (REGARDING GUTTER SPREAD AND OR OTHER CRITERIA).	
	LINE CODE		REFER TO THE LATEST EDITION OF THE "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA" FOR DESIGN CRITERIA. A DIVERSION CHANNEL DETAIL MUST ALSO BE PROVIDED IN THE ESPCP. RUNOFF FROM DISTURBED AREAS WITHIN THE PROJECT AREA SHALL NOT BE ALLOWED TO CONVERGE WITH OFFSITE RUNOFF WITHIN THIS DIVERSION.		LINE CODE			
Dn1	TEMPORARY DOWNDRAIN STRUCTURE FLEXIBLE CONSTRUCTION DETAIL D-19 SECTION 163		A TEMPORARY PIPE SLOPE DRAIN IS A PLASTIC FLEXIBLE PIPE TO CARRY WATER FROM THE WORK AREA TO A LOWER ELEVATION. TEMPORARY SLOPE DRAINS SHOULD BE PLACED AT INTERVALS OF 350 FEET ON 0% - 2% GRADES, 200 FEET ON STEEPER GRADES AND MORE FREQUENTLY AS DICTATED BY FIELD CONDITIONS. THE TYPICAL PIPE SIZE IS A CORRUGATED 10". THE PIPE WILL BE ANCHORED WITH STAKES AT INTERVALS NOT TO EXCEED 10'.					
	LINE CODE		THE OUTLET AREA SHALL BE STABILIZED FOR VELOCITY DISSIPATION AND EROSION CONTROL.	LINE CODE				

**NOTE:**

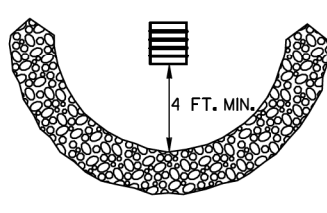

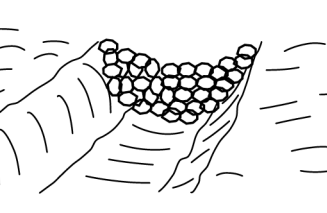

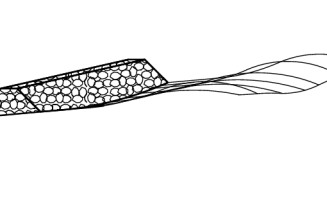

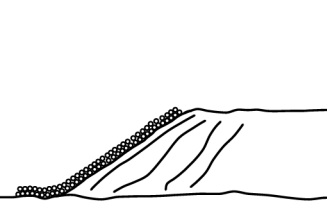

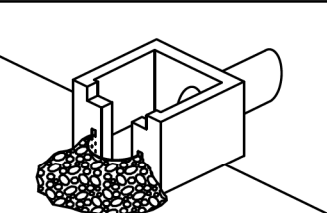
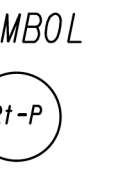
- DO NOT USE EROSION CONTROL ITEMS IN A FLOWING STREAM OR IN A TIDAL AREA BELOW HIGH TIDE.
- FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA".

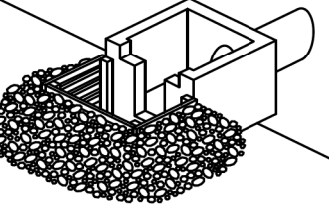
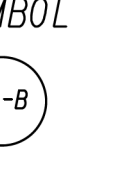
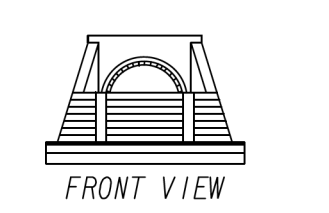
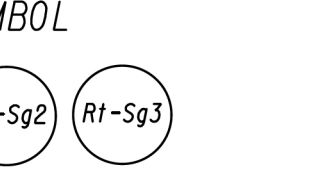
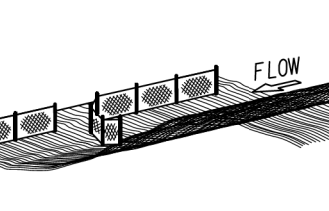
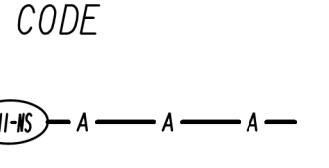
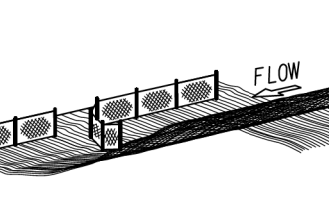
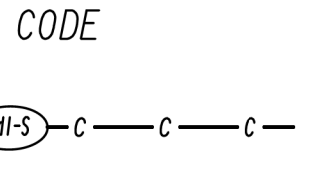
  

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REVISION DATES		EROSION CONTROL LEGEND	
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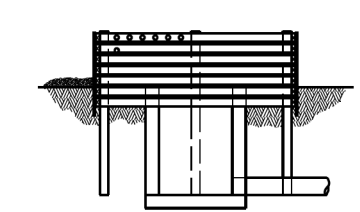

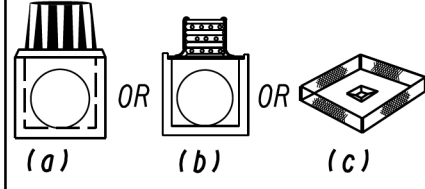
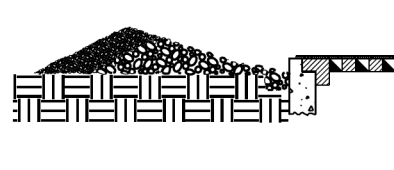
CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Fr	FILTER RING  CONSTRUCTION DETAIL D-46 SECTION 163		A TEMPORARY STONE BARRIER CONSTRUCTED AT DRAINAGE STRUCTURE INLETS AND POST-CONSTRUCTION POND OUTLETS. IT REDUCES RUNOFF VELOCITY AND HELPS PREVENT SEDIMENT FROM LEAVING SITE PRIOR TO PERMANENT STABILIZATION OF THE DISTURBED AREA.  REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR ADDITIONAL INFORMATION ON USAGE.
		SYMBOL 	
Rd	ROCK FILTER DAM  CONSTRUCTION DETAIL D-43 SECTION 163, 603		ROCK FILTER DAMS ARE CONSTRUCTED OF TYPE 3 STONE RIP-RAP FACED WITH *57 STONE ON THE UPSTREAM SIDE. THEY ARE PLACED ACROSS DRAINAGEWAYS WHICH DRAIN 50 ACRES OR LESS. GEOTEXTILE UNDERLINER SHALL BE USED WHEN PLACING ROCK FILTER DAMS.  THE DAM SHOULD NOT BE HIGHER THAN THE CHANNEL BANKS.  ROCK FILTER DAMS SHOULD BE USED IN DITCHES PRIOR TO DISCHARGING INTO STREAMS, WETLANDS, OPEN-WATERS, OR OTHER ESAs.
		SYMBOL 	
Rd-B	STONE FILTER BERM  CONSTRUCTION DETAIL D-50 SECTION 163, 603		STONE FILTER BERMS ARE CONSTRUCTED SIMILAR TO ROCK FILTER DAMS FOR A LINEAR APPLICATION. THEY ARE CONSTRUCTED OF TYPE-3 STONE RIP-RAP FACED WITH *57 STONE ON THE UPSTREAM SIDE. GEOTEXTILE UNDERLINER SHALL BE USED WHEN PLACING STONE FILTER BERMS.  STONE FILTER BERMS ARE IDEAL ALONG THE PERIMETER FOR SHEET FLOW AND/OR SHALLOW CONCENTRATED FLOW TO A COMMON LOW AREA WHERE PERIMETER SILT FENCE ALONE MAY BE INSUFFICIENT. THERE IS NO WELL-DEFINED CHANNEL FOR A STANDARD ROCK FILTER DAM. AND/OR CONSTRUCTING A ROCK OUTLET TEMPORARY SEDIMENT TRAP IS NOT APPLICABLE.
		LINE CODE 	
Rp	RIP-RAP  SECTION 603		RIP-RAP IS A FLEXIBLE PERMANENT BLANKET FOR PROTECTION OF FILL SLOPES AND BRIDGE END ROLLS. RIP-RAP TYPE-1 SHOULD BE PLACED ON TOP OF A GEOTEXTILE UNDERLINER AT A MINIMUM 24" THICKNESS OR AS INDICATED ON THE PLANS.  RIP-RAP MAY ALSO BE USED AT DRAINAGE STRUCTURE OUTLETS WITHIN THE RIGHT-OF-WAY. HOWEVER, APPROPRIATE OUTLET PROTECTION SHOULD BE PROVIDED AT OUTFALLS. REFER TO STORM DRAIN OUTLET PROTECTION FOR ADDITIONAL INFORMATION ON USING RIP-RAP AT OUTFALLS.
		PATTERN 	
Ri-P	RETROFITTING PERFORATED HALF-ROUND PIPE  CONSTRUCTION DETAIL D-44 SECTION 163		A PERFORATED HALF-ROUND PIPE WITH STONE FILTER PLACED IN FRONT OF A PERMANENT STORMWATER DETENTION POND OUTLET STRUCTURE TO SERVE AS A TEMPORARY SEDIMENT FILTER.  SHOULD BE USED ONLY IN DETENTION PONDS WITH LESS THAN 30 ACRES TOTAL DRAINAGE AREA.  SHALL ONLY BE USED IN DETENTION BASINS LARGE ENOUGH TO STORE 67 CUBIC YARDS OF SEDIMENT PER ACRE OF DISTURBED AREA.  REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR DESIGN CRITERIA.
		SYMBOL 	

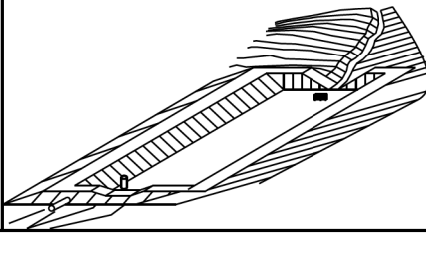
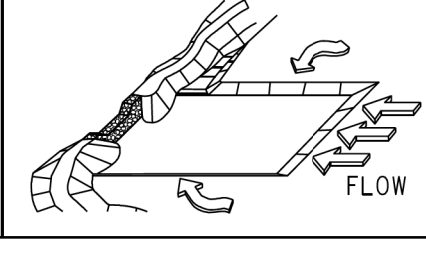
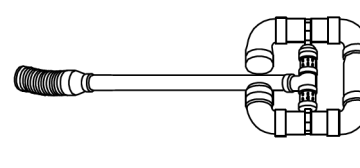
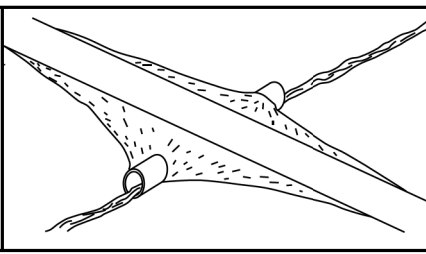
CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Rt-B	RETROFITTING SLOTTED BOARD DAM  CONSTRUCTION DETAIL D-45 SECTION 163		A SLOTTED BOARD DAM CONSISTS OF STONE AND/OR FILTER FABRIC AND BOARDS WITH 0.5" - 1.0" SPACING TO SERVE AS A TEMPORARY SEDIMENT FILTER.  PERMANENT STORMWATER DETENTION POND OUTLET: -DRAINAGE AREA UP TO 100 ACRES -DETENTION BASINS LARGE ENOUGH TO STORE 67 CUBIC YARDS OF SEDIMENT PER ACRE OF DISTURBED AREA  ROADWAY DRAINAGE STRUCTURE: -OPEN END PIPES, WINGED HEADWALLS, OR CONCRETE WEIR OUTLETS WITH DRAINAGE AREA LESS THAN 30 ACRES  REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR DESIGN CRITERIA.
		SYMBOL 	
Rt-Sg1 Rt-Sg2 Rt-Sg3	RETROFITTING SILT CONTROL GATES  CONSTRUCTION DETAIL D-20 SECTION 163		A SILT CONTROL GATE CONSISTS OF BOARDS WITHOUT SPACING AND FILTER FABRIC TO BE USED FOR TEMPORARY SEDIMENT STORAGE ON ROADWAY PROJECTS AT THE INLET OF STRUCTURES WITH A DRAINAGE AREA UP TO 50 ACRES. THE DISTURBED AREA WITHIN THE DRAINAGE AREA SHALL NOT EXCEED 5 ACRES. SILT CONTROL GATES SHOULD NOT BE USED ALONE, BUT WITH ANOTHER BMP DOWNSTREAM PRIOR TO DISCHARGE LEAVING PROJECT AREA.  DO NOT USE SILT GATES IN STATE WATERS.  Rt-Sg1+TYPE 1: USED ON BOX CULVERTS Rt-Sg2+TYPE 2: USED ON STRAIGHT HEADWALLS Rt-Sg3+TYPE 3: USED ON FLARED END SECTIONS AND TAPERED HEADWALLS
		SYMBOL 	
SdI-NS	SEDIMENT BARRIER (NON-SENSITIVE) SILT FENCE TYPE A  CONSTRUCTION DETAIL D-24 SECTION 171		SEDIMENT BARRIERS MINIMIZE AND PREVENT SEDIMENT CARRIED BY SHEET FLOW FROM LEAVING THE PROJECT AREA BY CAUSING DEPOSITION AND/OR FILTRATION OF SEDIMENT. SILT FENCE USED AS PERIMETER CONTROL SHALL NOT BE INSTALLED ACROSS CONCENTRATED FLOW.  TYPE-A SILT FENCE IS TYPICALLY USED IN NON-ENVIRONMENTALLY SENSITIVE AREAS (ESAs) OR IN AREAS WITH FILLS LESS THAN 10'.  IT SHOULD BE PLACED A MINIMUM OF 10' FROM CONSTRUCTION LIMITS OR ALONG THE RIGHT-OF-WAY LINE.
		LINE CODE 	
SdI-S	SEDIMENT BARRIER (SENSITIVE) SILT FENCE TYPE C  CONSTRUCTION DETAIL D-24 SECTION 171		SEDIMENT BARRIERS MINIMIZE AND PREVENT SEDIMENT CARRIED BY SHEET FLOW FROM LEAVING THE PROJECT AREA BY CAUSING DEPOSITION AND/OR FILTRATION OF SEDIMENT. SILT FENCE USED AS PERIMETER CONTROL SHALL NOT BE INSTALLED ACROSS CONCENTRATED FLOW.  TYPE-C SILT FENCE IS TYPICALLY USED IN ENVIRONMENTALLY SENSITIVE AREAS (ESAs) OR IN AREAS WITH FILLS 10' AND GREATER.  ALL ENVIRONMENTALLY SENSITIVE AREAS (ESAs) SHALL BE PROTECTED WITH A DOUBLE-ROW OF TYPE-C SILT FENCE REGARDLESS OF FILL HEIGHT. A SINGLE-ROW MAY BE USED FOR OTHER APPLICATIONS.  IT SHOULD BE PLACED A MINIMUM OF 10' FROM CONSTRUCTION LIMITS OR ALONG THE RIGHT-OF-WAY LINE.
		LINE CODE 	

**NOTE:**

- DO NOT USE EROSION CONTROL ITEMS IN A FLOWING STREAM OR IN A TIDAL AREA BELOW HIGH TIDE.
- FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA'.

	NO SCALE	REVISION DATES		EROSION CONTROL LEGEND	
		3/2/2017		UNIFORM CODE SHEET	
				SHEET 5 OF 7	
				CHECKED: D. EAGLETON	DATE: 01/01/16
				BACKCHECKED:	DATE:
				CORRECTED:	DATE:
				VERIFIED:	DATE:
				DRAWING No. <b>52-0005</b>	

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Sd1-BB	SEDIMENT BARRIER BRUSH BARRIER  CONSTRUCTION DETAIL D-24B SECTION 201		THIS ITEM CONSISTS OF INTERMINGLED BRUSH, LOGS, ETC. SO AS NOT TO FORM A SOLID DAM. CONSTRUCTED AT THE TOE OF FILL SLOPES ONLY DURING THE CLEARING AND GRUBBING OPERATION. THE BARRIER SHOULD BE USED AT THE TOE OF FILL SLOPES ON GRADING PROJECTS IN RURAL AREAS WHERE SUFFICIENT RIGHT-OF-WAY OR EASEMENT IS AVAILABLE (10 FEET OR MORE). THE BARRIER SHOULD RUN ROUGHLY PERPENDICULAR TO THE FLOW OF WATER WHERE THIS DOES NOT CONFLICT WITH RIGHT-OF-WAY OR EASEMENT LIMITS. THEY WILL NOT BE PLACED IN WETLANDS.
	LINE CODE  * * * (Sd1-BB) * * *		TYPICALLY NOT SHOWN ON PLANS.  PAYMENT FOR THIS ITEM IS INCLUDED IN THE CLEARING AND GRUBBING COST. NO SEPARATE PAYMENT SHALL BE MADE.
Sd2-B	INLET SEDIMENT TRAP (BAFFLE BOX) CONSTRUCTION DETAIL D-42 SECTION 163		BAFFLE BOX INLET SEDIMENT TRAP USED FOR INLETS RECEIVING HIGH FLOW RATE AND/OR VELOCITY. A GUIDE FOR USE WILL BE FOR AN INLET RECEIVING FLOW RATES 7 cfs AND GREATER.
	SYMBOL  (Sd2-B)		
Sd2-Bg	INLET SEDIMENT TRAP (BLOCK & GRAVEL) CONSTRUCTION DETAIL D-42 SECTION 163		BLOCK AND GRAVEL DROP INLET PROTECTION USED FOR WHERE HEAVY FLOWS ARE EXPECTED AND WHERE OVERFLOW CAPACITY IS NECESSARY TO PREVENT EXCESSIVE PONDING AROUND THE STRUCTURE. CAN BE USED AT CULVERT INLETS. A GUIDE FOR USE WILL BE FOR AN INLET RECEIVING FLOW RATES THAT RANGE FROM 5 - 7 cfs.
	SYMBOL  (Sd2-Bg)		
Sd2-F	INLET SEDIMENT TRAP (FILTER FABRIC) CONSTRUCTION DETAIL D-24C SECTION 163		(a) A SEDIMENT BARRIER CONSISTING OF A PREFABRICATED FRAME WITH FILTER FABRIC USED AROUND A DROP INLET OR CATCH BASIN. (b) A SEDIMENT BARRIER CONSISTING OF A PERFORATED METAL STAND PIPE WITH FILTER FABRIC USED AROUND A DROP INLET OR CATCH BASIN. (c) TYPE C SILT FENCE WITH SUPPORTING FRAME CAN BE USED AS AN ALTERNATE TO INLET SEDIMENT TRAP FOR AREAS WITH SLOPES < 5%.
	SYMBOL  (Sd2-F)		THIS ITEM IS USED TO PREVENT SILT FROM ENTERING THE PIPE SYSTEM. SHALL NOT APPLY TO INLETS RECEIVING CONCENTRATED FLOWS. RECOMMENDED FOR INLET RECEIVING FLOW RATES THAT RANGE FROM 0 - 4 cfs.
Sd2-G	INLET SEDIMENT TRAP (GRAVEL) CONSTRUCTION DETAIL D42 SECTION 163		GRAVEL DROP INLET PROTECTION USED WHERE HEAVY CONCENTRATED FLOWS ARE EXPECTED. STONE AND GRAVEL ARE USED TO TRAP SEDIMENT. THE SLOPE TOWARD THE INLET SHALL BE NO MORE THAN 3:1. A GUIDE FOR USE WILL BE FOR AN INLET RECEIVING FLOW RATES THAT RANGE FROM 3 - 5 cfs.
	SYMBOL  (Sd2-G)		

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Sd3	TEMPORARY SEDIMENT BASIN  CONSTRUCTION DETAIL D-22A, D-22B SECTION 163		A BASIN CREATED BY EXCAVATING AN AREA, DAMMING CONCENTRATED FLOW, OR A COMBINATION OF BOTH. THE BASIN IS DESIGNED TO STORE 67 CUBIC YARDS OF SEDIMENT PER ACRE OF DRAINAGE AREA. THE DRAINAGE AREA SHOULD NOT EXCEED 150 ACRES. BASINS TYPICALLY CONSISTS OF A DAM, PRINCIPAL SPILLWAY, AND AN EMERGENCY SPILLWAY. A FLOATING SURFACE SKIMMER SHALL BE REQUIRED AS PART OF THE PRINCIPAL SPILLWAY UNLESS INFEASIBLE. SUFFICIENT RIGHT-OF-WAY OR EASEMENT IS NEEDED FOR BASIN CONSTRUCTION AND MAINTENANCE ACCESS.
	SYMBOL  (Sd3)		SEDIMENT BASINS SHALL BE CONSIDERED ON ALL PROJECTS, BUT MAY NOT BE PRACTICAL. BASINS SHOULD BE LOCATED TO MINIMIZE INTERFERENCE WITH CONSTRUCTION ACTIVITIES AND UTILITIES. REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR DESIGN CRITERIA.
Sd4-C	ROCK OUTLET TEMPORARY SEDIMENT TRAP  CONSTRUCTION DETAIL D-53 SECTION 163		TEMPORARY POND WITH ROCK OUTLET DESIGNED TO STORE 67 CUBIC YARDS OF SEDIMENT PER DRAINAGE AREA. DRAINAGE AREA SHALL NOT EXCEED 5 ACRES. DISTINGUISHED FROM TEMPORARY SEDIMENT BASIN BY LACK OF PRINCIPAL SPILLWAY. MAXIMUM POND DEPTH FROM BOTTOM OF POND TO EMERGENCY SPILLWAY IS 4 FEET.
	SYMBOL  (Sd4-C)		A TEMPORARY SEDIMENT BASIN SHALL BE EVALUATED PRIOR TO CONSIDERING A TEMPORARY SEDIMENT TRAP. A TEMPORARY SEDIMENT TRAP IS IDEAL FOR SMALL AREAS WITH NO UNUSUAL DRAINAGE FEATURES AND EFFECTIVE AGAINST COARSE SEDIMENT, BUT NOT AGAINST SILT OR CLAY PARTICLES THAT REMAIN SUSPENDED.  REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR DESIGN CRITERIA.
Sk	FLOATING SURFACE SKIMMER  CONSTRUCTION DETAIL D-22A, D-22B SECTION 163		A BUOYANT DEVICE THAT DRAINS WATER FROM THE SURFACE OF A TEMPORARY SEDIMENT BASIN AT A CONTROLLED FLOW RATE. THE INLET/ORIFICE SIZE IS DESIGNED TO DRAIN THE BASIN WITHIN 24 - 48 HOURS. THE SKIMMER INFORMATION SHALL BE PROVIDED IN CONJUNCTION WITH THE SEDIMENT BASIN INFORMATION IN PLANS. IF A SKIMMER IS INFEASIBLE, THE DESIGNER SHALL PROVIDE A WRITTEN JUSTIFICATION IN THE PLANS.
	SYMBOL  (Sk)		SKIMMERS ARE ATTACHED TO A RISER WITHOUT PERFORATIONS AND ACTS AS THE PRIMARY SPILLWAY. THE SKIMMER BMP SYMBOL SHALL BE SHOWN IN CONJUNCTION WITH THE TEMPORARY SEDIMENT BASIN BMP SYMBOL WHEN APPLICABLE.  REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR ADDITIONAL INFORMATION.
Sr	TEMPORARY STREAM CROSSING  SECTION 107		A TEMPORARY STRUCTURE INSTALLED ACROSS A FLOWING STREAM OR WATERCOURSE FOR USE BY CONSTRUCTION EQUIPMENT. THIS BMP PROVIDES A MEANS TO CROSS STREAMS OR WATERCOURSES WITHOUT MOVING SEDIMENT INTO STREAMS, DAMAGING THE STREAM BED OR CHANNEL, OR CAUSING FLOODING. THIS BMP SHOULD NOT BE USED ON STREAMS WITH DRAINAGE AREAS GREATER THAN ONE SQUARE MILE, UNLESS SPECIFICALLY DESIGNED TO ACCOMMODATE THE ADDITIONAL DRAINAGE AREA BY THE DESIGN PROFESSIONAL. A CERTIFICATION STATEMENT AND SIGNATURE SHALL ACCOMPANY THE DESIGN.
	SYMBOL  (Sr)		THIS BMP SHALL BE DESIGNED ACCORDING TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA'.  FOR CONTRACTOR'S USE ONLY!

NOTE:

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- FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA".



NO SCALE

REVISION DATES

3/2/2017		
11/28/2018		

EROSION CONTROL LEGEND

UNIFORM CODE SHEET  
SHEET 6 OF 7

CHECKED:	D. EAGLETON	DATE:	01/01/16	DRAWING No.
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52-0006

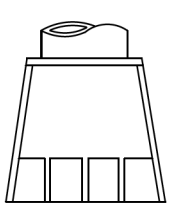

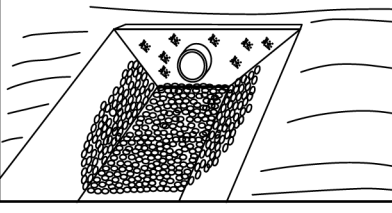
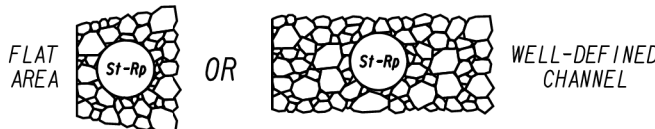
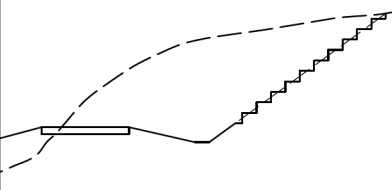
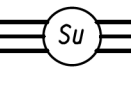
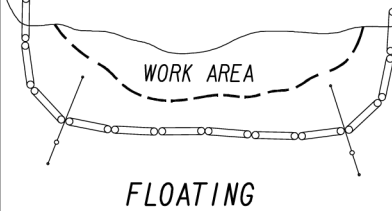
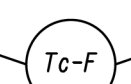
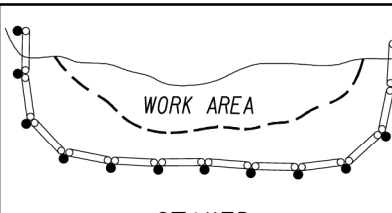
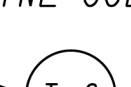
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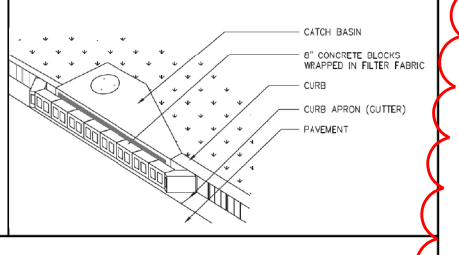
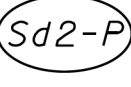

EROSION CONTROL LEGEND  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

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52-0006



CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
St	STORM DRAIN OUTLET PROTECTION  GA. STD. 1125 & 2332		A PIPE OR BOX CULVERT OUTLET HEADWALL WITH AN APRON AND DISSIPATOR BLOCKS IS USED TO REDUCE VELOCITY AT THE OUTLET OF A PIPE PRIOR TO ENTERING AN EXISTING STREAM OR PUBLICLY MAINTAINED DRAINAGE SYSTEM.  IT IS USED ON THE OUTLET OF ALL BOX CULVERTS AND ON 48" AND LARGER PIPES. MAY BE USED ON INLET FOR FLOWING STREAMS. USE ON SMALL PIPES WHEN OUTLET VELOCITY OF THE 25-YEAR STORM IS 12 FPS AND GREATER.
	SYMBOL 		
St-Rp	STORM DRAIN OUTLET PROTECTION (RIP-RAP)  CONSTRUCTION DETAIL D-55 SECTION 603		RIP-RAP OUTLET PROTECTION IS USED TO REDUCE VELOCITY AT THE OUTLET OF A PIPE, CHANNEL, OR STRUCTURE PRIOR TO ENTERING AN EXISTING STREAM OR PUBLICLY MAINTAINED DRAINAGE SYSTEM. THE MINIMUM DESIGN OF RIP-RAP OUTLET PROTECTION SHALL BE THE 25-YEAR STORM PEAK FLOW, BUT LARGER STORMS ARE RECOMMENDED.  TYPE-1 RIP-RAP AT A DEPTH OF 36" AND PLACED ON FILTER FABRIC IS PREFERRED FOR ALL d50 < 1.2 FEET. TYPE-3 RIP-RAP AT A DEPTH OF 18" AND PLACED ON FILTER FABRIC MAY BE USED FOR d50 < 0.7 FEET.
	PATTERN 		REFER TO THE LATEST EDITION OF THE 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA' FOR REQUIRED DESIGN DIMENSIONS AND OTHER INFORMATION TO BE INCLUDED IN THE PLANS.
Su	SURFACE ROUGHENING SERRATED SLOPES CONSTRUCTION DETAIL S-7 SECTION 205		PROVIDING A ROUGH SOIL SURFACE WITH HORIZONTAL DEPRESSIONS, BY OPERATING A CLEATED DOZER ON THE SLOPE IN A VERTICAL DIRECTION. CREATING SERRATED SLOPES IN THE GRADING PROCESS TO CONSTRUCT BENCHES WILL REDUCE RUNOFF VELOCITY AND INCREASE INFILTRATION OF WATER.  IN MOST CASES THIS BMP IS NOT REQUIRED TO BE SHOWN ON THE PLANS, BUT REQUIRED TO BE COMPLETED BY THE CONTRACTOR UNDER ALL PROJECTS.  IF SERRATED SLOPES ARE SPECIFIED BY THE SOIL SURVEY, THEN THIS BMP SHALL BE SHOWN ON THE PLANS WHERE SERRATED SLOPES ARE TO BE USED.
	LINE CODE 		
Tc-F	TURBIDITY CURTAIN FLOATING CONSTRUCTION DETAIL D-51 SECTION 170		A FLOATING TURBIDITY CURTAIN IS USED TO PREVENT SEDIMENT FROM MOVING IN WATER BY ALLOWING IT TO DROP OUT OF SUSPENSION AND REMAIN WITHIN THE CONSTRUCTION AREA. IT IS TYPICALLY USED WHERE CONSTRUCTION IS REQUIRED IN A LARGE BODY OF WATER SUCH AS LAKES AND RIVERS. IT SHOULD BE USED AS DIRECTED BY THE ENGINEER.  THIS BMP IS ONLY TO BE USED WHEN PERMITTED FILL IS BEING PLACED INTO A STATE WATER, OR AS A SUPPLEMENT TO ADEQUATELY PLACED PERIMETER BMPs.  IT MAY ALSO BE REFERRED TO AS A FLOATING BOOM, SILT BARRIER, OR SILT CURTAIN.
	LINE CODE 		
Tc-S	TURBIDITY CURTAIN STAKED CONSTRUCTION DETAIL D-51 SECTION 170		A STAKED TURBIDITY CURTAIN IS USED TO PREVENT SEDIMENT FROM MOVING IN WATER BY ALLOWING IT TO DROP OUT OF SUSPENSION AND REMAIN WITHIN THE CONSTRUCTION AREA. IT IS TYPICALLY USED IN SHALLOW INUNDATED AREAS. IT MAY BE USED TO PROTECT A SMALL STREAM BEING REALIGNED OR RESTORED. IN THIS CASE, CURTAIN SHOULD EXTEND TO BOTTOM OF STREAMBED. THE HEIGHT SHOULD BE LIMITED TO 5 FEET UNLESS DIRECTED AND EXTEND 2 FEET ABOVE NORMAL WATER ELEVATION. IT SHOULD BE USED AS DIRECTED BY THE ENGINEER.  THIS BMP IS ONLY TO BE USED WHEN PERMITTED FILL IS BEING PLACED INTO A STATE WATER, OR AS A SUPPLEMENT TO ADEQUATELY PLACED PERIMETER BMPs.  IT MAY BE REFERRED TO AS A SILT BARRIER OR SILT CURTAIN.
	LINE CODE 		

CODE	PRACTICE STD OR DETAIL SPEC. SECT.	DETAIL	DESCRIPTION
Sd2-P	CURB INLET PROTECTION  MANUAL FOR EROSION AND SEDIMENT CONTROL, SIXTH EDITION		ONCE PAVEMENT HAS BEEN INSTALLED, OR ON EXISTING INLETS, A CURB INLET FILTER SHALL BE INSTALLED ON INLETS RECEIVING RUNOFF FROM DISTURBED AREAS. THIS METHOD OF INLET PROTECTION SHALL BE REMOVED IF SAFETY HAZARD IS CREATED. PIG-IN-A-BLANKET: 8 IN CMU WRAPPED IN FILTER FABRIC, WIRE, PLASTIC MESH, OR EQUIVALENT MATERIAL. A GAP OF 4 INCHES SHALL BE LEFT BETWEEN THE INLET FILTER AND THE INLET. SEE CONSTRUCTION DETAIL DRAWING ON SHEET 56-0006 FOR DETAILS.
	LINE CODE 		

- NOTE:
- DO NOT USE EROSION CONTROL ITEMS IN A FLOWING STREAM OR IN A TIDAL AREA BELOW HIGH TIDE.
  - FOR ADDITIONAL INFORMATION ON THE DESIGN AND APPLICATION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), REFER TO THE LATEST EDITION OF THE GEORGIA SOIL AND WATER CONSERVATION COMMISSION'S, 'MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA'.



NO SCALE

REVISION DATES		EROSION CONTROL LEGEND	
3/2/2017		UNIFORM CODE SHEET	
		SHEET 7 OF 7	
CHECKED:	D. EAGLETON	DATE:	01/01/16
BACKCHECKED:		DATE:	
CORRECTED:		DATE:	
VERIFIED:		DATE:	
		DRAWING No. 52-0007	

REVISION DATES		EROSION CONTROL LEGEND	
8/16/2019		BRIARWOOD ROAD SIDEWALK	
		CITY OF BROOKHAVEN	
CHECKED:		DATE:	
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VERIFIED:		DATE:	
		DRAWING No. 52-0007	

**24 HOUR CONTACT:**

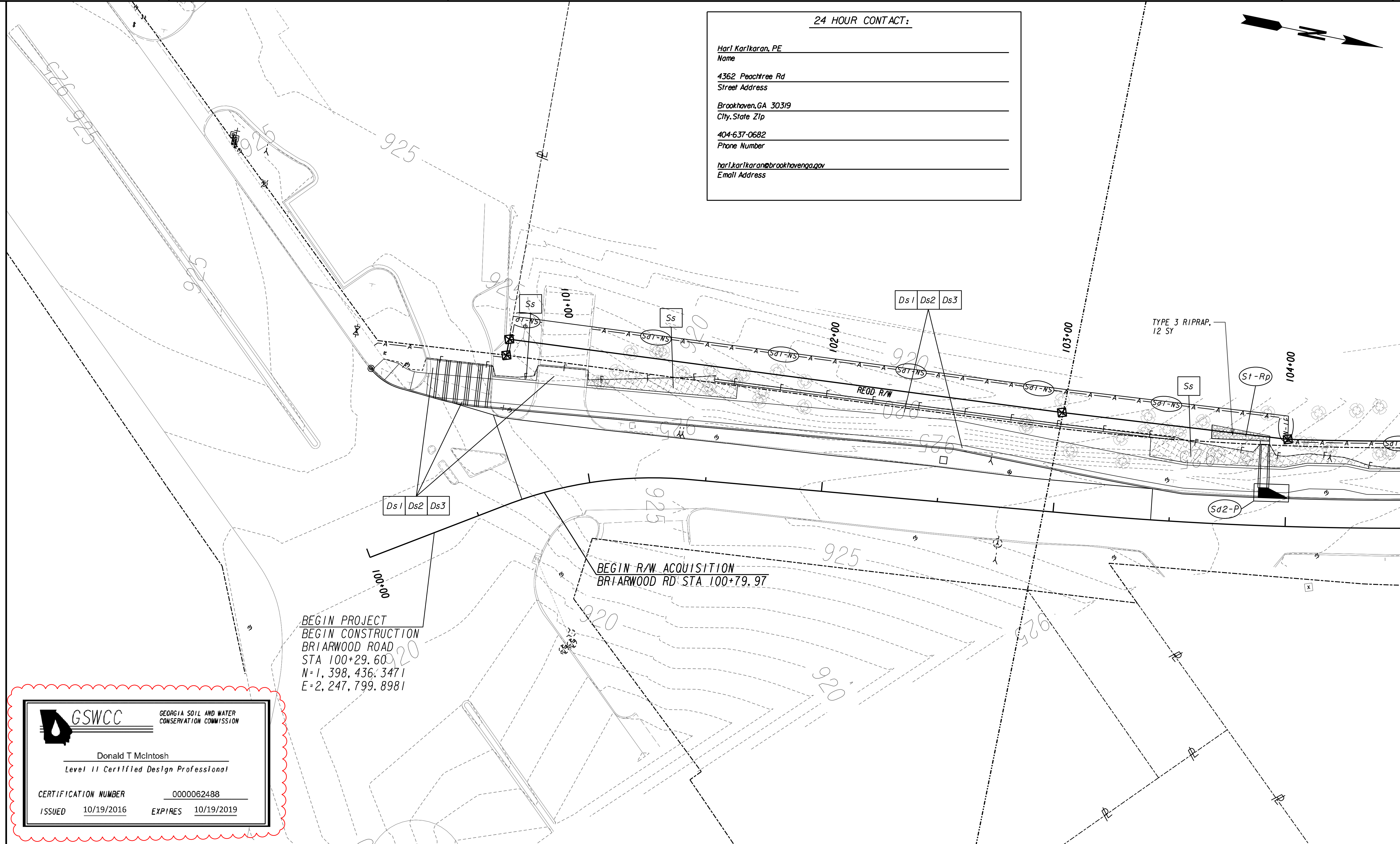
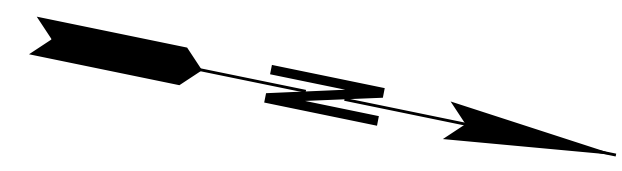
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BEGIN PROJECT  
BEGIN CONSTRUCTION  
BRIARWOOD ROAD  
STA 100+29.60  
N=1, 398, 436, 3471  
E=2, 247, 799, 8981

BEGIN R/W ACQUISITION  
BRIARWOOD RD STA 100+79.97

MATCHLINE STA 104+50.00  
SEE DRWG 54-0002

**GSWCC** GEORGIA SOIL AND WATER CONSERVATION COMMISSION

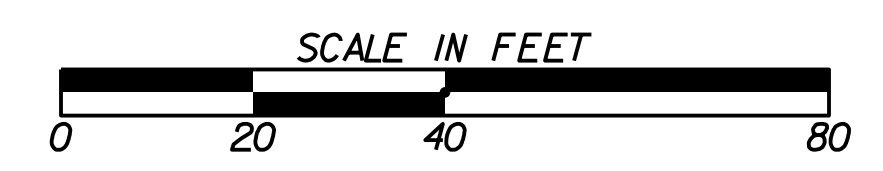
Donald T McIntosh  
Level II Certified Design Professional

CERTIFICATION NUMBER 0000062488  
ISSUED 10/19/2016 EXPIRES 10/19/2019

PROPERTY AND EXISTING R/W LINE	-----e-----
REQUIRED R/W LINE	-----f-----
CONSTRUCTION LIMITS	-----g-----
EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES	-----h-----
EASEMENT FOR CONSTR OF SLOPES	-----i-----
EASEMENT FOR CONSTR OF DRIVES	-----j-----

BEGIN LIMIT OF ACCESS.....BLA	-----k-----
END LIMIT OF ACCESS.....ELA	-----l-----
LIMIT OF ACCESS	-----m-----
REQ'D R/W & LIMIT OF ACCESS	-----n-----
ORANGE BARRIER FENCE	-----o-----
ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)	-----p-----

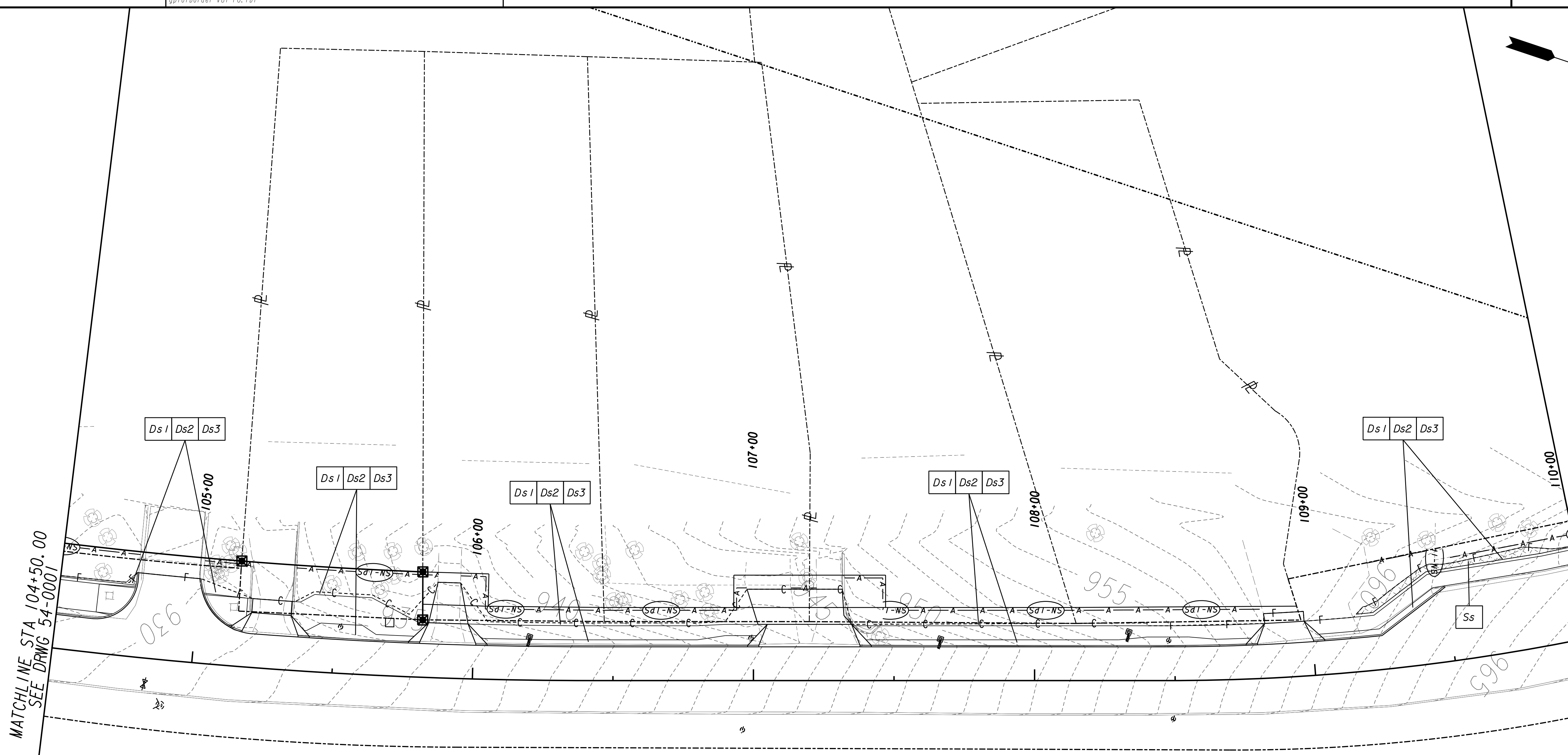
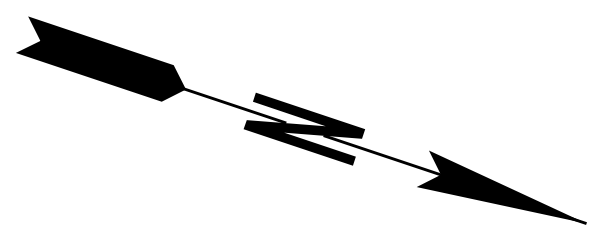
**IE INFRASTRUCTURE**  
CONSULTING & ENGINEERING



REVISION DATES	
8/16/2019	

**BMP LOCATION DETAILS**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 100+36.07 TO STA 104+50.00

CHECKED:	DATE:	DRAWING No. <b>54-0001</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	



MATCHLINE STA 104+50.00  
SEE DRWG 54-0001

MATCHLINE STA 110+00.00  
SEE DRWG 54-0003

**24 HOUR CONTACT:**

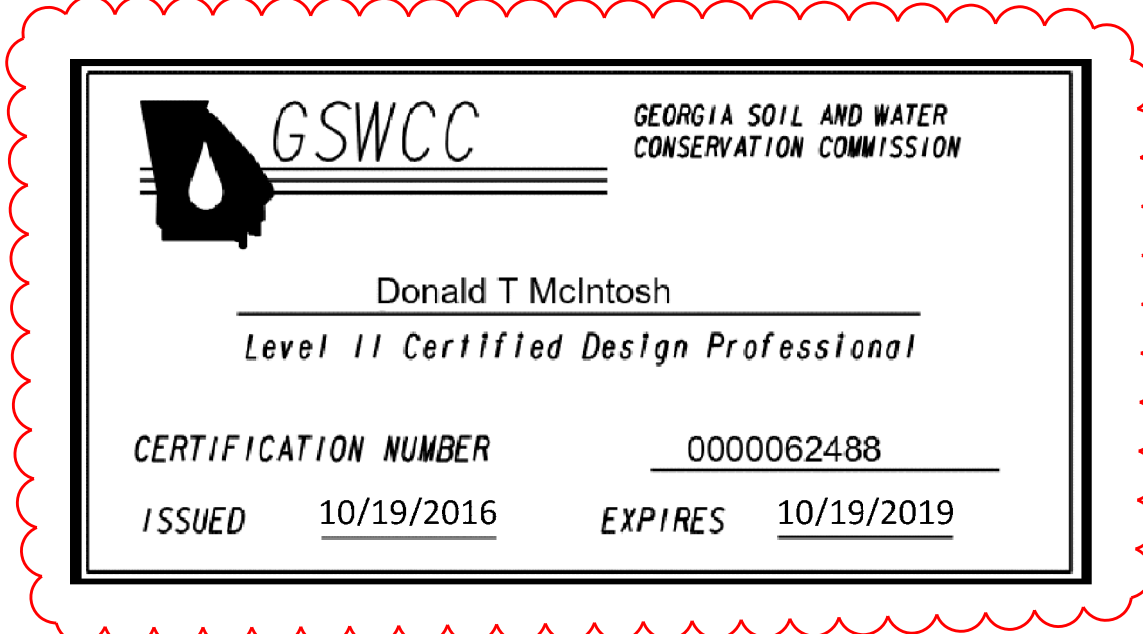
Harj Karikaran, PE  
Name

4362 Peachtree Rd  
Street Address

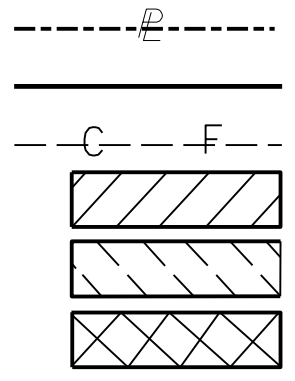
Brookhaven, GA 30319  
City, State Zip

404-637-0682  
Phone Number

harj.karikaran@brookhavenga.gov  
Email Address



PROPERTY AND EXISTING R/W LINE  
REQUIRED R/W LINE  
CONSTRUCTION LIMITS  
EASEMENT FOR CONSTR  
& MAINTENANCE OF SLOPES  
EASEMENT FOR CONSTR OF SLOPES  
EASEMENT FOR CONSTR OF DRIVES



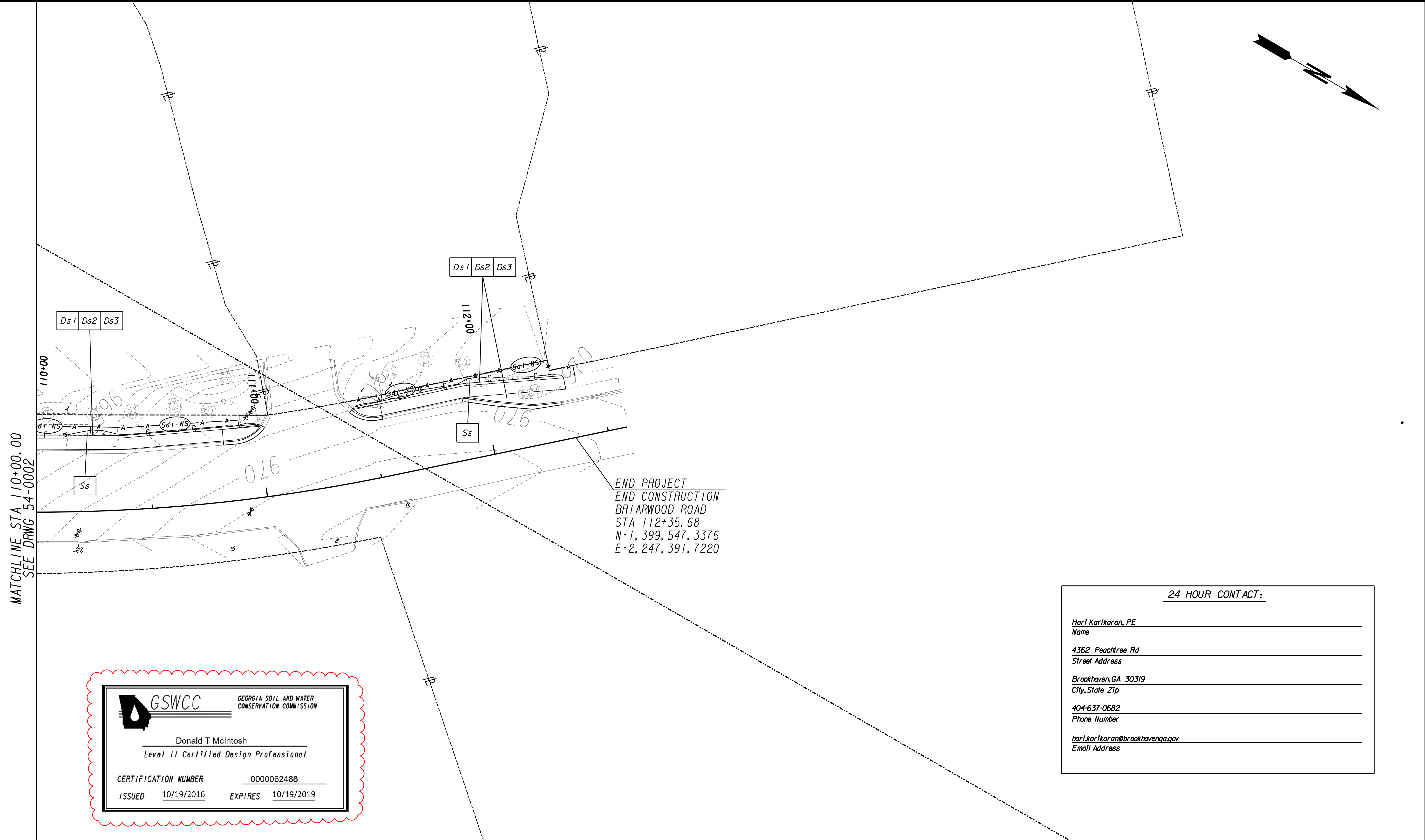
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REQ'D R/W & LIMIT OF ACCESS  
ORANGE BARRIER FENCE  
ESA - ENV. SENSITIVE AREA  
(SEE ERIT TABLE)

**INFRASTRUCTURE**  
CONSULTING & ENGINEERING




REVISION DATES	
8/16/2019	

BMP LOCATION DETAILS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN STA 104+50.00 TO STA 110+00.00		
CHECKED:	DATE:	DRAWING No. <b>54-0002</b>
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	



MATCHLINE STA 110+00.00  
SEE DRWG 54-0002

END PROJECT  
END CONSTRUCTION  
BRIARWOOD ROAD  
STA 112+35.68  
N=1,399,547.3376  
E=2,247,391.7220



**GSWCC**  
GEORGIA SOIL AND WATER  
CONSERVATION COMMISSION

Donald T McIntosh  
Level II Certified Design Professional

CERTIFICATION NUMBER 0000062488  
ISSUED 10/19/2016 EXPIRES 10/19/2019

24 HOUR CONTACT:

Hari Karikaran, PE  
Name

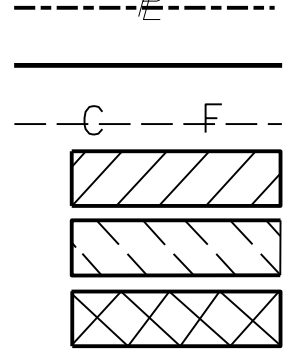
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Street Address

Brookhaven, GA 30319  
City, State Zip

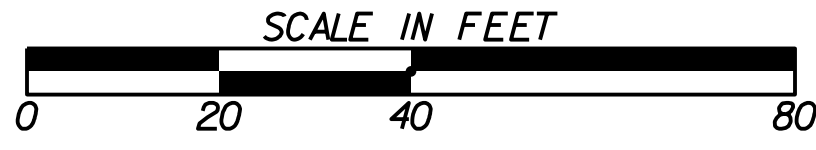
404-637-0682  
Phone Number

hari.karikaran@brookhavenga.gov  
Email Address

PROPERTY AND EXISTING R/W LINE  
REQUIRED R/W LINE  
CONSTRUCTION LIMITS  
EASEMENT FOR CONSTR  
& MAINTENANCE OF SLOPES  
EASEMENT FOR CONSTR OF SLOPES  
EASEMENT FOR CONSTR OF DRIVES



BEGIN LIMIT OF ACCESS.....BLA  
END LIMIT OF ACCESS.....ELA  
LIMIT OF ACCESS  
REQ'D R/W & LIMIT OF ACCESS  
ORANGE BARRIER FENCE  
ESA - ENV. SENSITIVE AREA  
(SEE ERIT TABLE)

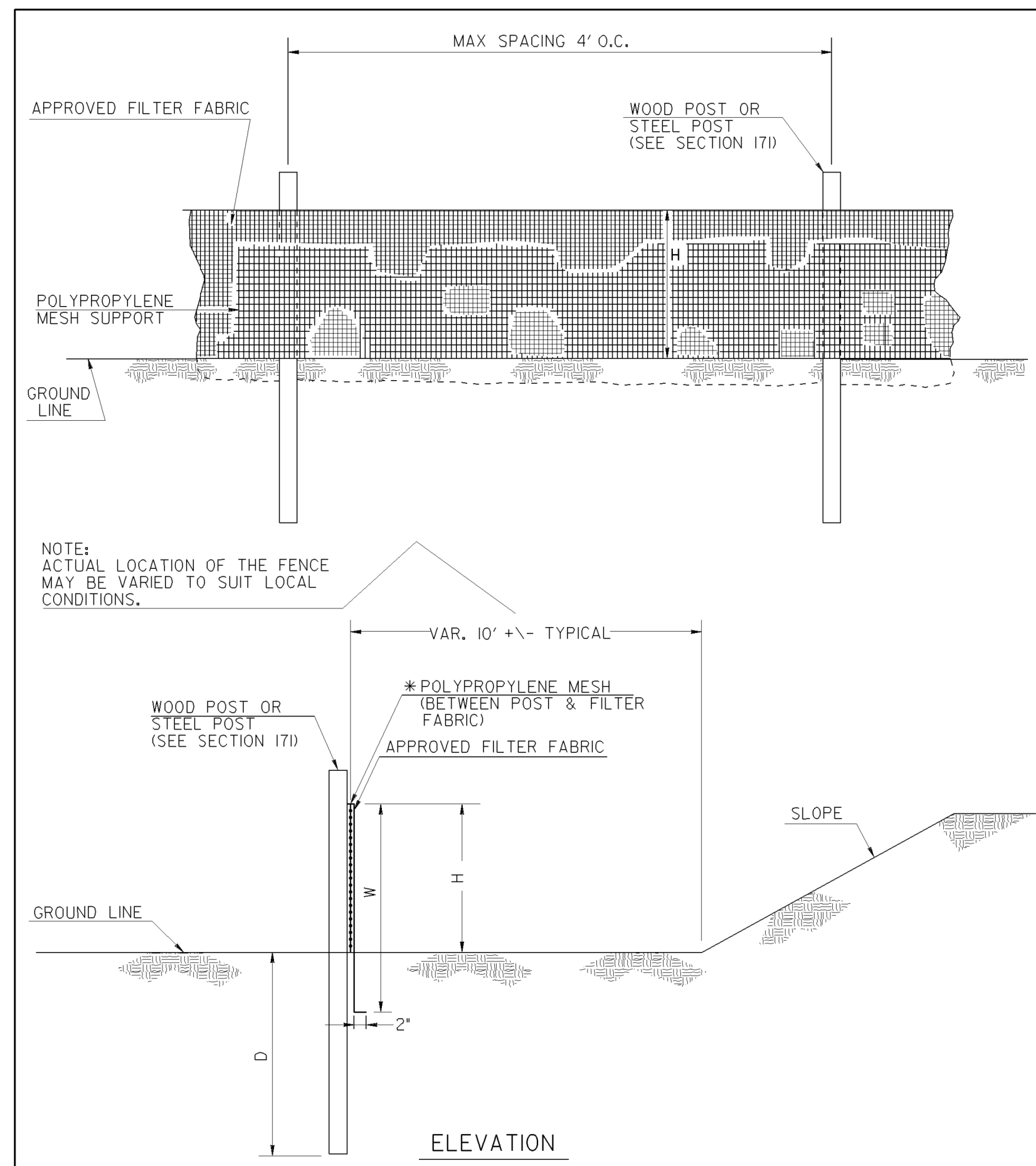


REVISION DATES	
8/16/2016	

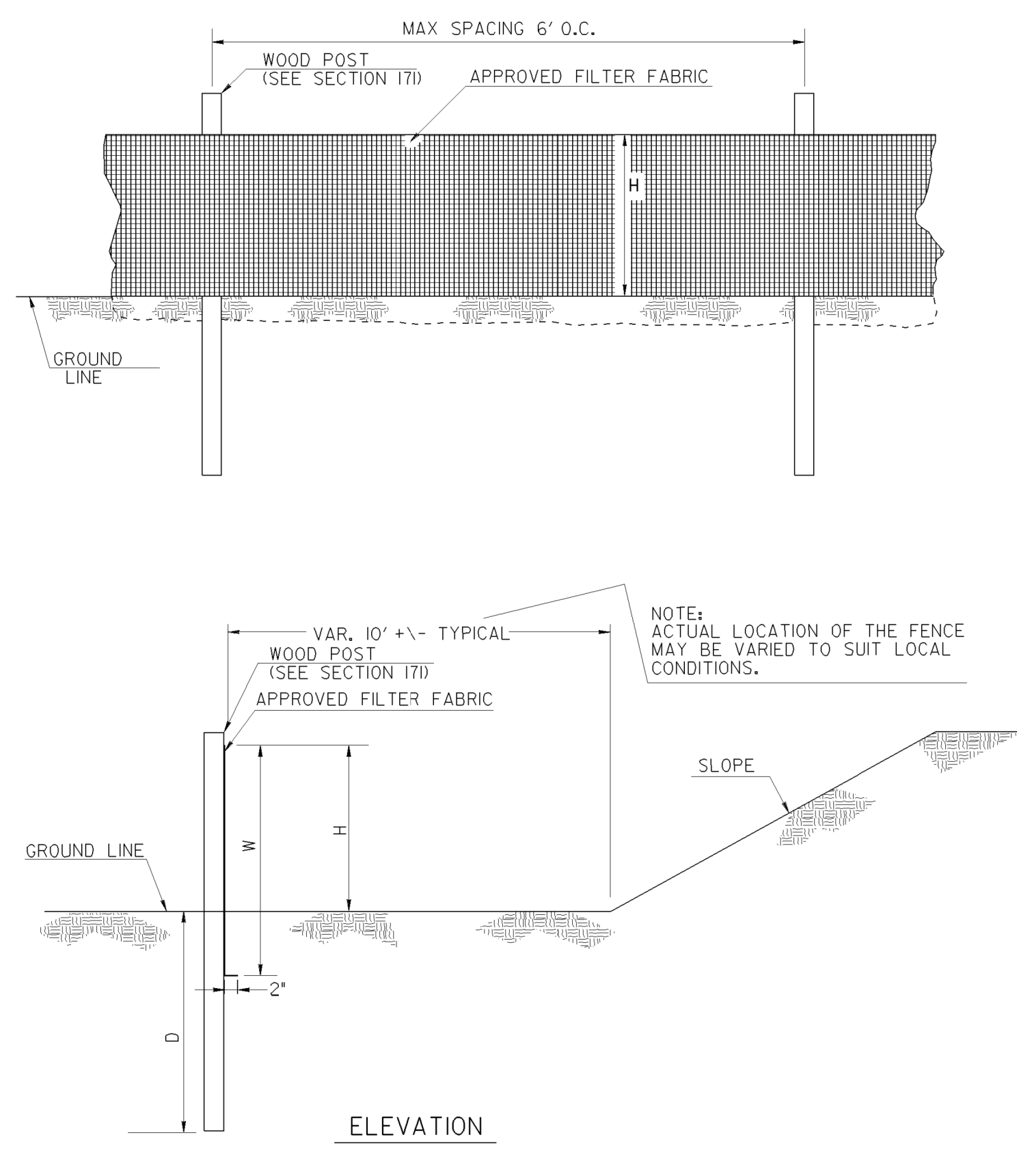
**BMP LOCATION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN  
STA 110+00.00 TO STA 112+35.68**

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CORRECTED:	DATE:	
VERIFIED:	DATE:	

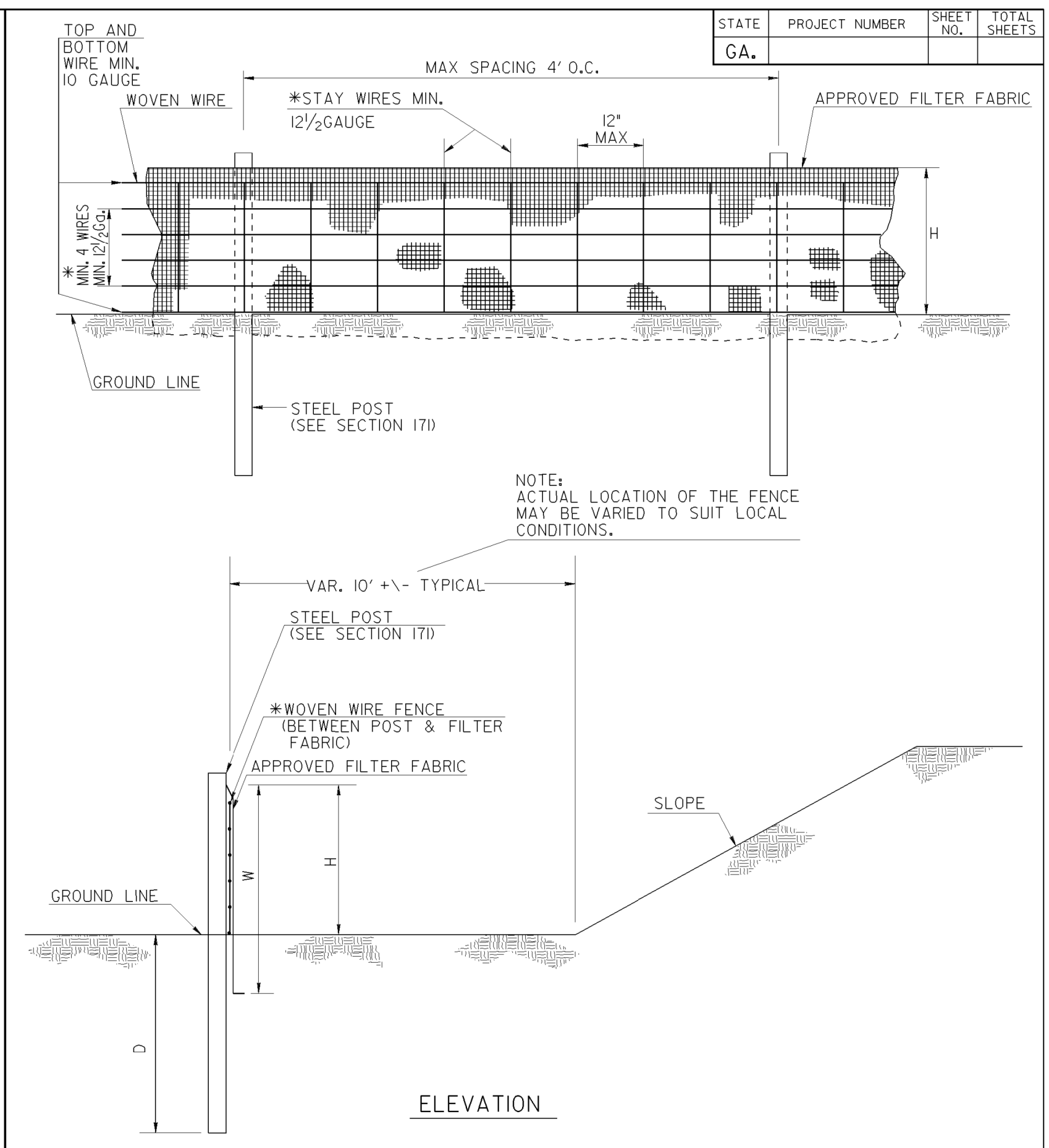
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SINGLE ROW TYPE C SILT FENCE WITH POLYPROPYLENE MESH SUPPORT



SINGLE ROW TYPE A SILT FENCE



SINGLE ROW TYPE C SILT FENCE WITH WOVEN WIRE SUPPORT

FENCE TYPE	POST LENGTH	H	D	W	TYPICAL USES
TYPE *A*	4 FT.	2'-4"	1'-6"	3'-0"	
TYPE *C*	4 FT.	2'-4"	1'-6"	3'-0"	AT BRIDGE END ROLLS, DOUBLE ROW ALONG STREAMS, WETLANDS AND ENVIRONMENTALLY SENSITIVE AREAS FOR USE OF THIS MATERIAL IN FABRIC CHECKDAMS SEE D-24D.

NOTES:

1. WIRE STAPLES SHALL BE AT LEAST 17 GAUGE, WITH LEGS AT LEAST 1/2 INCHES LONG AND A CROWN AT LEAST 3/4 INCHES WIDE. NAILS SHALL BE AT LEAST 14 GAUGE, 1 INCH LONG, WITH BUTTON HEADS AT LEAST 3/4 INCHES WIDE.
2. NAILS OR STAPLES SHALL BE EVENLY PLACED WITH AT LEAST 5 PER POST FOR TYPE A FENCE AND 4 PER POST FOR TYPE C FENCE.
3. THE VERTICAL WIRES FOR THE WOVEN WIRE SUPPORT FENCE SHALL HAVE A MAXIMUM SPACING OF 12 INCHES. THE TOP AND BOTTOM WIRES SHALL BE AT LEAST 10 GAUGE AND ALL OTHER WIRES SHALL BE AT LEAST 12 1/2 GAUGE.
4. TEMPORARY SILT FENCE INSTALLATION IS DIFFERENT THAN THE SILT RETENTION BARRIER INSTALLATION.
5. SEE SECTION 171 FOR SILT FENCE SPECIFICATIONS.
6. SEE SECTION 894 FOR FENCING SPECIFICATIONS.
7. SEE OPL-36 FOR A LIST APPROVED SILT FENCE FABRIC.
8. TEMPORARY SILT FENCE SHALL NOT BE PLACED WITHIN STATE WATERS UNLESS PERMITTED.

DATE	DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA
REVISION	CONSTRUCTION DETAILS TEMPORARY SILT FENCE
BY	NO SCALE REV. AND REDRAWN JAN. 2011
	NUMBER D-24A (SHEET 1 OF 4)

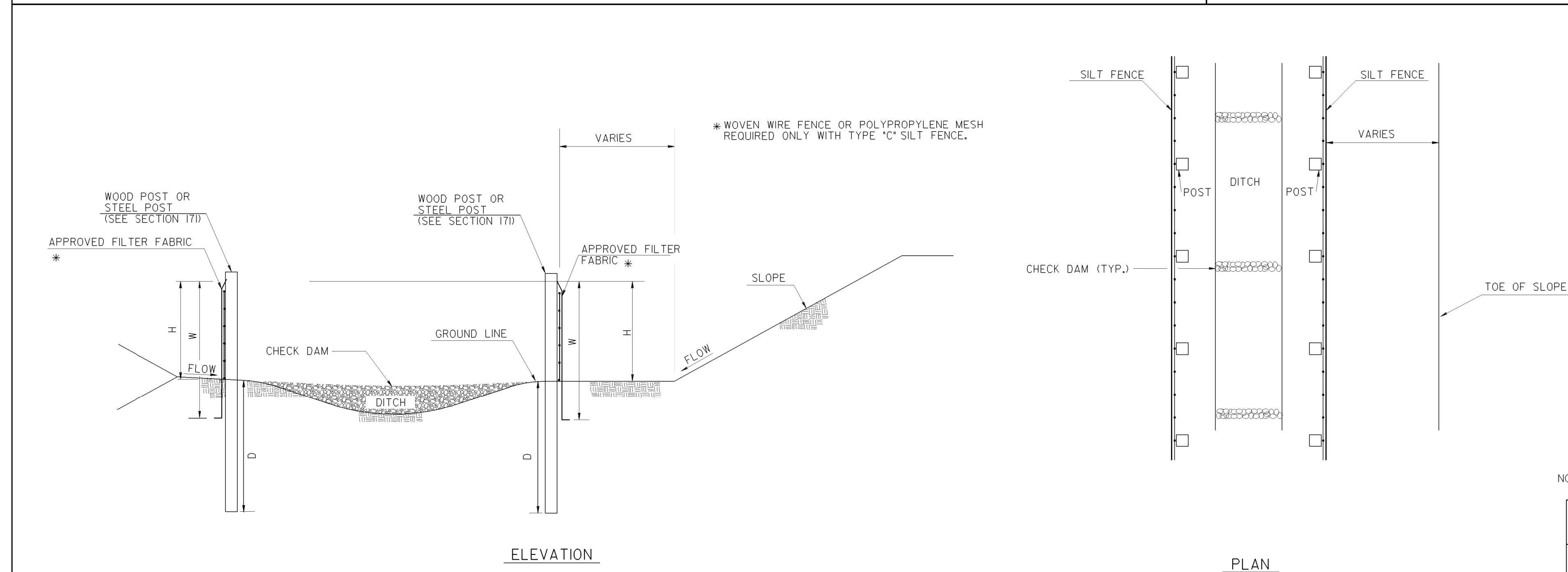
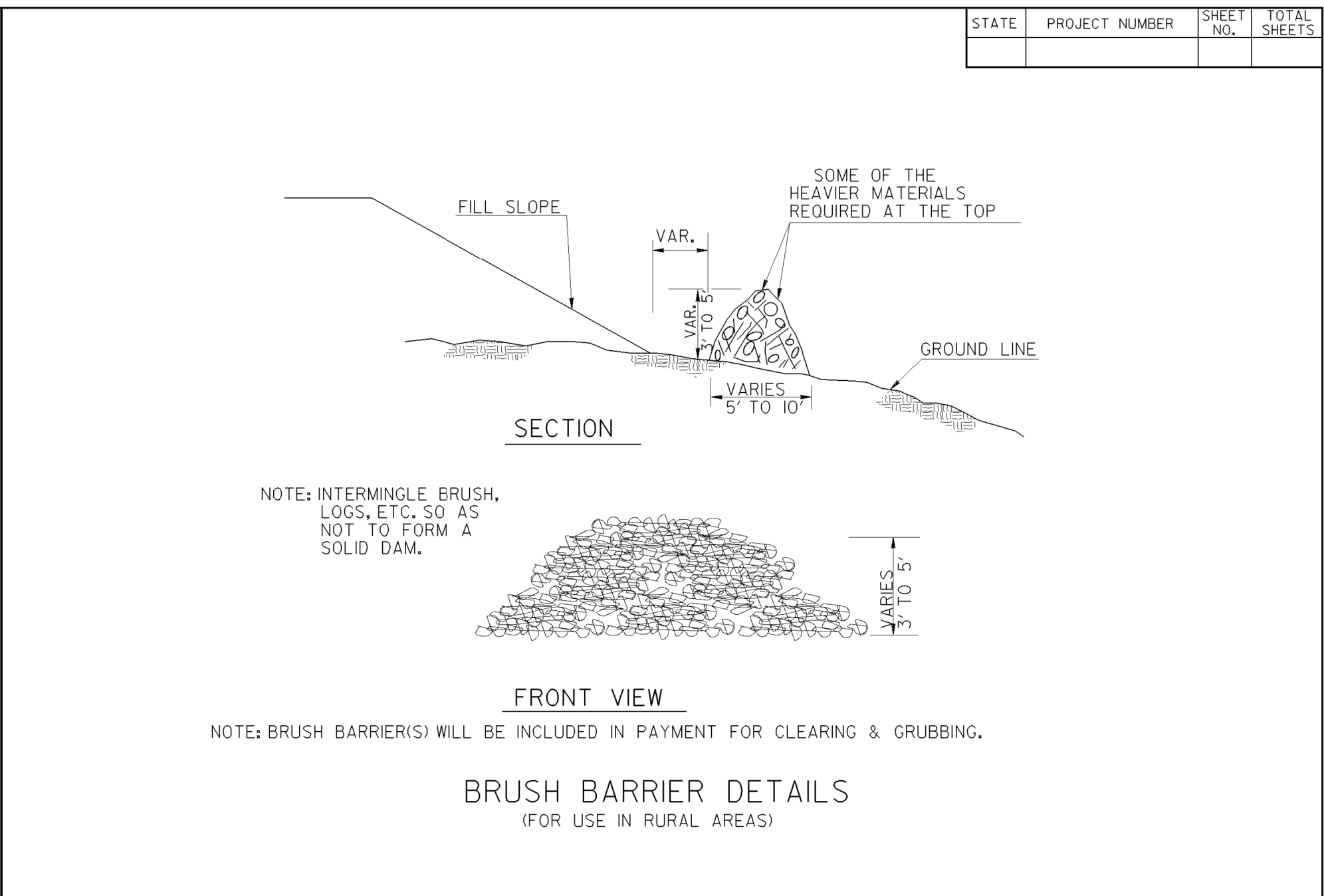
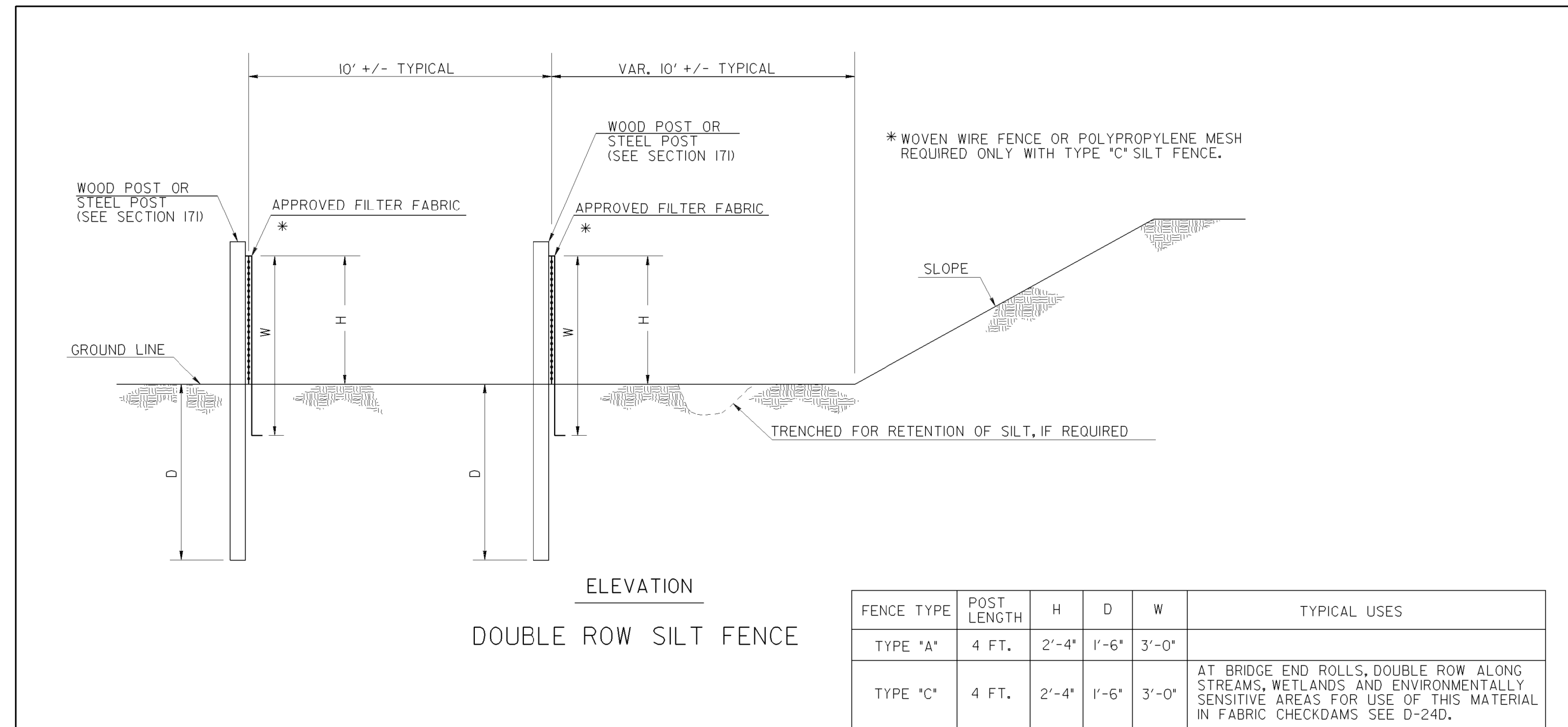
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REVISION DATES

**EROSION CONTROL CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	56-0001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

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FENCE TYPE	POST LENGTH	H	D	W	TYPICAL USES
TYPE 'A'	4 FT.	2'-4"	1'-6"	3'-0"	AT BRIDGE END ROLLS, DOUBLE ROW ALONG STREAMS, WETLANDS AND ENVIRONMENTALLY SENSITIVE AREAS FOR USE OF THIS MATERIAL IN FABRIC CHECKDAMS SEE D-24D.
TYPE 'C'	4 FT.	2'-4"	1'-6"	3'-0"	

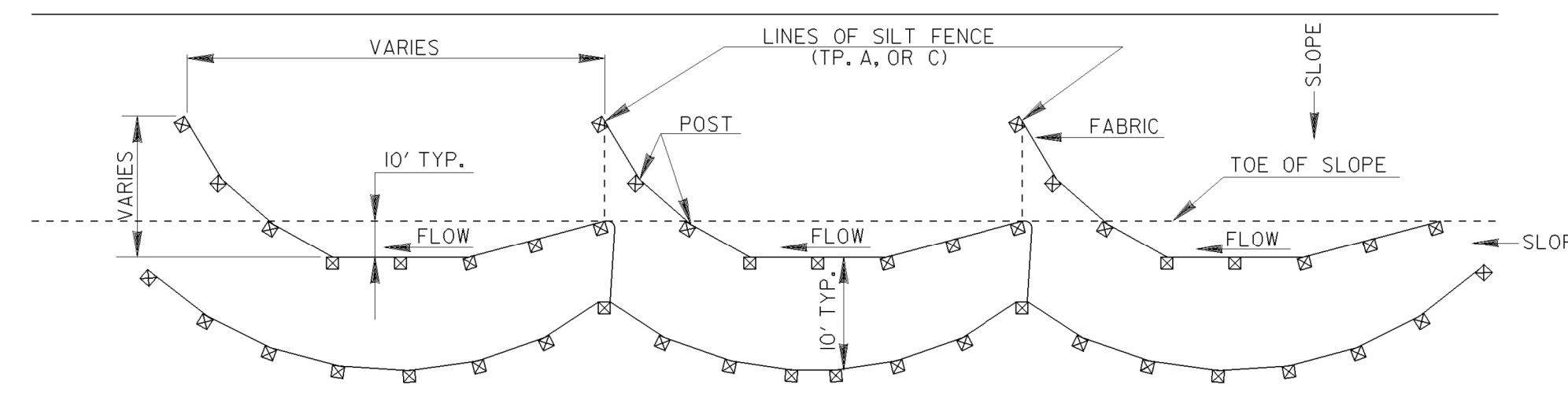
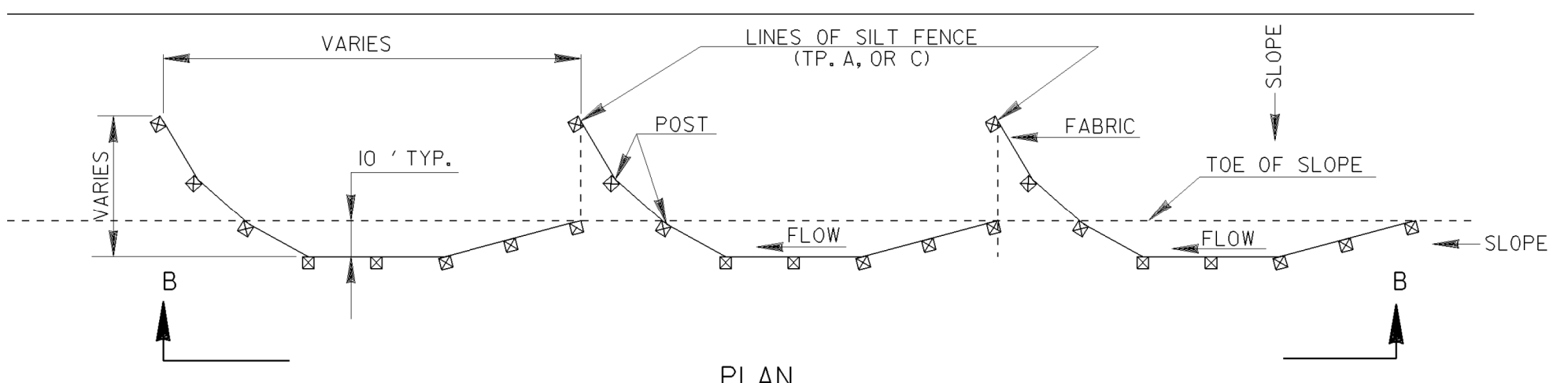
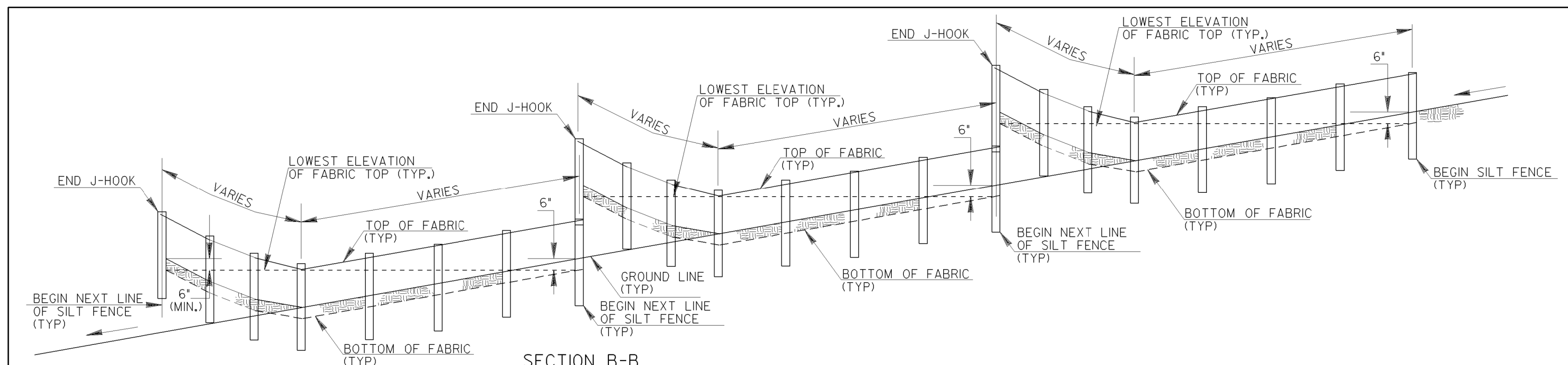
**SILT FENCE**  
PERIMETER INSTALLATION ALONG DITCH SECTION

NOTE: TEMPORARY SILT FENCE SHALL NOT BE PLACED WITHIN STATE WATERS.

DATE	DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REVISION	CONSTRUCTION DETAILS TEMPORARY SILT FENCE BERM DITCH, INSTALLATION, BRUSH BARRIER	
BY	NO SCALE	REV. AND REDRAWN JAN. 2011
	NUMBER D-24B (SHEET 2 OF 4)	

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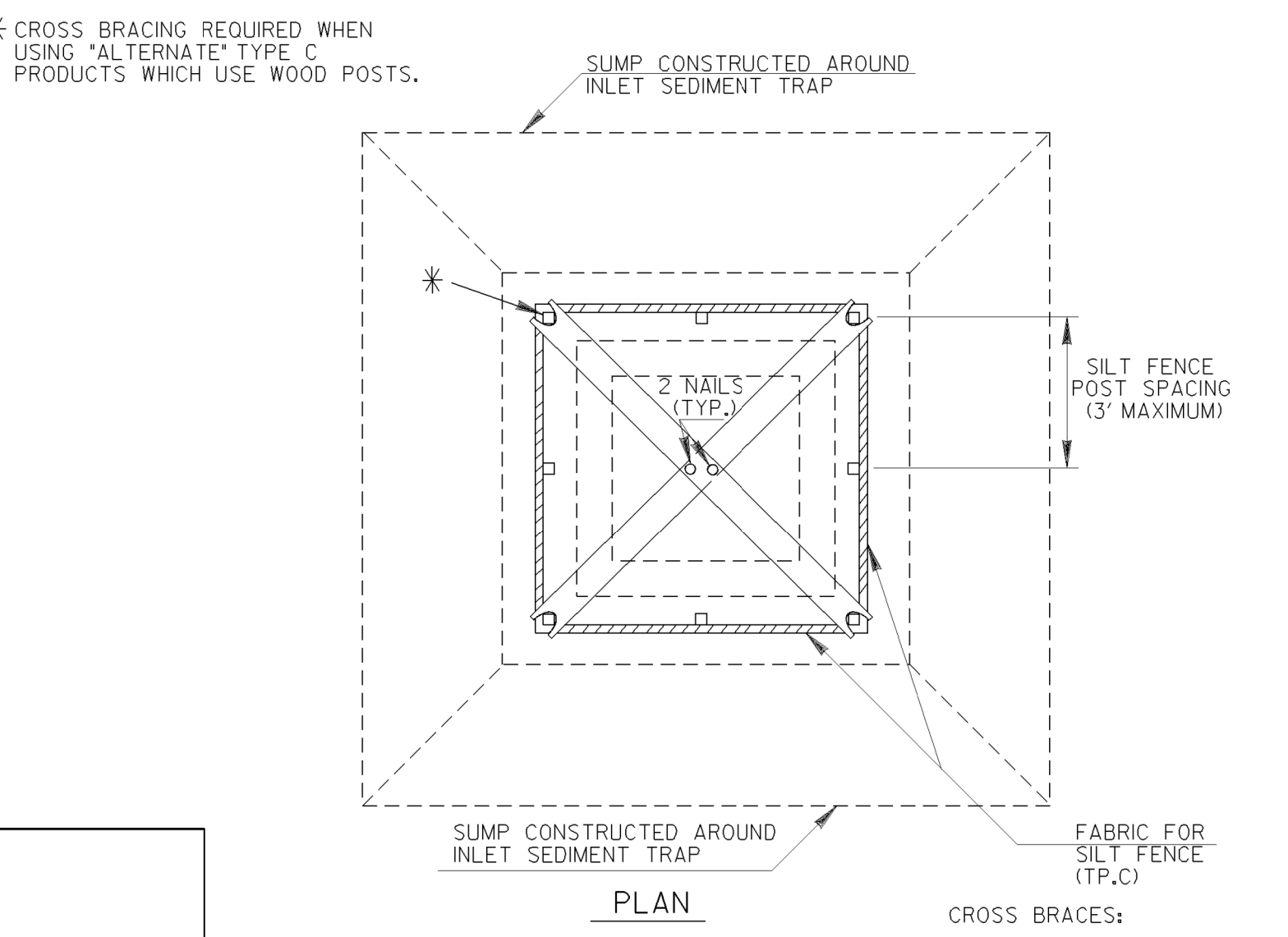
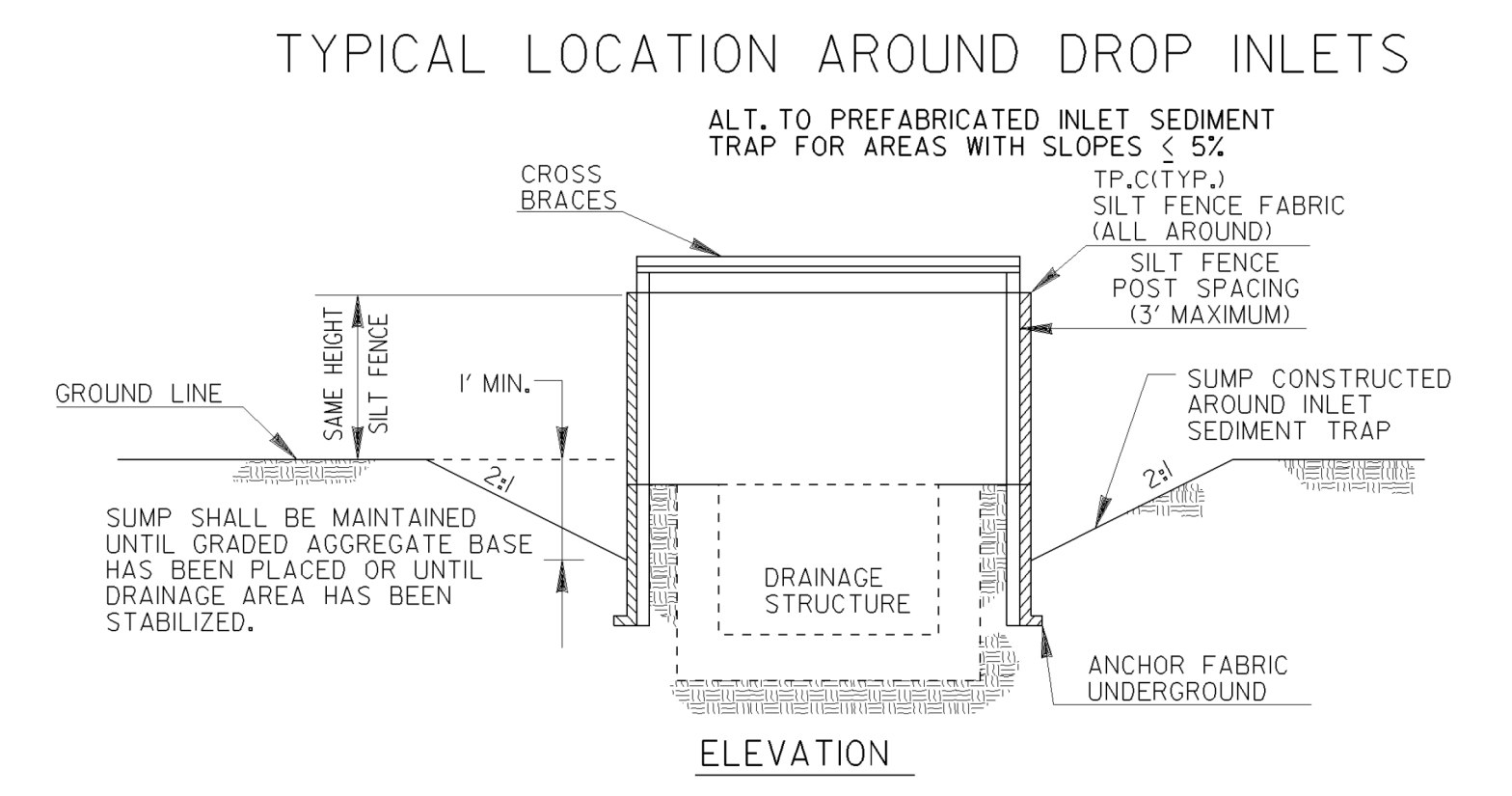
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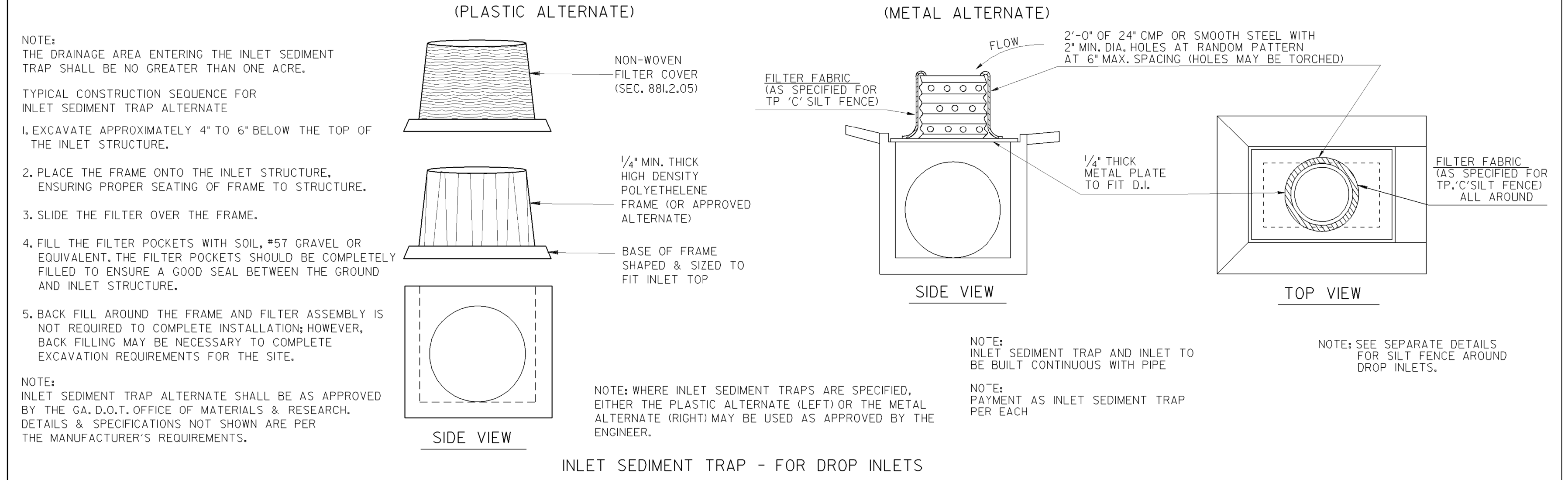
TYPICAL J HOOK SPACING		
SLOPE PERCENT	TYPE OF SILT FENCE	MINIMUM SPACING (FEET)
1% TO 2%	TYPE A	100' ±
2% TO 3%	TYPE A	50' ±
3% TO 4%	TYPE C	50' ±
4% TO 5%	TYPE C	25' ±

NOTE:  
1. IF THE GRADE IS BETWEEN 0 TO 1 PERCENT, THE SILT FENCE SHALL BE PLACED ACROSS THE DITCH.  
2. TEMPORARY SILT FENCE SHALL NOT BE PLACED WITHIN STATE WATERS.

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



NOTE: PAYMENT AS INLET SEDIMENT TRAP PER EACH.  
NOTE: SEE SEPARATE SHEET ENTITLED 'TEMPORARY SILT FENCE DETAILS' FOR SILT FENCE ERECTION DETAILS.



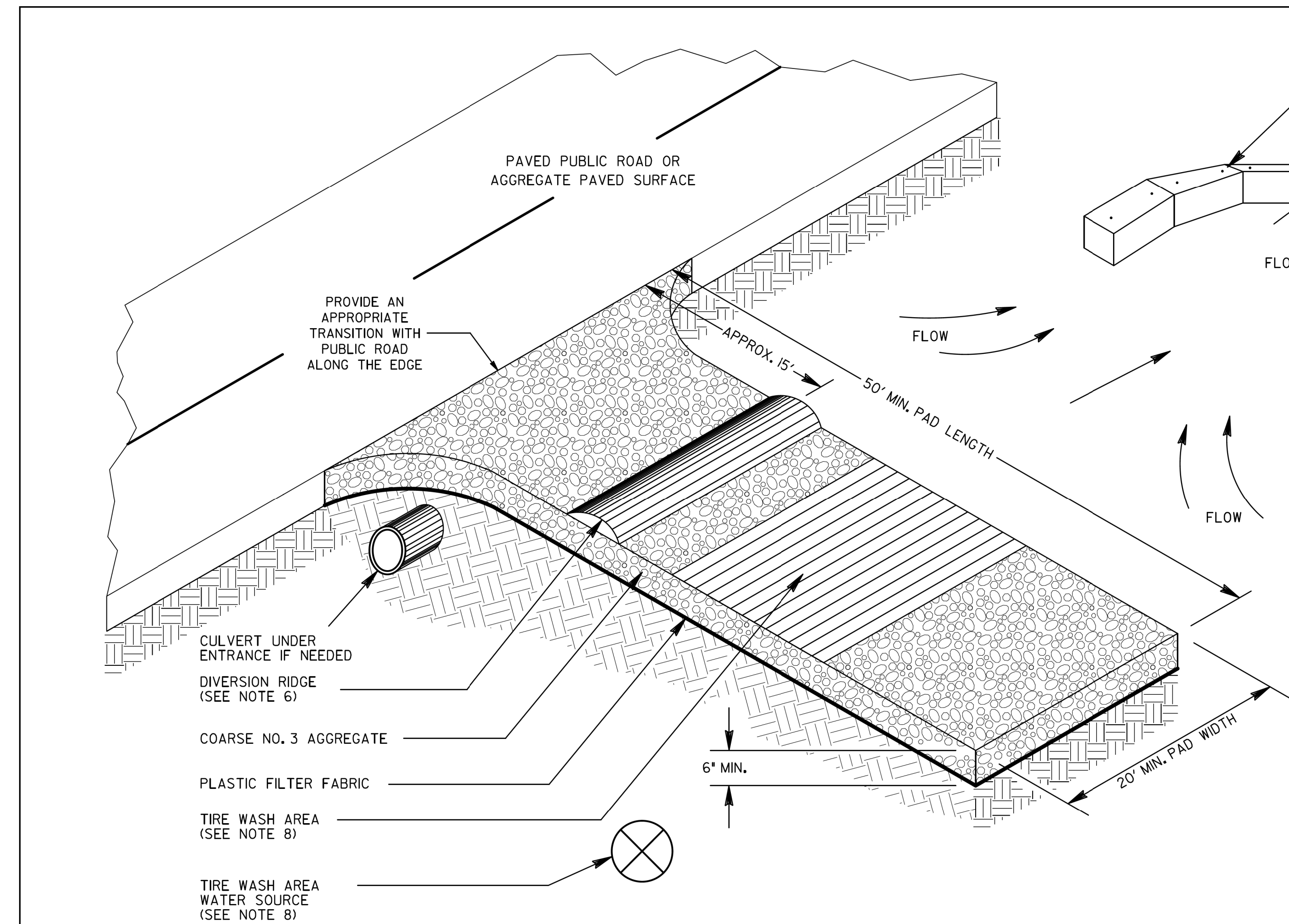
NOTE: THE DRAINAGE AREA ENTERING THE INLET SEDIMENT TRAP SHALL BE NO GREATER THAN ONE ACRE.  
TYPICAL CONSTRUCTION SEQUENCE FOR INLET SEDIMENT TRAP ALTERNATE  
1. EXCAVATE APPROXIMATELY 4\"/>

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
CONSTRUCTION DETAILS TEMPORARY SILT FENCE J-HOOK, INLET SEDIMENT TRAPS	
NO SCALE	JANUARY 2011
BY	NUMBER D-24C (SHEET 3 OF 4)

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REVISION DATES		EROSION CONTROL CONSTRUCTION DETAILS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN	
CHECKED:	DATE:	CHECKED:	DATE:
BACKCHECKED:	DATE:	BACKCHECKED:	DATE:
CORRECTED:	DATE:	CORRECTED:	DATE:
VERIFIED:	DATE:	VERIFIED:	DATE:
		DRAWING No. 56-0003	

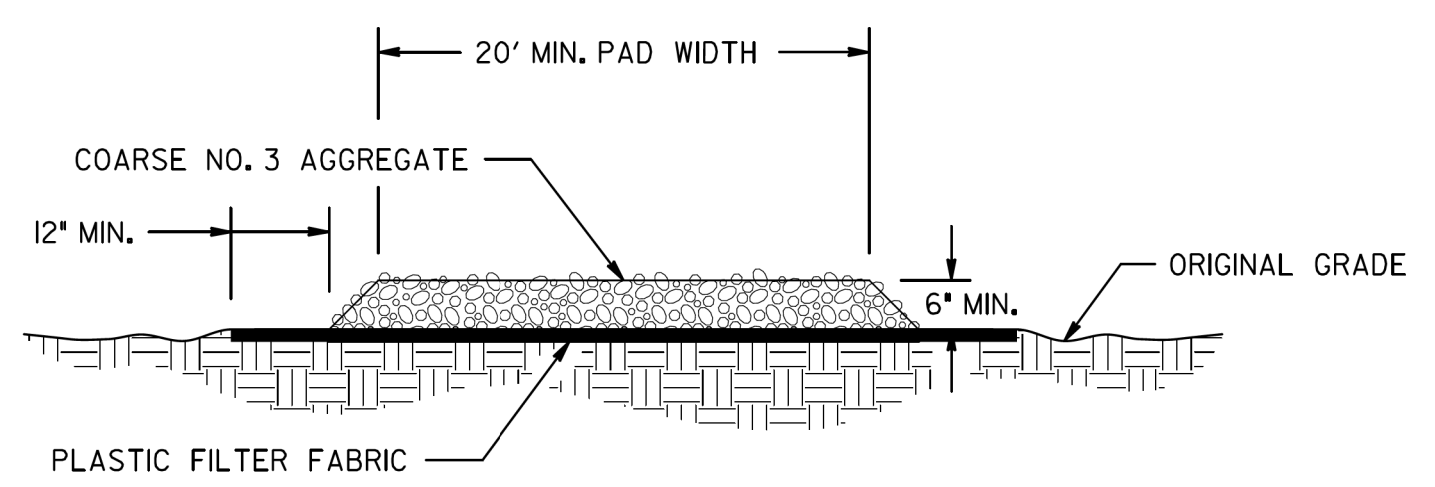
STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			



**GENERAL NOTES:**

1. AVOID LOCATING CONSTRUCTION EXITS ON STEEP SLOPES OR AT SHARP CURVES ON PUBLIC ROADS. CONSTRUCTION EXITS ARE NOT REQUIRED FOR DIRT PUBLIC ROADS.
  2. REMOVE ALL VEGETATION AND OTHER UNSUITABLE MATERIAL FROM THE FOUNDATION AREA AND GRADE FOR POSITIVE DRAINAGE.
  3. AGGREGATE SIZE SHALL BE COARSE NO. 3 AGGREGATE WITH 0.0% PASSING THE 106 INCH U.S. STANDARD SIEVE.
  4. GRAVEL PAD SHALL HAVE A MINIMUM THICKNESS OF 6 INCHES AND PLACED ON APPROVED PLASTIC FILTER FABRIC.
  5. GRAVEL PAD WIDTH SHALL BE EQUAL FULL WIDTH AT ALL POINTS OF VEHICULAR EGRESS, BUT NO LESS THAN 20'.
  6. PROVIDE A TRAVERSABLE DIVERSION RIDGE CONSTRUCTED OF AGGREGATE 6 INCHES TO 8 INCHES HIGH WHEN GRADE TOWARD PAVED AREA IS GREATER THAN 2%.
  7. INSTALL CULVERT UNDER THE ENTRANCE IF NEEDED TO MAINTAIN DRAINAGE DITCHES.
  8. IF THE ACTION OF THE VEHICLE TRAVELING OVER THE GRAVEL PAD DOES NOT SUFFICIENTLY REMOVE THE MUD PRIOR TO ENTERING PUBLIC ROADS, THE CONTRACTOR SHALL ADD A CONSTRUCTION EXIT TIRE WASH ASSEMBLY TO AN EXISTING CONSTRUCTION EXIT WHEN DIRECTED BY THE ENGINEER. THE CONSTRUCTION EXIT TIRE WASH ASSEMBLY INCLUDES: TIRE WASH AREA, WATER SOURCE, AND SEDIMENT TRAP OR OTHER ACCEPTABLE SEDIMENT STORAGE DEVICE.  
  
THE WASHING SHALL BE DONE ON AN AREA STABILIZED WITH AGGREGATE THAT DRAINS INTO A SEDIMENT TRAP OR OTHER ACCEPTABLE SEDIMENT STORAGE DEVICE. DIVERT ALL SURFACE RUNOFF AND DRAINAGE FROM THE CONSTRUCTION EXIT TO THE SEDIMENT CONTROL DEVICE. ACCEPTABLE SEDIMENT STORAGE DEVICE EXAMPLES INCLUDE TEMPORARY SEDIMENT TRAPS, HAY BALES OR STONE FILTER RING WITH THE SEDIMENT STORAGE SIZED FOR 67 CUBIC YARDS PER ACRE OF DRAINAGE. TIRE WASHING SHALL BE DONE MANUALLY OR BY EQUIPMENT SUITABLE FOR TRUCK TRAFFIC THAT REMOVES MUD AND DIRT.
  9. AGGREGATE SHALL BE KEPT LOOSE OR SCARIFIED WHEN AGGREGATE BECOMES CONSOLIDATED.
  10. CONSTRUCTION EXIT SHALL BE MAINTAINED IN A CONDITION THAT PREVENTS TRACKING AND/OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR, AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. MAINTENANCE OF CONSTRUCTION EXIT WILL BE PAID ON THE BASIS OF HAVING OR NOT HAVING A CONSTRUCTION EXIT TIRE WASH ASSEMBLY WHEN DIRECTED BY THE ENGINEER. ALL MUD AND DEBRIS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES OR SITE ONTO ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY.
- SEE STANDARD SPECIFICATION 163, AND SUPPLEMENTS THERETO FOR THE CONSTRUCTION AND REMOVAL OF CONSTRUCTION EXITS. SEE STANDARD SPECIFICATION 165, AND SUPPLEMENTS THERETO FOR THE MAINTENANCE OF CONSTRUCTION EXITS.

**ENTRANCE ELEVATION**



PAY ITEM:	CONSTRUCTION EXIT	(EA)
163-0300	CONSTRUCTION EXIT TIRE WASH ASSEMBLY	(EA)
163-0310	MAINTENANCE OF CONSTRUCTION EXIT	(EA)
165-0101	MAINTENANCE OF CONSTRUCTION EXIT TIRE WASH ASSEMBLY	(EA)
165-0310		

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
CONSTRUCTION DETAILS	
CONSTRUCTION EXIT	
NO SCALE	FEBRUARY 2001
DLE	DESIGNED
DLE	DRAWN
TPC	TRACED
BY	CHECKED
NUMBER D-41	

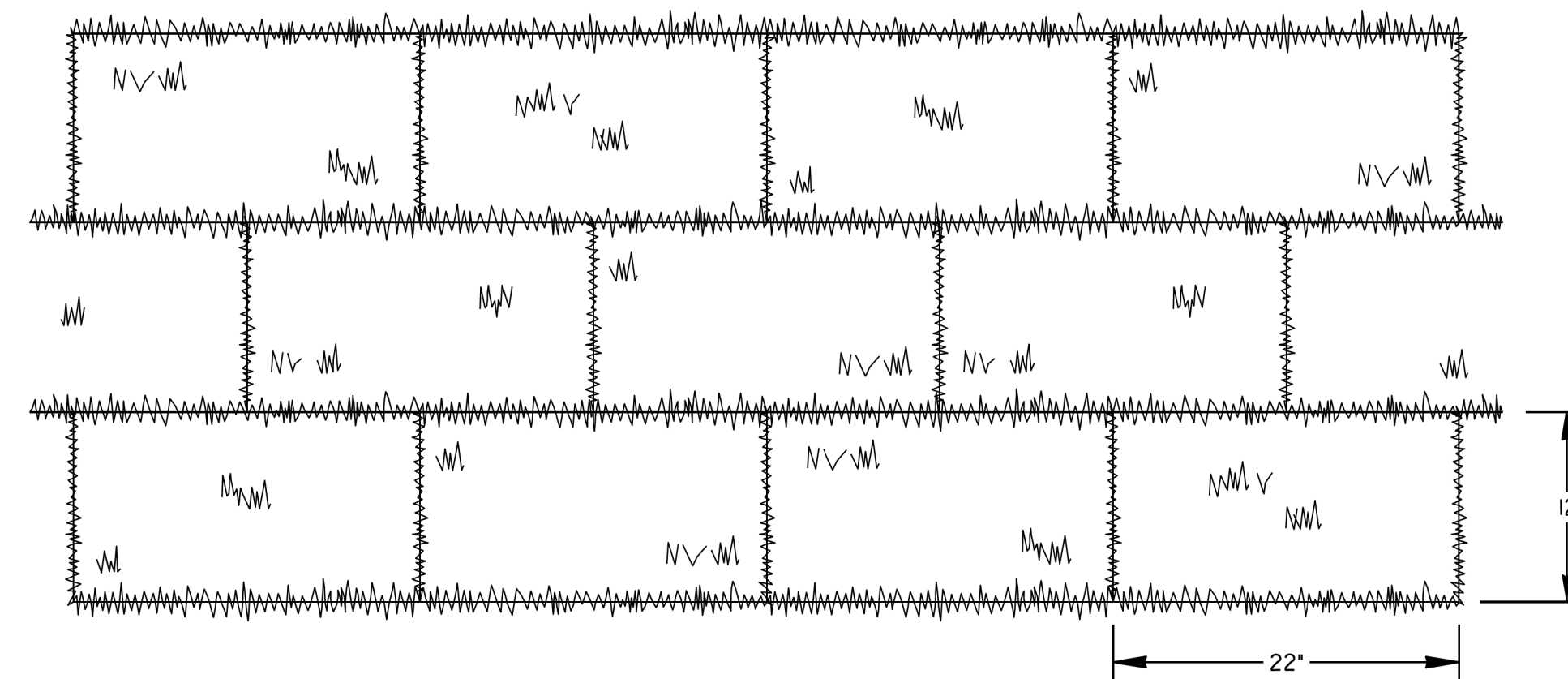
REVISION DATES	
9/3/2019	

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	56-0004
VERIFIED:	DATE:	



STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

**SOD LAYOUT**

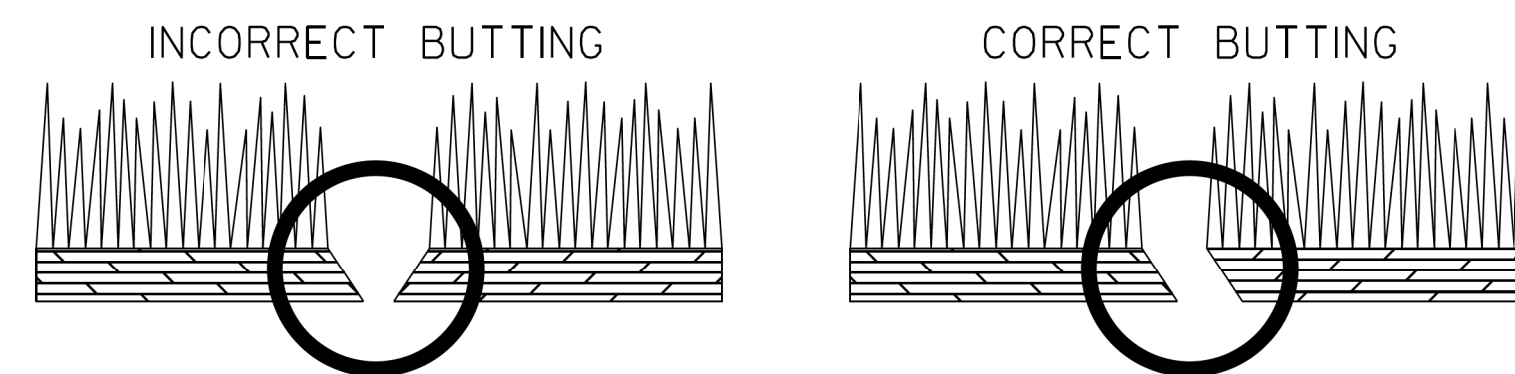


NOTE: SOD MAY BE EITHER 12" WIDE BY 22" LONG BLOCKS OR 21" WIDE BY 52" LONG ROLLS.

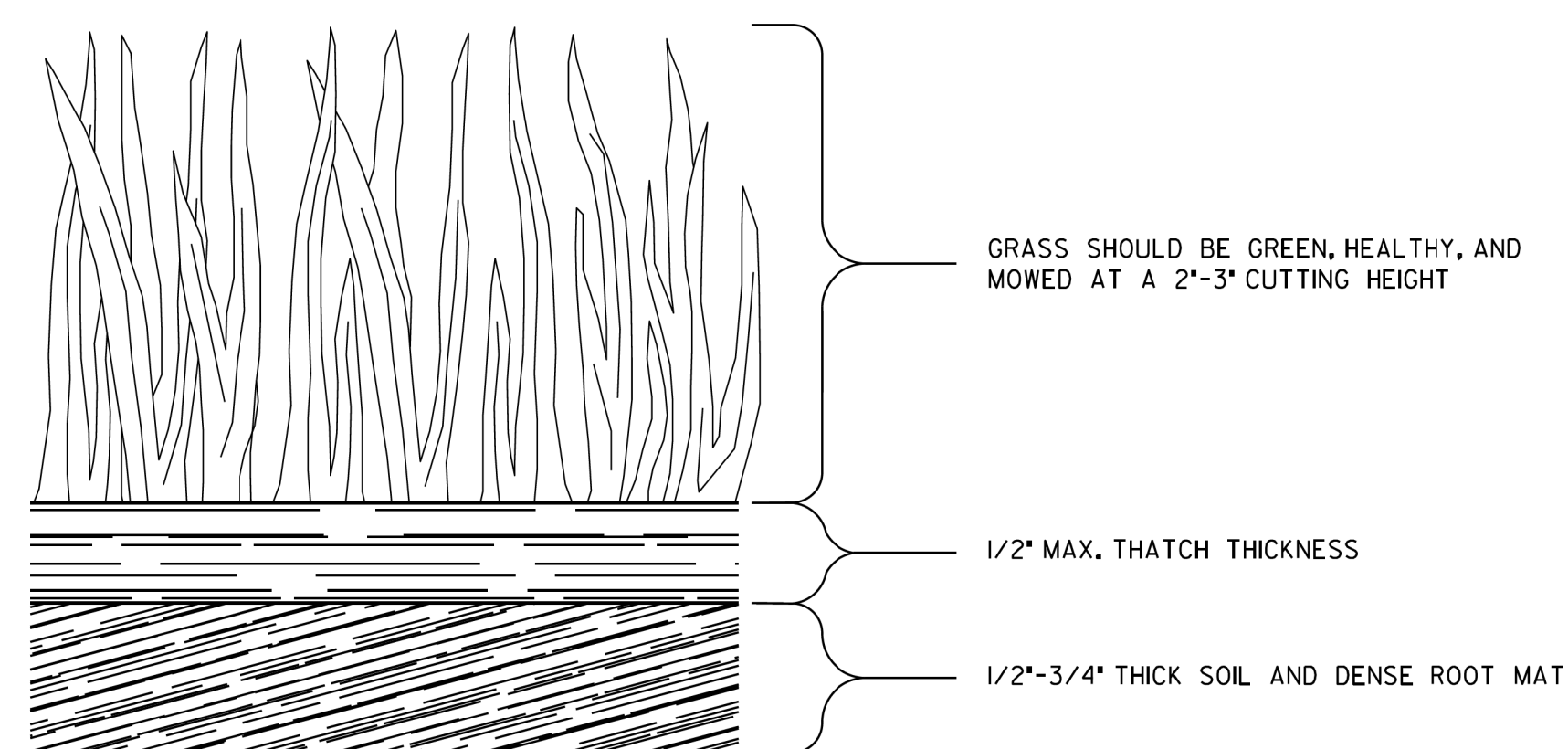
**GENERAL NOTES:**

- SOD SHALL MEET SECTIONS 700 AND 890 OF THE STANDARD SPECIFICATIONS AND SUPPLEMENTS THERETO. SOD SHALL BE CUT INTO 12"Wx22"L BLOCKS OR 21"Wx52"L ROLLS.
- PLACE SOD IN A STAGGERED PATTERN ENSURING FIRM CONTACT WITH THE SOIL. BUTT THE STRIPS TIGHTLY AGAINST EACH OTHER WITH THE AUTOMATIC SOD CUTTER ANGLES CORRECTLY MATCHED WITHOUT SPACES OR OVERLAP.
- PLACE THE LONG SIDE OF SOD PERPENDICULAR TO DRAINAGE FLOW IF INSTALLED IN DITCHES.
- STAKE SOD PLACED IN DITCHES OR SLOPES STEEPER THAN 2:1 OR ANY OTHER AREAS WHERE SOD SLIPPING MAY OCCUR. USE WOOD STAKES THAT ARE A MINIMUM OF 8' LONG AND A MAXIMUM OF 1" WIDE. DRIVE STAKES FLUSH WITH THE TOP OF SOD AND USE A MINIMUM OF 8 STAKES PER SQUARE YARD TO HOLD SOD IN PLACE.
- ROLL SOD IMMEDIATELY TO ACHIEVE FIRM CONTACT WITH THE SOIL.
- WATER THE SOD IMMEDIATELY AFTER INSTALLATION AND WATER TO A DEPTH OF 4" AS NEEDED.
- MOW ESTABLISHED SOD TO A HEIGHT NOT LESS THAN 2"-3" AS NECESSARY.

**ABUTTING SOD**



**SOD APPEARANCE**



PAY ITEM:  
700-9300 SOD (SY)

DATE		DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REVISION		CONSTRUCTION DETAILS SOD INSTALLATION	
NO SCALE		4-22-2016	
BY	DESIGNED DRAWN TRACED CHECKED	DLE _____ _____ _____	NUMBER D-54

**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING

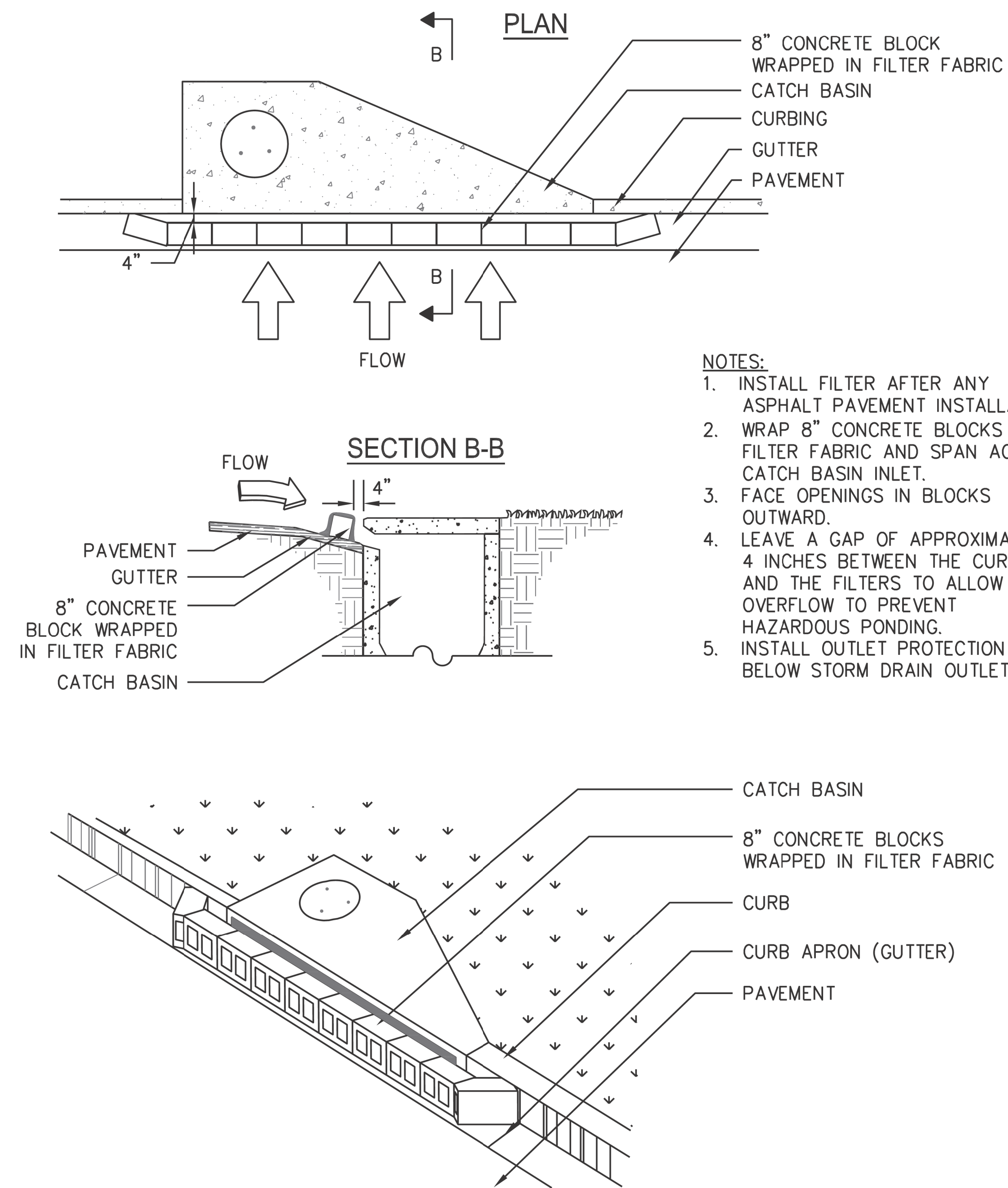
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**REVISION DATES**

**EROSION CONTROL CONSTRUCTION DETAILS  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	56-0005
CORRECTED:	DATE:	
VERIFIED:	DATE:	

# CURB INLET FILTER "PIGS IN BLANKET"



- NOTES:**
1. INSTALL FILTER AFTER ANY ASPHALT PAVEMENT INSTALLATION.
  2. WRAP 8" CONCRETE BLOCKS IN FILTER FABRIC AND SPAN ACROSS CATCH BASIN INLET.
  3. FACE OPENINGS IN BLOCKS OUTWARD.
  4. LEAVE A GAP OF APPROXIMATELY 4 INCHES BETWEEN THE CURB AND THE FILTERS TO ALLOW FOR OVERFLOW TO PREVENT HAZARDOUS PONDING.
  5. INSTALL OUTLET PROTECTION BELOW STORM DRAIN OUTLETS.

Figure 6-28.6 Curb Inlet Filter "Pigs in Blanket"

6-154

GSWCC 2016 Edition

**JE INFRASTRUCTURE**  
CONSULTING & ENGINEERING

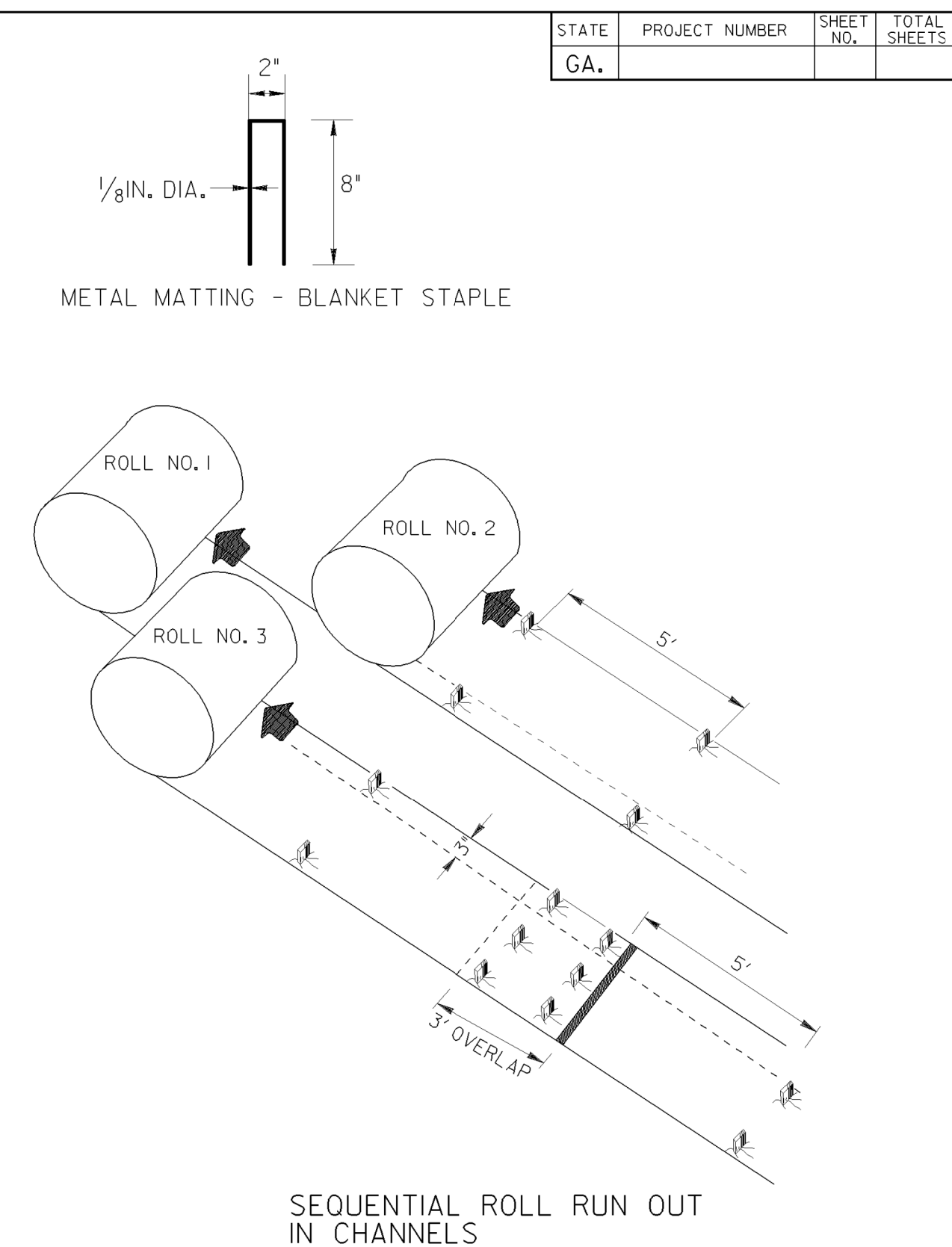
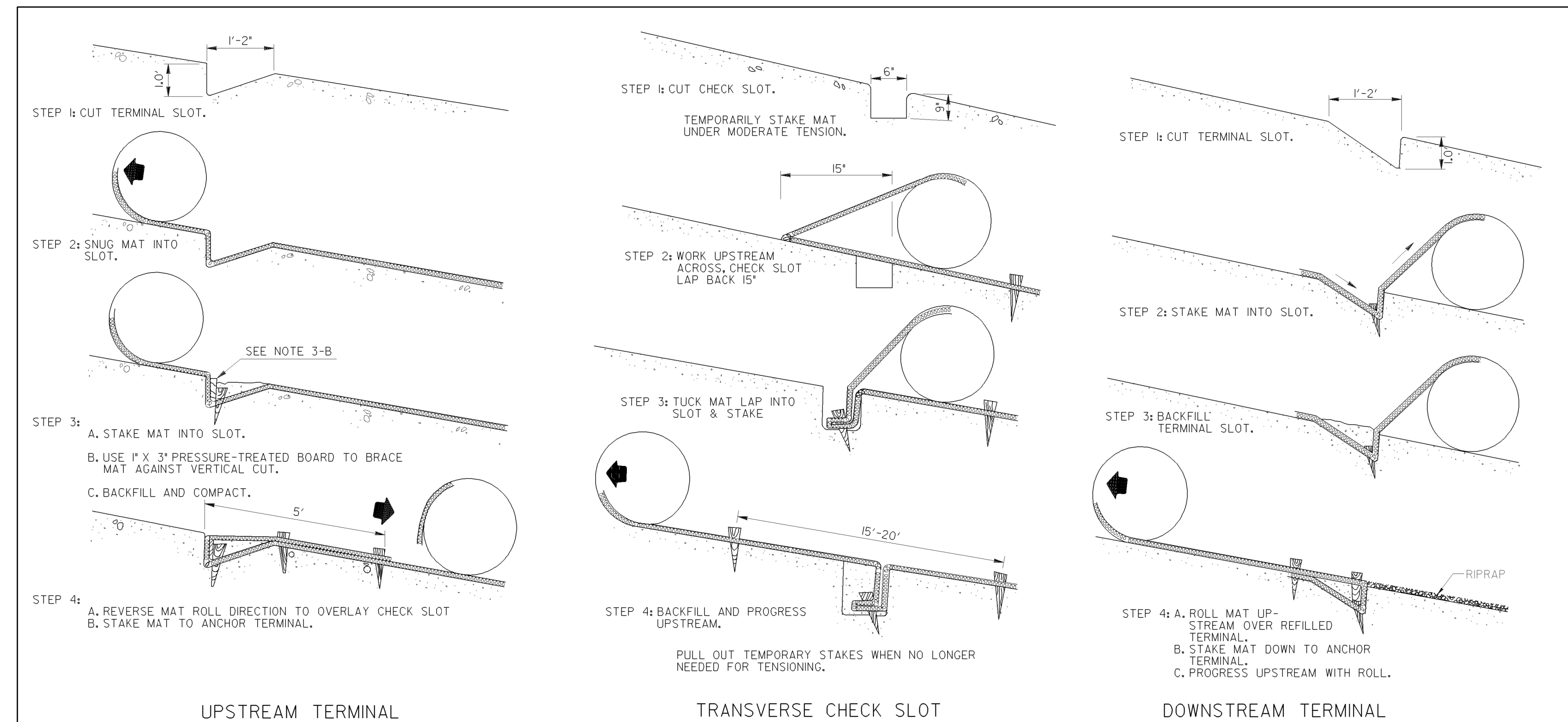
NOT TO SCALE

REVISION DATES

**EROSION CONTROL CONSTRUCTION DETAILS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

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BACKCHECKED:	DATE:	56-0006
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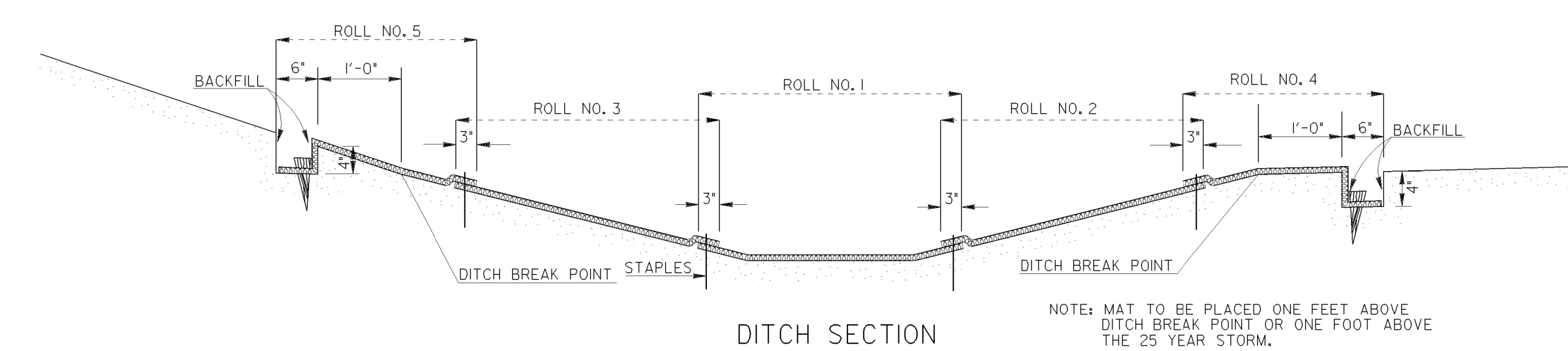
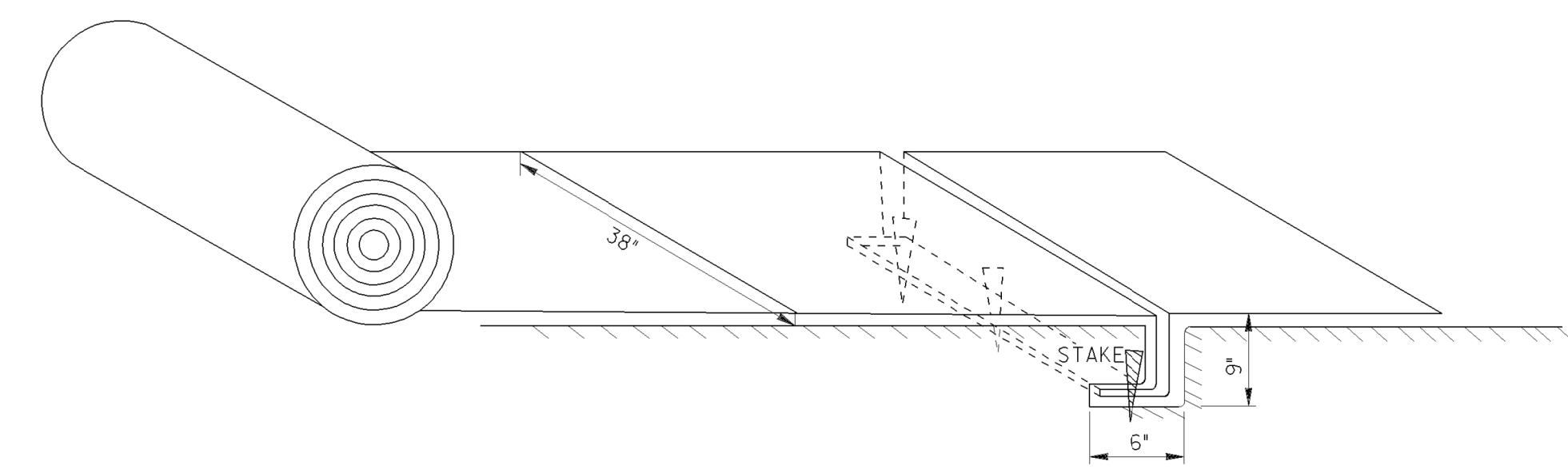
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STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

Metal Matting - Blanket Staple (Dimensions: 2" width, 8" height, 1/8" DIA.)

- GENERAL NOTES**
- INSTALLATION TO BE DONE AS PER MANUFACTURER'S RECOMMENDATIONS.
  - START AT DOWNSTREAM TERMINAL AND PROGRESS UPSTREAM.
  - FIRST ROLL IS CENTERED LONGITUDINALLY IN MID CHANNEL AND PINNED WITH TEMPORARY STAKES TO MAINTAIN ALIGNMENT.
  - SUBSEQUENT ROLLS FOLLOW IN STAGGERED SEQUENCE BEHIND FIRST ROLL. USE CENTER ROLL FOR ALIGNMENT TO CHANNEL CENTER.
  - WORK OUTWARDS FROM CHANNEL CENTER TO EDGE.
  - USE 3' OVERLAP AND STAKE AT 5' INTERVAL ALONG SEAMS.
  - USE 3' OVERLAPS AND SHINGLE DOWNSTREAM TO CONNECT LINING AT ROLL ENDS.
  - METAL STAPLES MAY BE USED IN LIEU OF WOODEN STAKES.



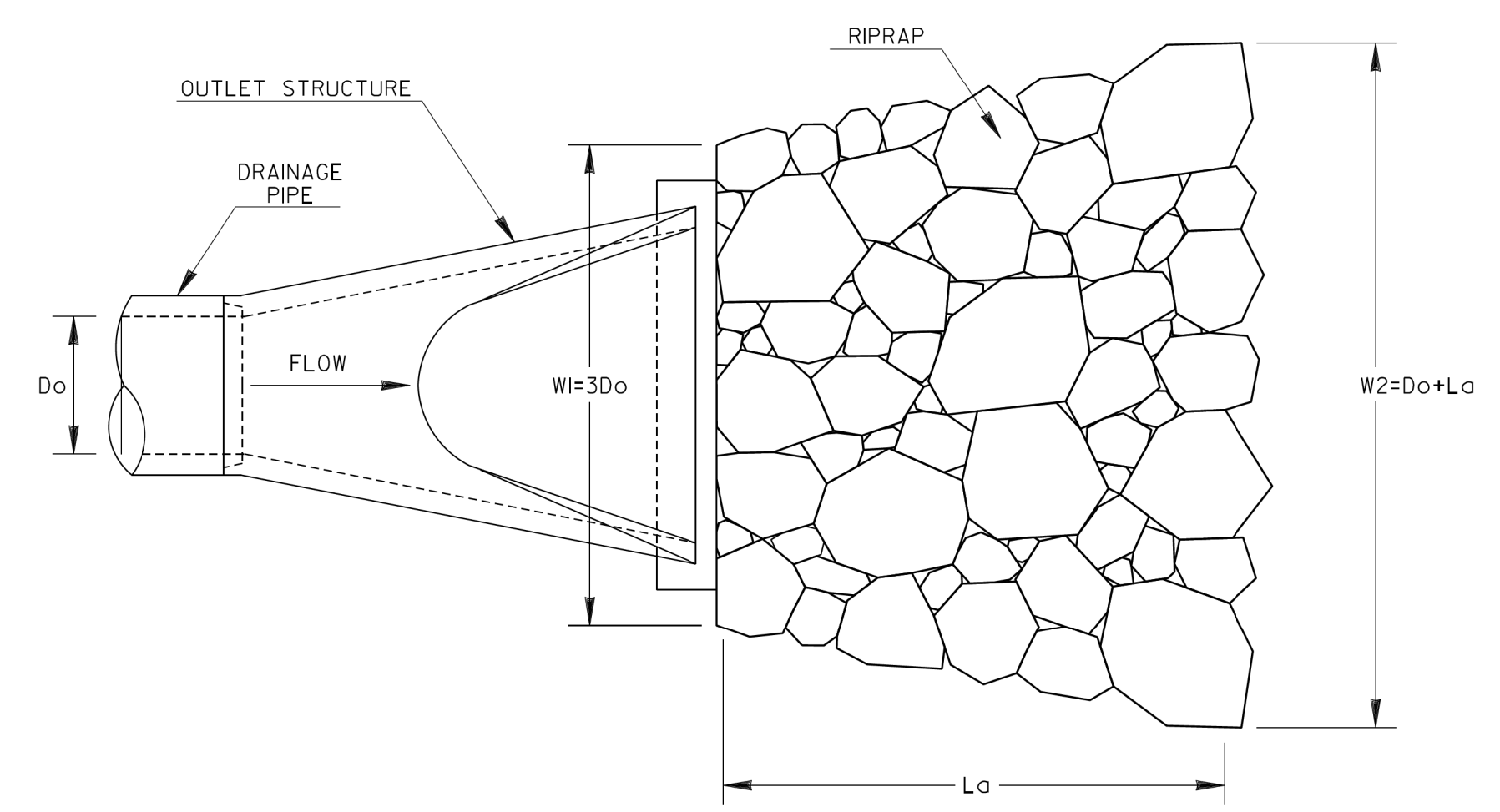
DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA			
CONSTRUCTION DETAILS PERMANENT SOIL REINFORCING MAT (TURF REINFORCING MATS) INSTALLATION ON DITCHES			
NO SCALE		AUGUST 1988	
NUMBER D-35			
DESIGNED	BY	DATE	
DRAWN	BY	DATE	
TRACED	BY	DATE	
CHECKED	BY	DATE	

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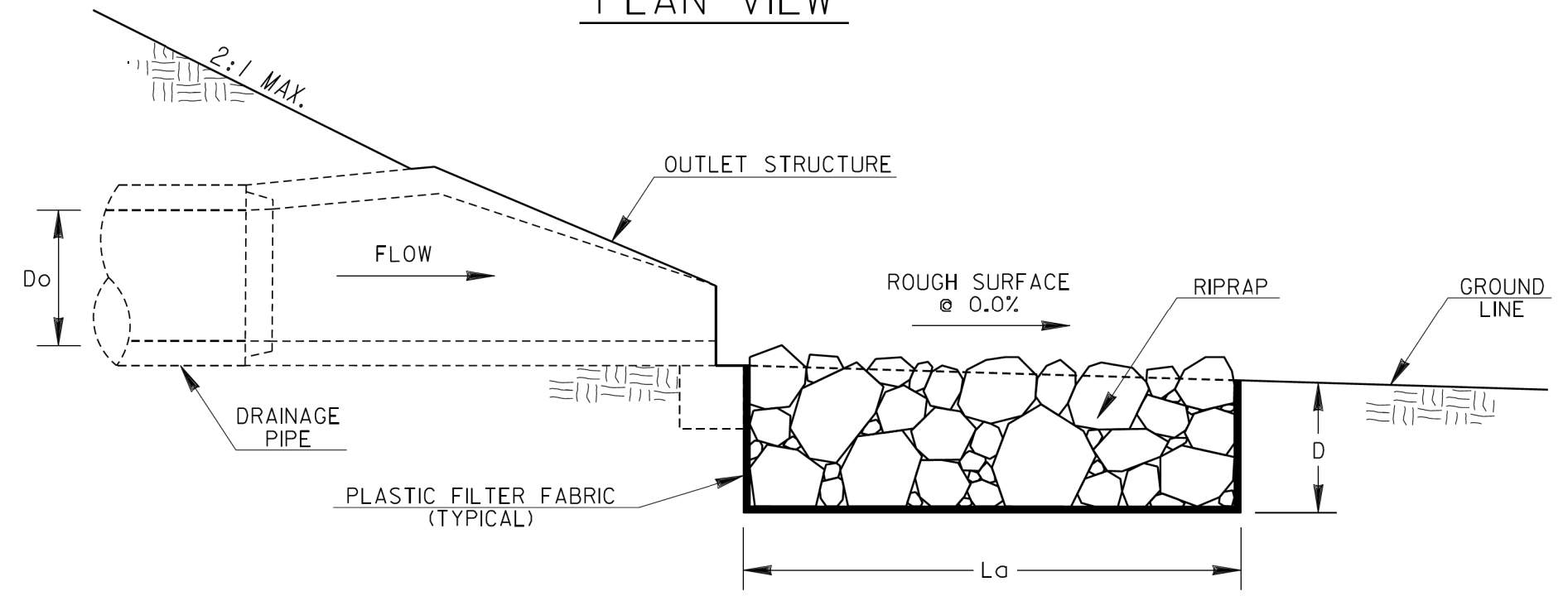
REVISION DATES		EROSION CONTROL CONSTRUCTION DETAILS BRIARWOOD ROAD SIDEWALK CITY OF BROOKHAVEN	
8/16/2019		CHECKED:	DATE:
		BACKCHECKED:	DATE:
		CORRECTED:	DATE:
		VERIFIED:	DATE:
		DRAWING No. 56-0007	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

OUTLET TO FLAT AREA

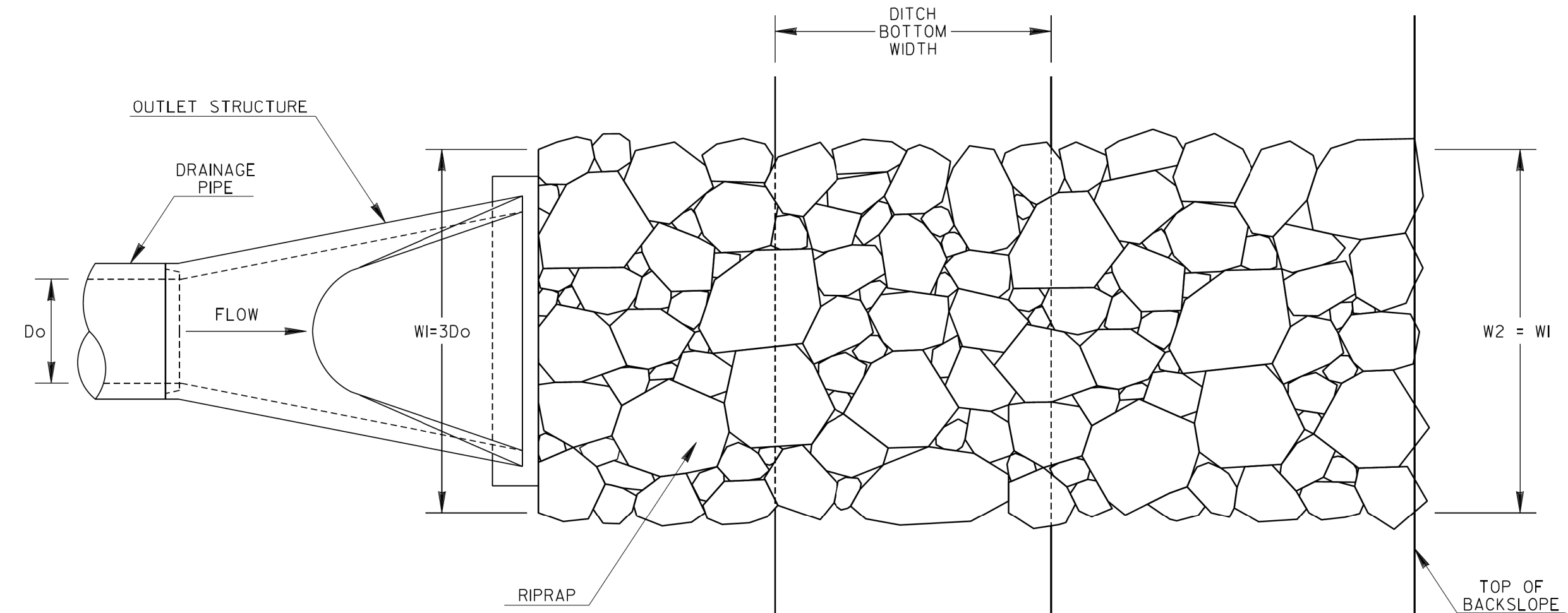


PLAN VIEW

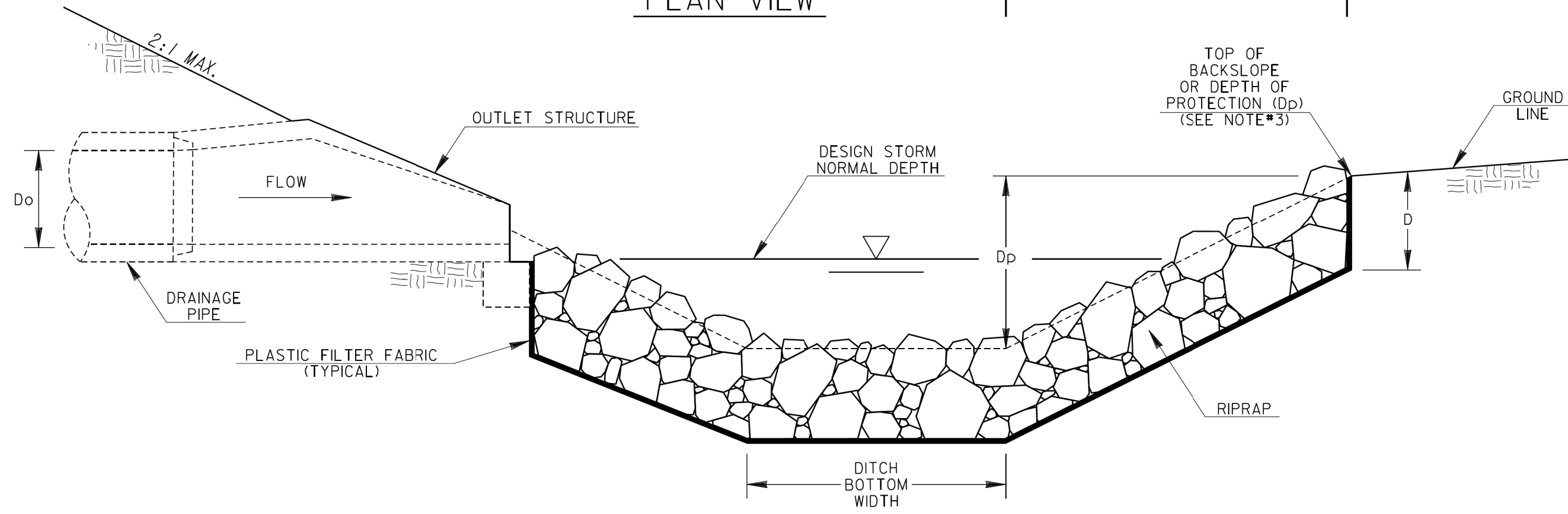


PROFILE VIEW

OUTLET PERPENDICULAR TO WELL-DEFINED CHANNEL



PLAN VIEW



PROFILE VIEW

GENERAL NOTES:

- RIPRAP OUTLET PROTECTION SHOULD BE USED TO REDUCE A DRAINAGE STRUCTURE'S DISCHARGE VELOCITY. RIPRAP OUTLET PROTECTION IS SHOWN FOR GEORGIA STANDARD 1120, BUT IS INSTALLED SIMILARLY FOR OTHER DRAINAGE OUTLET STRUCTURES.
- RIPRAP OUTLET PROTECTION SHALL BE DESIGNED IN ACCORDANCE WITH THE "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA". THE DESIGNER SHALL PROVIDE THE FOLLOWING IN THE PLANS: PIPE DIAMETER (Do), FLOW RATE OF DESIGN STORM (Q), VELOCITY (V), TAILWATER CONDITION (Tw), APRON LENGTH (La), APRON WIDTH AT DRAINAGE STRUCTURE (W1), APRON WIDTH DOWNSTREAM (W2), AVERAGE STONE DIAMETER (d50), INSTALLATION DEPTH (D), AND TYPE OF RIPRAP WITH QUANTITY.  
  
THE MINIMUM DESIGN FOR RIPRAP OUTLET PROTECTION SHALL BE THE 25-YEAR STORM EVENT, BUT LARGER STORMS ARE RECOMMENDED.
- THE APRON WIDTHS SHALL BE THE SAME WHEN THE DRAINAGE STRUCTURE DISCHARGES PERPENDICULAR INTO A WELL-DEFINED CHANNEL. THE LENGTH SHALL EXTEND ACROSS THE CHANNEL AND UP TO THE TOP OF THE CHANNEL BACKSLOPE OR 1-FOOT ABOVE THE NORMAL DEPTH OF THE CHANNEL'S DESIGN STORM (WHICHEVER IS LESS). THE DESIGNER SHALL PROVIDE THE DEPTH OF PROTECTION (Dp) IF THE APRON DOES NOT EXTEND TO THE TOP OF THE BACKSLOPE.
- IF THE OUTLET HYDRAULICS REQUIRE A d50<=0.70 FEET, TYPE-3 RIPRAP MAY BE USED.  
IF THE OUTLET HYDRAULICS REQUIRE A d50<=1.20 FEET, TYPE-1 RIPRAP SHOULD BE USED.  
IF THE OUTLET HYDRAULICS REQUIRE A d50>1.20 FEET, THE DESIGNER SHALL DESIGN AND PROVIDE A SPECIAL DETAIL FOR APPROPRIATE OUTLET PROTECTION.
- PLASTIC FILTER FABRIC IS REQUIRED UNDERNEATH RIPRAP APRON.
- PAYMENT FOR RIPRAP SHALL BE MEASURED IN SQUARE YARDS FOR SPECIFIED INSTALLATION DEPTH. PAYMENT FOR PLASTIC FILTER FABRIC SHALL BE MEASURED IN SQUARE YARDS CONSISTENT WITH RIPRAP QUANTITY AND PAID FOR SEPARATELY.

- Do = PIPE DIAMETER
- Q = DESIGN STORM FLOW RATE
- V = DESIGN STORM VELOCITY
- Tw = TAILWATER CONDITION/DESIGN STORM NORMAL DEPTH
- La = APRON LENGTH
- W1 = APRON WIDTH UPSTREAM
- W2 = APRON WIDTH DOWNSTREAM
- d50 = AVERAGE STONE DIAMETER
- D = INSTALLATION DEPTH
- Dp = DEPTH OF PROTECTION

RIPRAP TYPE	REQUIRED d50 (FT)	MIN. DEPTH "D" (IN)
1	≤1.20	36
3	≤0.67	18

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
CONSTRUCTION DETAILS	
RIPRAP OUTLET PROTECTION (SHEET 1 OF 2)	
NO SCALE	4-22-2016
DESIGNED <u>DLE</u>	NUMBER
DRAWN <u>DLE</u>	D-55A
TRACED <u>_____</u>	
CHECKED <u>_____</u>	

**INFRASTRUCTURE**  
CONSULTING & ENGINEERING

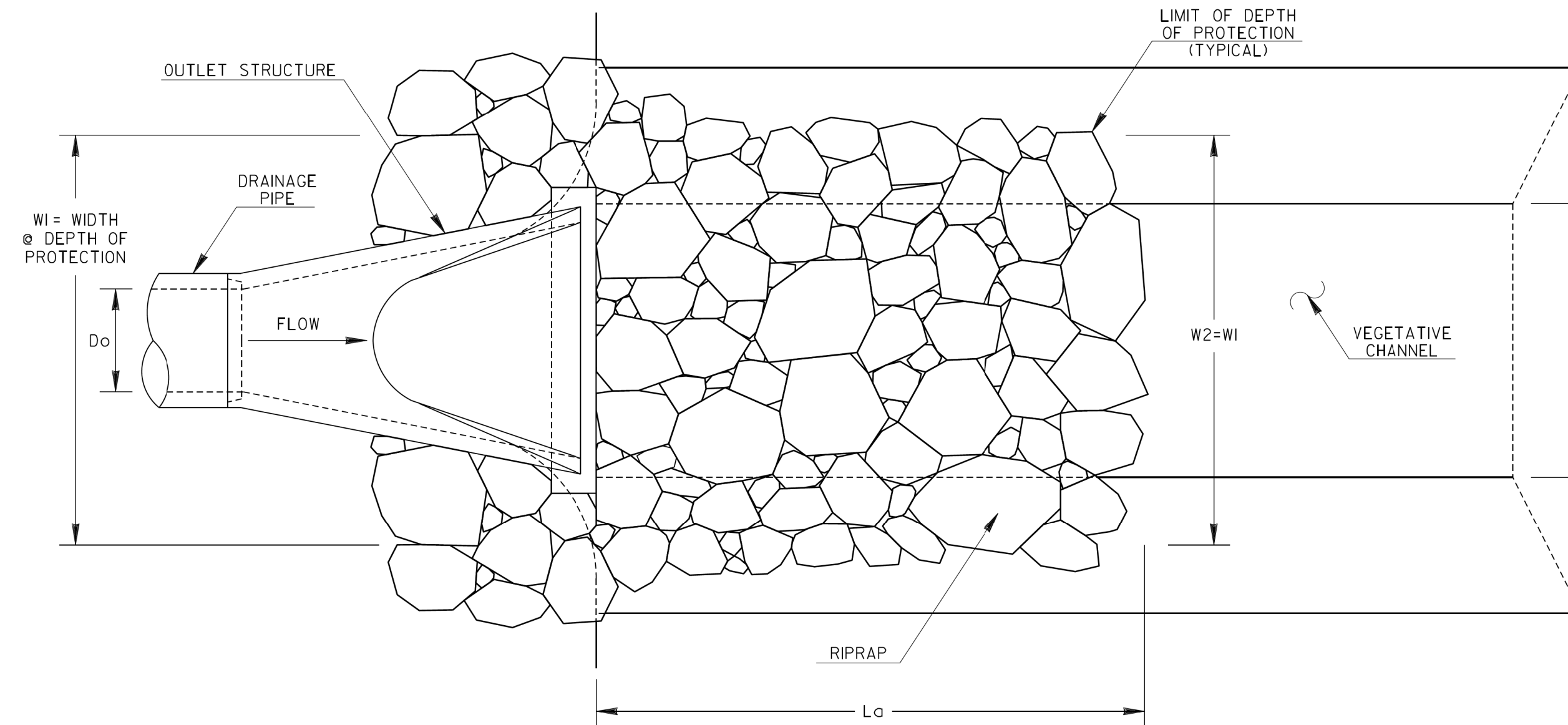
REVISION DATES
8/16/2019

**EROSION CONTROL CONSTRUCTION DETAILS**  
BRIARWOOD ROAD SIDEWALK  
CITY OF BROOKHAVEN

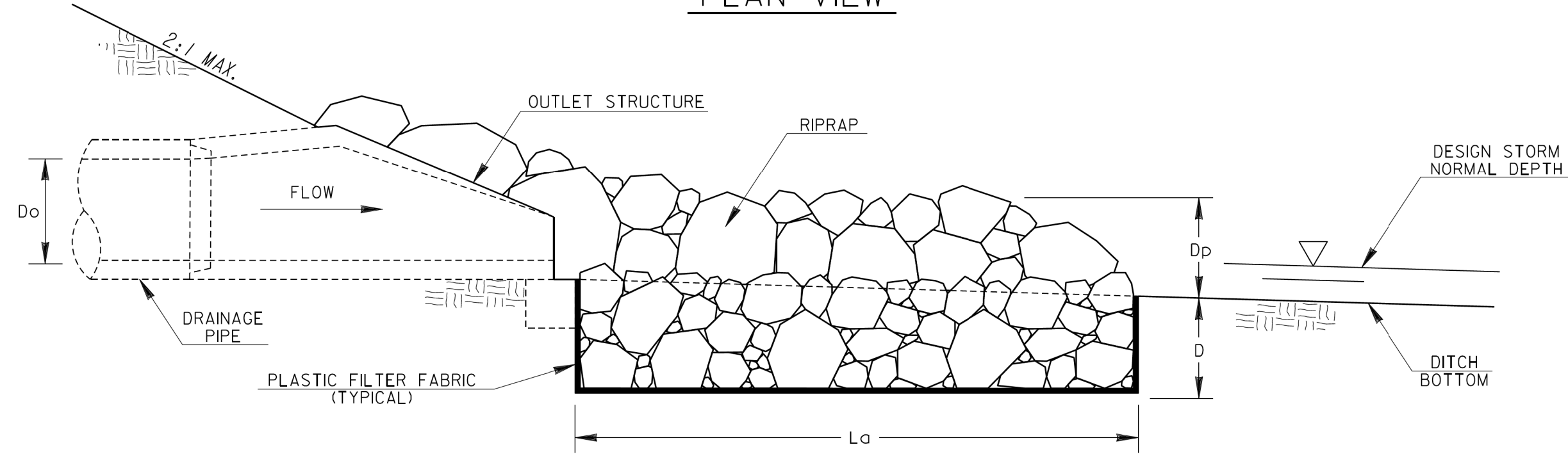
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BACKCHECKED:	DATE:	
CORRECTED:	DATE:	56-0008
VERIFIED:	DATE:	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.			

**OUTLET PARALLEL TO WELL-DEFINED CHANNEL**

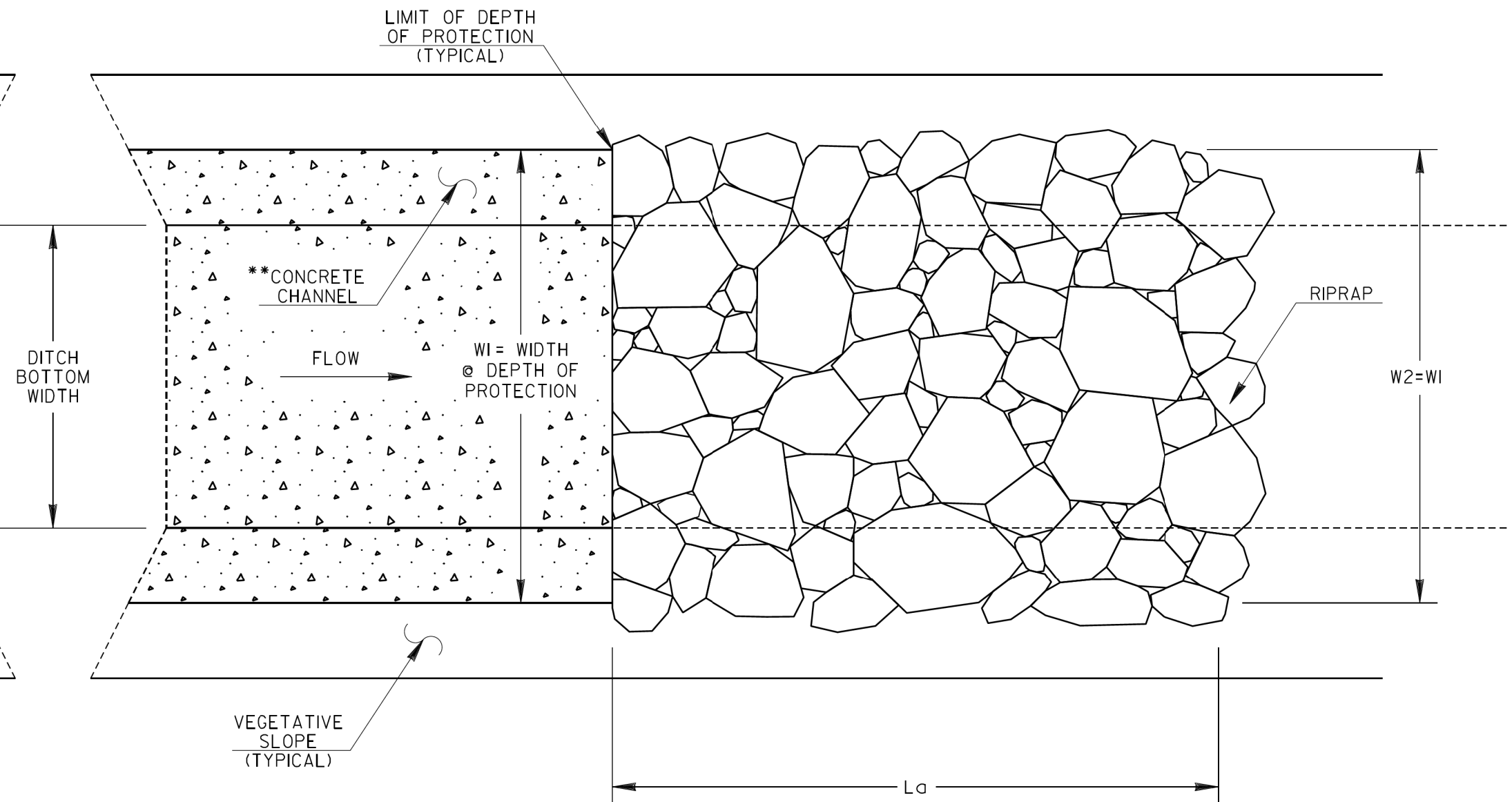


PLAN VIEW



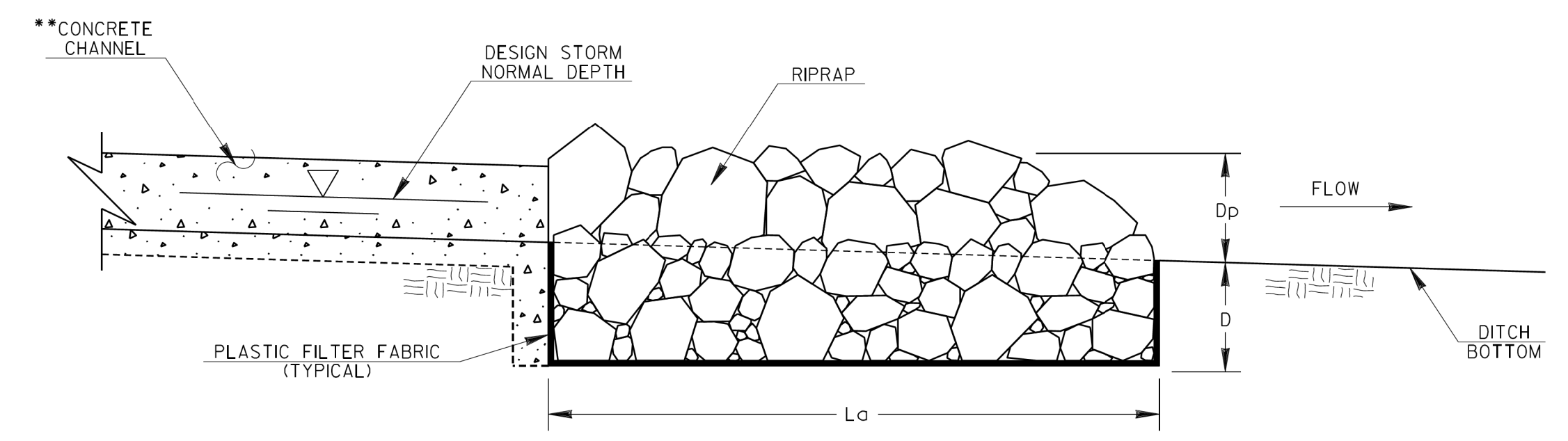
PROFILE VIEW

**CONCRETE CHANNEL TO RIPRAP TRANSITION**



PLAN VIEW

\*\*REFER TO CONSTRUCTION DETAIL D-10 FOR CONCRETE DITCH PAVING INFORMATION



PROFILE VIEW

**GENERAL NOTES:**

- RIPRAP OUTLET PROTECTION SHOULD BE USED TO REDUCE A DRAINAGE STRUCTURE'S DISCHARGE VELOCITY. RIPRAP OUTLET PROTECTION IS SHOWN FOR GEORGIA STANDARD 1120, BUT IS INSTALLED SIMILARLY FOR OTHER DRAINAGE OUTLET STRUCTURES. RIPRAP OUTLET PROTECTION IS SHOWN FOR A CONCRETE DITCH, BUT IS INSTALLED SIMILARLY TO TRANSITION FROM OTHER CHANNEL LININGS.
- RIPRAP OUTLET PROTECTION SHALL BE DESIGNED IN ACCORDANCE WITH THE "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA". THE DESIGNER SHALL PROVIDE THE FOLLOWING IN THE PLANS: PIPE DIAMETER (Do), FLOW RATE OF DESIGN STORM (Q), VELOCITY (V), TAILWATER CONDITION (Tw), APRON LENGTH (La), APRON WIDTH AT DRAINAGE STRUCTURE (W1), APRON WIDTH DOWNSTREAM (W2), AVERAGE STONE DIAMETER (d50), INSTALLATION DEPTH (D), AND TYPE OF RIPRAP WITH QUANTITY.  
THE MINIMUM DESIGN FOR RIPRAP OUTLET PROTECTION SHALL BE THE 25-YEAR STORM EVENT, BUT LARGER STORMS ARE RECOMMENDED.
- THE APRON WIDTHS SHALL BE THE SAME WHEN THE DRAINAGE STRUCTURE DISCHARGES PARALLEL INTO A WELL-DEFINED CHANNEL. THE APRON WIDTHS IN THIS CASE SHALL REPRESENT THE WIDTH AT THE DEPTH OF PROTECTION. THE RIPRAP SHALL BE INSTALLED TO THE TOP OF CHANNEL OR 1-FOOT ABOVE THE NORMAL DEPTH OF THE CHANNEL'S DESIGN STORM (WHICHEVER IS LESS). THE DESIGNER SHALL PROVIDE THE DEPTH OF PROTECTION (Dp) IF THE RIPRAP SHOULD NOT BE INSTALLED TO THE TOP OF THE CHANNEL. RIPRAP SHOULD ALSO BE INSTALLED TO ARMOR CHANNEL CORNER AT THE OUTLET STRUCTURE.
- IF THE OUTLET HYDRAULICS REQUIRE A d50<=0.70 FEET, TYPE-3 RIPRAP MAY BE USED.  
IF THE OUTLET HYDRAULICS REQUIRE A d50<=1.20 FEET, TYPE-1 RIPRAP SHOULD BE USED.  
IF THE OUTLET HYDRAULICS REQUIRE A d50>1.20 FEET, THE DESIGNER SHALL DESIGN AND PROVIDE A SPECIAL DETAIL FOR APPROPRIATE OUTLET PROTECTION.
- PLASTIC FILTER FABRIC IS REQUIRED UNDERNEATH RIPRAP APRON.
- PAYMENT FOR RIPRAP SHALL BE MEASURED IN SQUARE YARDS FOR SPECIFIED INSTALLATION DEPTH. PAYMENT FOR PLASTIC FILTER FABRIC SHALL BE MEASURED IN SQUARE YARDS CONSISTENT WITH RIPRAP QUANTITY AND PAID FOR SEPARATELY.

- Do = PIPE DIAMETER
- Q = DESIGN STORM FLOW RATE
- V = DESIGN STORM VELOCITY
- Tw = TAILWATER CONDITION/DESIGN STORM NORMAL DEPTH
- La = APRON LENGTH
- W1 = APRON WIDTH UPSTREAM AT DEPTH OF PROTECTION
- W2 = APRON WIDTH DOWNSTREAM AT DEPTH OF PROTECTION
- d50 = AVERAGE STONE DIAMETER
- D = INSTALLATION DEPTH
- Dp = DEPTH OF PROTECTION

RIPRAP TYPE	REQUIRED d50 (FT)	MIN. DEPTH "D" (IN)
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3	≤0.67	18

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
CONSTRUCTION DETAILS	
RIPRAP OUTLET PROTECTION (SHEET 2 OF 2)	
NO SCALE	4-22-2016
BY: DESIGNED <u>DLE</u> DRAWN <u>DLE</u> TRACED _____ CHECKED _____	NUMBER D-55B



REVISION	DATE
1	8/16/2019

**EROSION CONTROL CONSTRUCTION DETAILS**  
**BRIARWOOD ROAD SIDEWALK**  
**CITY OF BROOKHAVEN**

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	56-0009
CORRECTED:	DATE:	
VERIFIED:	DATE:	